



CITY OF
ISSAQUAH
WASHINGTON

Development Services
1775 – 12th Ave. NW | P.O. Box 1307
Issaquah, WA 98027
425-837-3100
issaquahwa.gov

NOTICE OF DECISION

ADMINISTRATIVE SITE DEVELOPMENT PERMIT GILMAN POINT

APPLICATION NO: ASDP15-00002

August 27, 2015

I. Application Information

Project name: Gilman Point

Staff Contact: Mike Martin, Associate Planner
Development Services Department, 425-837-3103
mikem@issaquahwa.gov

Property Owner: Kirk and Karen Catterall
160 NW Gilman Boulevard
Issaquah, WA 98027

Authorized Agent: Bob Power
Project Coordinator, Authorized Agent
SeaCon
165 NE Juniper Street, Suite 100
Issaquah, WA 98027

Architect: Dirk McCulloch
Magellan Architects
8383 158th Ave NE, Suite 280
Redmond, WA 98052

Request: Application for an Administrative Site Development Permit for construction of one 4-story, fully enclosed self-storage building of approximately 88,000 sq. ft. and one tire shop of appx. 12,000 sq. ft. with (7) service bays and a retail/office space. Site improvements consist of grading and associated improvements including 44 parking spaces, landscaping, utility improvements and buffer enhancements across a total site area of 82,134 sq. ft (1.89 acres).

Location: The Property is located at 160 NW Gilman Boulevard.

Existing Land Use: The property currently contains one 5,976 square foot commercial/retail building with three independent retailers (per King County, the building was constructed in 1950).

Adjacent Uses:

- North: Interstate 90
- South: NW Gilman Boulevard
- East: Pogacha Restaurant
- West: East Lake Sammamish (King County) Multi-purpose trail, 4th Avenue NW

Zoning: "MU" (Mixed Use), Effective April 29, 2013

Comprehensive Plan:

- Ord. 2706, Amended effective February 18, 2014
- Land Use: "Mixed Use"
- Subarea: "Gilman"

II. Decision

Based upon the application, submitted plans (March 10, 2015), listed Attachments, supplemental materials and rationale contained in this Notice of Decision, the Development Services Department approved the Administrative Site Development Permit application, subject to the notes, terms and conditions of this Notice of Decision.

III: Approval Criteria

The purpose of the Administrative Site Development Permit (ASDP) is to obtain planning level approval with the confidence that the project meets the standards and guidelines contained in the CIDDs and, where appropriate, City Code, prior to the preparation of detailed infrastructure, building, and/or engineering or architectural drawings. The CIDDs contain a variety of development goals and standards that are organized into individual appendices. Each appendix provides direction to the applicant generally in the form of guidelines and/or development standards for a particular aspect of the project.

The Development Services Department is the Decision Maker for the Administrative Site Development Permit application. CIDDS Chapter 3.9 identifies the approval criteria as:

- A. The development proposal is consistent with the Comprehensive Plan and Central Issaquah Plan;
- B. The development proposal meets all applicable codes, rules, regulations and policies; and
- C. The development proposal satisfies the elements of the Development and Design Standards.

Only those standards that apply to the Development proposal are discussed in this report. Also see Notice of Decision Attachment 1.

The applicant will develop the property in one phase, though no specific timing is required. As the specific development applications are made, additional conditions may be applied through subsequent permits to ensure compliance with the Development Agreement, CIP, CIDDS, IMC, etc....

IV. Review of Central Issaquah Development and Design Standards (CIDDS)

Attachment 1 contains a table and provides a detailed review of the development proposal and associated application materials and drawings against the CIDDS. To facilitate use of the table, the following are provided to explain the column headings:

CIDDS Standard Number: The numbers used in the CIDDS to identify various development and design standards.

Name: The name/title used in the CIDDS, associated with the CIDDS Standard Number. If no title is provided, a brief description is used in parenthesis, e.g. 14.4.A.5 (primary entrances)

Staff Analysis: Staff analysis of the project's compliance with the specific CIDDS development or design standard.

Review at Construction: These standards are not reviewed, or only partially reviewed, with this development proposal and will be reviewed with future land use and construction permits.

Meets Standard: X - Staff believes this item has been acceptably addressed, at a land use level of review, as shown or described in the Development proposal. This item will receive further review with future land use and construction permits when additional detail is provided.

Does Not Meet Standard: X - Staff believes this item necessitates clarification or changes to comply with the CIDDS. This may be the basis for a condition of the

ASDP. When a condition is applicable, it is noted in the adjacent column “Conditions of Approval”.

Conditions of Approval: Where a project element does not comply with the applicable development or design standard, a Condition may be applied to the ASDP. The condition number and language is found under this column.

ASDP15-00002 – CONDITIONS OF APPROVAL

Pursuant to the Central Issaquah Development and Design Standards, the following Conditions shall apply to the project. Also see CIDDS Review Spreadsheet for detailed review and explanation of the CIDDS.

Condition 1: Unless expressly identified, approval of this ASDP application does not modify any Central Issaquah Development and Design Standards (CIDDS) or City standards which are in conflict with elements of the ASDP plan or application. Modification of the standards or guidelines requires an explicit approval in the Notice of Decision for this application or a separate Modification as allowed under the applicable section of the CIDDS.

Condition 2: The retail portion of the storage facility must have a ceiling height of at least 15 feet. This will be verified with the Building permit.

Condition 3: The Density Bonus Program shall apply to all Gross Floor Area of the project that exceeds Base FAR; or, for the total floor area of all floors above 48 feet in height, whichever is greater.

Condition 4: A trail connection shall be provided near the primary entrance of the storage facility to the East Lake Sammamish Trail. The applicant shall work with the City and King County to obtain the necessary approvals to construct the trail.

Condition 5: All pedestrian circulation facilities shall be a minimum of 5 feet in width, clear of intrusions (bollards, handrails, railings, etc.). For the west sidewalk of the internal street, the applicant shall explore options that would allow the west sidewalk to be widened.

Condition 6: Prior to submittal of any Building permits, provide a Transportation Demand Study that shows that the amount of parking provided for the project is adequate based comparable projects in the Pacific Northwest. The Transportation Demand Study shall be performed by a parking professional that is jointly selected by the Applicant and the City.

Condition 7: Prior to issuance of any Building permits, the Applicant shall provide a parking agreement, acceptable to the Director, which demonstrates that sufficient parking is available (in perpetuity) for both of the proposed uses. The agreement shall be recorded on title and filed with the King County Recorder’s Office.

Condition 8: At least 2 bicycle parking spaces shall be provided for each use (4 total at a minimum). The bicycle parking shall be covered and shall be provided within a close proximity to the primary entrance to each building. This will be reviewed with the Site Work and Landscape permits.

Condition 9: Parking stall dimensions shall be the minimum necessary per the requirements. For standard stalls, the maximum dimension shall be 9 x 18.5 feet. If compact stalls are used, they shall be 8 x 16 feet. Where feasible, use of the 2 foot overhang into planted areas is encouraged.

Condition 10: Where parking lot drive aisle widths must be wider than the standardized dimension in order to provide emergency and large vehicle access, special techniques, e.g. concrete, shall be used where the drive aisle exceeds the allowed width (e.g., the area to the east of the tire facility and adjacent to the loading area).

Condition 11: "No Parking – Fire Lane" zones shall be indicated by the use of signs. Painted curbs shall not be used unless expressly directed by the Fire Marshal.

Condition 12: The total of all interior landscaping for the parking lot, excluding Edge Landscaping, shall be equal to or greater than 10 percent of the total parking lot area. The portion of parking lot that is underneath the building overhang of the storage facility shall not be counted toward the overall parking lot area calculation.

Condition 13: All utility vaults, cabinets, switchgear, pull boxes, meters, equipment, and appurtenances are assumed to be shown on the Permit submittal. Anything not shown on the Permit submittal (location, relative height, presence above ground) is assumed to be located within the buildings. Any revisions or additions to what the Permit has shown and approved outside of the buildings requires a modification to the Permit, except for fire hydrants.

Condition 14: Any retaining walls shall not be constructed as rockeries and shall be block or other allowed materials. This will be reviewed with the Site Work and Landscape permits.

Condition 15: In order to mitigate for the reduction in tree retention; and, to meet the minimum tree density for the lot, replacement trees must be provided at a rate of 5 trees per 5,000 square feet of Developable Site Area and per Section 10.13 and 10.14 of the CIDDS. The value of the existing trees shall be established with the Site Work permit and prior to the trees removal.

Condition 16: Additional site furnishings, including but not limited to waste receptacles, benches, sculptures, artwork, etc. shall be provided. This condition will be reviewed and approved with the Site Work and Landscape permits.

Condition 17: Decorative concrete shall be provided for a minimum of 30 feet in length at the project entrance (between the primary entrance to the tire facility and the sidewalk connection

on opposite side of internal street). Paving treatment may include, but is not limited to, stamping, scoring and variations in color and texture.

Condition 18: Any existing non-conforming ADA crosswalks and ramps that provide immediate access to circulation facilities within the project shall be reconstructed so that they are in compliance with ADA standards in effect at the time of permit issuance.

Condition 19: The external waste enclosure shall be wildlife proof, covered and sized appropriately in order to accommodate three waste streams (trash, recycling and food waste). Sizing of the enclosure shall be confirmed prior to issuance of the Site Work permit. Screening of the enclosure shall also be provided.

Condition 20: The rear (east) elevation of the tire facility (including the wing wall extension for the loading area) shall incorporate additional wall treatments, including but not limited to canopies, building modulation and detailing, varied materials and textures, applied elements, trellises, etc., in order to reduce its blank appearance.

Condition 21: The following walls of the storage facility shall incorporate additional wall treatments, including but not limited to canopies, building modulation and detailing, varied materials and textures, applied elements, trellises, etc., in order to reduce its blank appearance:

1. Wall on south elevation, underneath building overhang between the retail store entrance and the overhead garage doors;
2. West elevation: the untreated wall section between and above the retail store front and the tri-color block wall located at the back of the building;
3. North elevation: the north wall elevation shall be designed with dark, muted tones and materials in order to reduce its visibility from off-site.
4. East elevation: the east wall elevation shall be designed with colors and materials to achieve balance between the presence of the rear of the building with the lush landscape that is present along Interstate 90. This includes the addition of varied color blocking, modulation or other techniques that serve to partially obscure the wall while still making it attractive to vehicles traveling at freeway speeds.

Condition 22: Exterior walls located above the 3rd floor level of the storage facility shall be designed so as to create the sense of a building setback through changes in building materials, articulation and modulation that differs from the first three floors.

Condition 23: Both of the retail storefront (customer) doors of the tire facility shall be open and available so that customers may both enter and exit during regular business hours. The doors are intended for public use and shall not be blocked or otherwise prevent in/out access by patrons.

Condition 24: Weather protection shall be provided for at least 75 percent of the tire and storage facility building that fronts onto the internal street. For the tire facility, the weather protection shall generally extend from the intersection of the crosswalk to the secondary

building entrance (parking lot side). Weather protection must conform to height and projection dimensions as required by the Standards of CIDD Section 14.5.A.

Condition 25: The tire and storage facility shall utilize parapets and/or projecting cornices to create a prominent edge when viewed against the sky.

Condition 26: Light poles shall not exceed 15 feet in height.

V. SEPA Review

The Development Services Department has determined that the proposed development does not have a probable significant adverse impact on the environment and therefore an environmental impact statement is not required under RCW 43.21C.030(2)(c). This decision was made after review of a completed environmental checklist and other information on file with the lead agency.

On July 9, 2015, the Development Services Department issued a Mitigated Determination of Non-Significance (MDNS) – see Attachment 4.

Pursuant to the SEPA MDNS the following Conditions shall apply to the project:

- Final wetland/wetland buffer enhancement plans are required for approval by the Issaquah Development Services Department (DSD) prior to issuing construction permits. Final plans shall include a planting plan and a 5-year monitoring/maintenance plan with performance standards for monitoring success of the enhancement planting. The plans shall meet standards of the King County Critical Areas Mitigation Guidelines for the planting density and monitoring performance standards. **(ASDP15-00002 – CONDITION 27)**
- To address the safety and operations of the site access, the driveway access shall be restricted to right-in/right-out turn movements only. The applicant shall install C-curb on NW Gilman Blvd to limit turn movements. Final plans for the driveway access onto NW Gilman Blvd shall be approved prior to issuance of construction permits. **(ASDP15-00002 – CONDITION 28)**
- Due to the configuration of the access driveway, turning movements of large delivery trucks may require both lanes on NW Gilman Blvd. Therefore, the site access for trucks and truck trailers 35-feet and longer shall be restricted to the hours between 10:00 PM and 6:00 AM. **(ASDP15-00002 – CONDITION 29)**
- The applicant shall mitigate for potential impacts on public services and bicycle and pedestrian facilities. The City may approve a voluntary payment in lieu of other mitigation. Rate studies for public services and bicycle and pedestrian facilities, adopted in IMC 18.10.260 SEPA Policies provide the methodology and formulas for determining the amount of the voluntary mitigation fee commensurate with the proposed land use

and project impacts. Applicant objections to the voluntary payments should be made during the SEPA comment period. The impact fee cost will be determined based on the new building area approved in the building permit application and the impact fee in effect at permit issuance. The applicant should pay the voluntary contribution prior to issuance of building permits. **(ASDP15-00002 – CONDITION 30)**

VII. Additional Review: Departments, Others, Public Comments

Engineering Utility Review (DSD, PWE, PWO) – Conditions of Approval

ASDP Condition 31: Cut and cap water service for 160 NW Gilman Boulevard with demolition of existing building, unless otherwise necessary for temporary construction usage. All salvageable materials shall be delivered to the City's Public Works Operations Department.

ASDP Condition 32: Water main shall be looped. Gate valves shall be provided at each point of connection.

ASDP Condition 33: Meter, hydrant, and fire line for storage building to come off of 12" loop. No dead-end water mains are allowed where future connection may be feasible.

ASDP Condition 34: New water main will need to start in front of 90 NW Gilman Boulevard. Existing hydrants shall be reconnected.

ASDP Condition 35: Move domestic water meter for storage building prior to hydrant (for water quality purposes).

ASDP Condition 36: Water main east of Les Schwab shall be 12" and terminated with a blow-off 6" fire line with valve shall tee off 12" main.

ASDP Condition 37: Underground vaults shall be designed to support fire department apparatuses where they will be impacted by the fire department. Point loads for outriggers shall be designed to support 10,800 pounds per square foot.

ASDP Condition 38: Storm water from NW Gilman Boulevard right-of-way shall be routed to public system, not on-site.

ASDP Condition 39: Storm maintenance and inspection agreement and easement must be in place prior to acceptance of storm facilities.

Eastside Fire and Rescue (EF&R) - Conditions of Approval:

ASDP Condition 40: Hydrant locations shall be approved by the Fire Marshal.

ASDP Condition 41: Access to the rear of the storage structure is required via an approved walkway that ends at a staintower with standpipe connections inside.

Public Works Engineering:

Review of the utilities and circulation facilities was reviewed by the Public Works Engineering Department. Their input has been reflected in this decision.

Construction Services Division of DSD:

The Construction Services Division of DSD will review future land use and construction permits.

Other Reviews

Waste Collection:

Preliminary review of the proposed design and calculations for waste collection were reviewed and approved by the City's waste purveyor Recology CleanScapes on March 9, 2015. Additional review of waste facilities will occur with the Site Work and Building permits to ensure that the waste enclosure is not over or undersized.

Public Comment Summary and Staff Responses:

Public notice was provided as required. A Notice of Application was mailed out to surrounding property owners on April 15, 2015. No comments have been received as of the date of this Notice of Decision.

VII. Attachments

1. Central Issaquah Development and Design Standards (CIDDS) review spreadsheet
2. Gilman Point – Construction Conditions
3. Permit Application – Received March 10, 2015
4. SEPA Mitigated Determination of Non-Significance – July 9, 2015
5. Notice of Application – April 15, 2015
6. ASDP Plan Drawings – Received March 10, 2015
7. Revised Elevations for Storage Facility – Received August 14, 2015
8. Administrative Adjustment of Standards – CIDDS 6.2.B – New Circulation Facilities
9. Administrative Adjustment of Standards – CIDDS 11.3.F – Streetwall

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
Note: This checklist should be read in conjunction with the staff report for the Gilman Point project, ASDP15-00002							
Ch. 1 PURPOSE AND APPLICABILITY							
1.1.A-C	Purpose, Authority and Applicability					The purpose of the standards is to implement the Central Issaquah Plan. Development projects are expected to meet the development and design standards of the Central Issaquah Plan to the greatest extent practical and feasible. This review spreadsheet is a tool that has been established in order to review each of the required project development and design standards against the applicable criteria. The purpose of the Administrative Development Permit review is to examine projects from general compliance with the standards. Detailed review of project elements and details is reserved for the Construction permit review and it is expected that project design details will be further modified with during the review of the Construction permits in order to achieve the standards and guidelines set forth by the Central Issaquah Development and Design Standards (CIDDS).	Condition 1: Unless expressly identified, approval of this ASDP application does not modify any Central Issaquah Development and Design Standards (CIDDS) or City standards which are in conflict with elements of the ASDP plan or application. Modification of the standards or guidelines requires an explicit approval in the Notice of Decision for this application or a separate Modification as allowed under the applicable section of the CIDDS.
1.1.D	Interpretations						
1.1.E	Adjustments					There are two Administrative Adjustment necessary as part of this application. See Sections 6.2.B-C, 6.3, and 11.3.F, and Attachments 8 and 9.	
Ch. 2 DEFINITIONS							
2.0							
Ch. 3 PROCEDURES							
3.2	Levels of Review					Level 2 Major Administrative Site Development Permit – Total project sq.ft. is < 100,000sf. see 4.3.A)	
Ch. 4 ZONING, USES							

ATTACHMENT 1 ASDP15-00002 – Gilman Point: CIDDS review

August 27, 2015

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
4.2 table	Intent of Zoning Districts		X			Mixed Use Zone Intent: provide mixed use neighborhoods with Class A office buildings, retail, medium density residential development. The proposed uses fall within the allowed uses for the Mixed Use zone.	
4.3.A table	Levels of Review		X			Level 2 - ≥45,000 - <100,000sf Gross Floor Area (GFA)	
4.3.B table	Permitted Uses		X			Both uses are permitted in the Mixed Use Zone. Tire shop: Specifically permitted under Automotive – Maintenance/Service Shops Storage facility: Specifically permitted under Storage – Self	
	Footnotes		X			The tire facility complies with Footnote 15 which requires vehicle maintenance and service facilities to have ground floor storefront orientation to sidewalk and street and parking and outdoor storage located at the rear of the site and screened from view of Circulation Facilities.	
4.4 table	FAR		X		X	There is no minimum Floor Area Ratio (FAR) for this zone. Based upon submitted GFA, the proposed FAR is ±1.33 which exceeds the Base FAR of 1.25. By exceeding Base FAR, additional amenities are required per the Density Bonus Program. See Section 5.0 for additional discussion. Reconciliation of amenities or payment in lieu of will be required prior to the issuance of building permits.	
	Height		X		X	Proposed: Tire shop - ±20ft Storage - 54ft. Allowed Base height maximum: 48 feet Per Footnote 3 of Table 4.4 – District Standards Summary Table, the storage facility exceeds the	Condition 2: The retail portion of the storage facility must have a ceiling height of at least 15 feet. This will be verified with the Building permit.

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
						allowed base height maximum of 48 feet for the Mixed Use zone. The maximum base height may be extended from 48 feet to 54 feet provided that the floor height (ceiling height) of the first floor of the retail space is a minimum of 15 feet in height. As the storage facility exceeds the 48 foot base height limit, the retail portion of the building and any portions of the building that are raised in order to accommodate under-building paking must have a ceiling height of at least 15 feet.	
	Setbacks		X		X	0' – side, rear. Complies.	
	Build-to-line		X		X	0-10'; Building shown at 10'. Complies.	
	Impervious		X			Gross Site Area: 82,134 sf Proposed Impervious: 61,858 sf Max impervious allowed: 90 percent Total impervious proposed: 75.3 percent	
	Footnotes	X				Footnotes 3 and 5 apply to this application. The plans, as shown, comply with the applicable Footnotes.	

Chap 05 DENSITY BONUS

5.4	Public Benefit Requirements				X	<p>Because the overall project exceeds the Base FAR of 1.25, the applicant must use the Density Bonus Program. The Gross Floor Area and Total Developable Area are provided on Sheet A0.1 of the plan set (Attachment 6).</p> <p>For the storage facility, the application does not state what the actual square footage of the floor area above the 48 foot Base Height limit is, and therefore, it is unknown at this time what factor the Density Bonus payment will be based upon (either Gross Floor Area of the entire project, or, the total floor area of the storage facility for any floors located above the 48 foot Base Height limit). The</p>	Condition 3: The Density Bonus Program shall apply to all Gross Floor Area of the project that exceeds Base FAR.
-----	-----------------------------	--	--	--	---	---	--

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
						<p>Density Bonus payment will apply to the greater of the two totals.</p> <p>Final determination of the applicable Density Bonus payment will be determined with the Building permit and payment of Density Bonus Program fees shall be collected prior to Building permit issuance.</p>	
5.5	Public Benefit Req. Affordable Housing				X	See Section 5.4.	
5.6	Public Benefit Req. Open Space				X	See Section 5.4.	
5.7	Density Bonus Fee				X	See Section 5.4.	
Chap 06 CIRCULATION							
6.2.A	Block length		X		X	<p>An additional trail connection shall be provided to connect the East Lake Sammamish Trail to the interior sidewalk near the entrance to the storage facility. This will reduce the block length as experienced by the pedestrian to acceptable an acceptable length of approximately 200 feet. Additionally, Interstate 90 precludes any future connection points to the north.</p>	Condition 4: A trail connection shall be provided near the primary entrance of the storage facility to the East Lake Sammamish Trail. The applicant shall work with the City and King County to obtain the necessary approvals to construct the trail.
6.2.B-C	Existing & New Circ Facilities – Fig.6A; Priorities			X	X	<p><u>Non-Motorized Facilities</u> Existing: East Lake Sammamish Trail (Multi-purpose trail), Sidewalk on Gilman Boulevard</p> <p>Proposed: Sidewalk along west side of new internal street, additional pedestrian connection from new sidewalk to East Lake Sammamish Trail</p> <p>Internal walks: There are several areas where internal walks are too narrow (less than 5 feet in width).</p> <p><u>Auto-Inclusive Facilities</u> The internal street design does not conform to any adopted Street Standard. The street shows a 24-</p>	Condition 5: All pedestrian circulation facilities shall be a minimum of 5 feet in width, clear of intrusions (bollards, handrails, railings, etc.). For the west sidewalk of the internal street, the applicant shall explore options that would allow the west sidewalk to be widened.

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
						<p>28' wide travel lane with a sidewalk and planter strip on the west side of the street. Because no sidewalk is provided along the east side (parking lot side) an Administrative Adjustment of Standards is required and has been granted – see Attachment 8. The new street is not identified on the Auto Inclusive Circulation Facility Map (Figure 6A) and thus shall be designed in accordance with the City of Issaquah Street Standards. Please refer to Section 12.3.A for discussion on the design of the new internal street.</p> <p><u>Gilman Boulevard Alterations</u> Additional modifications to Gilman Boulevard will be necessary based upon findings of the SEPA MDNS (Attachment 4) and the Traffic Impact Study dated March 9, 2015 by Transportation Solutions Inc.</p> <p>Changes to Gilman Boulevard are summarized below:</p> <ul style="list-style-type: none"> - Driveway access from Gilman Boulevard shall be restricted to right-in/right-out turning movements only. This will be achieved through the use of C-curbs which must be installed on NW Gilman Boulevard. - Access to the site by trucks and trailers 35 feet and longer shall be restricted to the hours between 10:00pm and 6:00am in order to limit the impact of traffic flow on NW Gilman Boulevard. <p>Please refer to SEPA MDNS, (Attachment 4) for comprehensive discussion on this matter and Conditions of Approval.</p>	

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
						<p><u>New Internal Street:</u> No Auto Inclusive Facilities are proposed to be designed per the Standards in Chapter 6.0. As such, facilities should be designed using the priorities identified in this section. The proposed internal street is not identified in the Auto-Inclusive Circulation Facility Map – Figure 6A. As such, the street shall be designed using the Priorities described within this Section.</p> <p>The internal street will utilize 10 foot wide lane widths which are consistent with the pedestrian prioritization goals of the Central Issaquah Plan. A planter strip and sidewalk will be provided on the west side of the internal street. No sidewalk will be provided along the east side of the street. As the sidewalk will be eliminated on the east side of the street, the applicant shall explore widening the west sidewalk if possible. An AAS will be required to approve this configuration. See Attachment 8, Sections 6.2.B-C, and 6.3.</p>	
6.2.D	Overpass/ Tunnel	X					
6.2.E	Add'l facilities		X		X	If permitted by King County Parks, a new trail connection is required to provide an additional connection from the East Lake Sammamish Trail to the storage facility. See Condition 4.	
6.2.F	Non-motorized routes		X			A shared use route, including bicycle infrastructure, is already provided via the East Lake Sammamish Regional Trail. Because of these existing facilities and because the street configuration and the limited amount of frontage onto NW Gilman Boulevard, additional Nonmotorized facilities (beyond those already present) will not be required with this proposal.	
6.2.G	No cul-de-sacs	X					
6.2.H&I	R.O.W. Dedication	X					

ATTACHMENT 1 ASDP15-00002 – Gilman Point: CIDDS review

August 27, 2015

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
6.2 J	In-lieu-of Payments instead of dedication of R.O.W.	X					
6.3	AAS		X		X	An AAS was granted in order to eliminate the sidewalk on the east side of the internal street. See Section 6.2.B-C and Attachment 8.	
6.4A	Circulation Facilities: Shared Use Route		X			There is an existing Shared Use Route provided by way of the East Lake Sammamish Trail.	
6.4B	Secondary Through Block Passage		X			A new trail connection is required to provide an additional connection from the East Lake Sammamish Trail to the storage facility. See Condition 4.	
6.4C	Primary Through Block Passage	X				None proposed for this project.	
6.4D	Pedestrian Priority Street	X				None proposed for this project.	
6.4E	Neighborhood Street	X				None proposed for this project.	
6.4F	Core Streets	X				None proposed for this project.	
6.4G	Boulevards		X			NW Gilman Boulevard is identified as a boulevard on the Auto-Inclusive Circulation Facilities Map – Figure 6A. Because of the considerable setback distance between NW Gilman Boulevard and the site entrance and building location; and, the limited amount of property frontage onto the street (appx. 80 feet), only and minor changes are proposed to the existing Gilman ROW, including signage to limit turning movements to right-in/right-out only; and, the addition of a C-curb to prevent left hand exit maneuvers. The existing sidewalk and street trees will not be changed with this project.	
6.4H	Parkways	X	X			None proposed for this project.	
6.4I	Alleys	X				None proposed for this project.	
6.4J	Fire Turnaround		X		X	The site plan has been reviewed by Eastside Fire and Rescue to ensure adequate fire truck access is provided. Additional review and approval of emergency access will occur with the construction permits to ensure that ER&R personnel, ladder	

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
						trucks can access and maneuver through the site.	
Chap 12 CIRCULATION DESIGN							
12.2.A	Multiple Routes		X		X	Because of the irregular geometry of the property, vehicular access to the property occurs only at the front of the property at the intersection with the NW Gilman Boulevard right-of-way. Non-motorized access to the property occurs via the East Lake Sammamish Trail and the sidewalk along NW Gilman Boulevard. A new connection linking the East Lake Sammamish Trail and the storage facility will also be provided to limit the block length and provide additional access for bikes and pedestrians.	
12.2.B	Universal Design		X		X	The project shall comply with the International Building Code and the City's Streets Standards. This will be reviewed with the Building and Site Work permits.	
12.2.C	Visual Cues				X	Additional information will be required with the construction permits to evaluate whether the circulation facilities have adequate visual cues. Per Condition 17, the site's entrance will be treated with surficial and/or architectural treatments in order to convey to drivers that they are entering a pedestrian priority space. Per Condition 10, where the internal street and parking lot drive aisles must exceed minimum widths, treatments shall be employed to the drive surface, planter islands, etc., in order to reduce the perceived width of the drive lane.	
12.2.D	Public & Private Facilities		X			The internal street will be privately owned and maintained and shall be designed according to the CIDDS circulation facilities standards.	
12.2.E	Multi-functionality		X			Recreation, passive use and informal gathering opportunities are provided at the site entrance	

ATTACHMENT 1 ASDP15-00002 – Gilman Point: CIDDS review

August 27, 2015

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
						and along the East Lake Sammamish Trail.	
12.3	Motorized Facility						
12.3.A	Motorized Facility Design			X	X	<p>Vehicular access to the site will be provided by way of a new internal street that connects to NW Gilman Boulevard at the existing location. The internal street shall be designed to incorporate elements of pedestrian oriented design per the CIDDS.</p> <p>The internal street will be constructed with 10 foot wide travel lanes and planter strips. Sidewalk will be provided on the west side only. This is a deviation from the standard as pedestrian facilities are only proposed on one side of the street. All other street design standards shall apply.</p> <p>Since the sidewalk is being eliminated on one side, the applicant shall explore the feasibility of widening the western sidewalk to improve the pedestrian facilities on that side of the street. An Administrative Adjustment of Standards is required and was granted subject to ASDP Condition 5. See Section 6.2.B-C, 6.3 and Attachment 8.</p>	
12.3.B	Minimize Pavement		X		X	Pavement for the site will be reduced to the minimum amount necessary while still providing for sufficient access by delivery vehicles and emergency access.	
12.3.C	Pedestrian Safety Measures				X	The plans identify two crosswalks - one located at the entrance to the site and one located between the parking lot and the service side of the tire facility. Per Condition 18, these crosswalks shall be constructed with colored, scored and/or textured concrete. Additionally, crosswalks shall apply with applicable ADA standards.	

ATTACHMENT 1 ASDP15-00002 – Gilman Point: CIDDS review

August 27, 2015

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
12.3.D	Minimize Driveways		X			In order to provide sufficient turning movements for fire and delivery trucks, some driveways are shown wider than typical. Where this occurs, surface treatments shall be incorporated which serve to reduce the perceived width of the driveways for private automobiles. For example, at the east end of the site (Pogacha side), asphalt may be used for the first 20-22 feet of driveway to accommodate passenger vehicles. The additional driveway area constructed beyond the asphalt section shall be constructed with textured, scored and/or colored concrete or other material which differentiates it from the primary drive aisle. See Section 8.18.B.4 and Condition 10.	
12.3.E	Street Intersection		X		X	A wide band of decorative concrete will be constructed at the entrance to the project in order to highlight the intersection and to signify pedestrian priority of the intersection.	
12.4	Nonmotorized Facility Standards						
12.4.A	General		X			Complies. The existing East Lake Sammamish Trail and newly constructed sidewalk will accommodate a variety of users including joggers, cyclists and other forms of Nonmotorized users.	
12.4.B	Pedestrian Friendliness		X			Street trees and planter strips are provided for the internal street to provide a comfortable and safe route for pedestrians.	
12.4.C	Wider Sidewalks		X			Several internal sidewalks have widths that do not support a pedestrian oriented design. Sidewalks shall be a minimum of 5 feet in width. See Section 6.2.B-C and Condition 5.	
12.4.D	Pedestrian Routes		X			Sidewalks provided are coordinated with the circulation facility type and provide connections to NW Gilman Boulevard and the East Lake Sammamish Trail. Overall, pedestrian connections are adequate and provide continuous access to off-site locations.	

ATTACHMENT 1 ASDP15-00002 – Gilman Point: CIDDS review

August 27, 2015

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
12.4.E	Pedestrian Crossings		X		X	See discussion above – Section 12.3.C.	
12.4.F	Transit Support		X			King County/Metro Route 200 bus stops are provided within 1000 feet of the project site.	
12.4.G	Tree Wells				X	The landscape plans do not show where tree wells and/or grates will be utilized. This will be evaluated with the Landscape permit and the appropriate mechanism will be utilized to suit the character of the development.	
12.4.H	Bike Circulation		X			Bike circulation is adequate and provided via bike lanes on both sides of NW Gilman Boulevard and on the East Lake Sammamish Trail.	
12.4.I	Bike Rails	X					
12.5	Connectivity and Block Structure						
12.5.A	Ped Connections					See discussion for 6.2.A	
12.5.B	Connections to Surrounding Circ. Fac.		X			The site is generally well connected to adjacent properties and circulation facilities given limitations created by Interstate 90 and the East Lake Sammamish Trail. No access to the adjacent property (Pogacha) is proposed with this application, however. Immediate access to Nonmotorized facilities including the East Lake Sammamish Trail and the sidewalk along NW Gilman Boulevard, a new point of access will be created via a new trail connection to the East Lake Sammamish Trail.	
12.5.C	Circ. Facility Types					See comments for 6.2 and 6.4	
12.5.D	Private Street Design		X			See comments for 12.2.D	
12.5.E	Pedestrian Curbs		X		X	Complies. Additional review will occur during review of the Construction permits.	
12.5.F	Walkway Separation		X			Complies.	
12.5.G	ROW Dedication	X					
12.5.H	Maintenance by private property		X		X	Currently, the adjacent landscape strip on NW Gilman Boulevard is maintained by the City's Parks and Recreation Department, though maintenance responsibilities may change in the future.	

ATTACHMENT 1 ASDP15-00002 – Gilman Point: CIDDS review

August 27, 2015

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
12.6.	Landscaping of Circulation Elements						
12.6.A	Planting areas		X		X	Complies.	
12.6.B	Planter width and design per Sec. 6.4		X		X	A 5-foot tree planter strip is provided for the internal street – see Sheet C2.0. This will be verified with the Site Work permit.	
12.6.C	Planter strips and tree wells sized appropriately		X		X	Complies.	
12.6.D	Integrate landscape to context: circulation facilities, buildings, public space, setbacks		X		X	Landscape is integrated into the circulation facilities, including the provision of trees in the parking areas that serve the tire and storage facilities. The full perimeter of the site has landscaped areas. Additional wetland mitigation planting, including approximately 11 evergreen trees, is also provided at the back of the site.	
12.6.E	Landscape w/in and adjacent to circulation facilities – safety and visibility		X		X	Generally complies but additional detailed review at construction permit will be required to ensure full compliance.	
12.6.F	Pruning	X					
12.6.G	Circulation corridor plantings				X		
12.6.H	Hardy landscapes next to parallel parking	X				No parallel parking is proposed.	
12.6.I	Incorporate annual and colorful plantings		X		X	A variety of colors and textures of planting materials are proposed; however, no annuals are provided. (See sheet L1.2).	
12.6.J	Use Landscape to moderate building scale and create pedestrian scale		X			Landscaping is provided around the perimeter of both buildings to moderate their scale.	
12.6.K	Green streets	X				Green streets are not proposed.	
12.6.A-L	Landscaping of Circulation elements		X		X	The proposed landscaping generally meets the standards. Final compliance will be during review of the construction permits.	
Chap 07 COMMUNITY SPACES							
7.2.B1-4 & Fig. 7A	Green Necklace		X			The project meets the intent for a Green Necklace via East Lake Sammamish Trail and through enhancement of the on-site wetland.	

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
7.3	Residential Community Spaces	X				Section 7.3 pertains to required Residential Community Spaces and is not applicable to this project.	
7.4	Significant Community Space per Fig.7B	X				Section 7.4 pertains to required Significant Community Space and is not a required element for this project.	
7.4A	General Provisions	X					
7.5	Parks and Recreation Mitigations & Credits	X				The project is not eligible to receive Parks and Recreation Mitigations or Credits.	
Chap 13 COMMUNITY SPACES							
13.2.A	Variety	X				No new community spaces are required or proposed. The existing East Lake Sammamish Trail and associated pedestrian elements provided at the trail head intersection (benches, kiosk, pet waste pickup station, trash receptacles, etc.) serve to provide many of the functions of a community space.	
13.2.B	Integration	X					
13.2.B.1	Enclosure	X					
13.2.B.2	Integrate with nonmotorized circulation;	X					
13.2.B.3	Location	X					
13.2.B.4	Scale	X					
13.2.B.5	View corridors, connect with nature	X					
13.2.B.6	Appeal to the senses; variety of experiences	X					
13.2.B.7	Amenities to define community space: comfortable, functional, safe	X					
13.2.B.7	Provide water & electrical outlets	X					
13.2.B.8 and B.9	Weather protection	X					
13.2.B.10	Variety of seating						
13.2.B.11	Temporary structures: food trucks, kiosks	X					
13.2.B.12	Wayfinding to Green Necklace	X					
13.3	Connect with Nature	X					

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
13.4	Playscape	X					
13.5.A-H	Plaza				X	To be evaluated for compliance with review of Construction permits.	
13.6.A-H	Community Garden	X					
13.7.A-D	Pet Amenity	X				A pet waste pickup station is provided at the East Lake Sammamish Trail trailhead near the entrance to the site.	

Chap 08 PARKING

8.4	CTR/TMAP	X				Does not meet threshold for requirement.	
8.5	Use of Req'd Pkg		X			No uses proposed for purposes within the parking lot other than vehicle parking.	
8.8-9	Computation, Unspecified uses					<p><u>Tire Center</u> Min: 2 spaces per 1000NSF Max: 2.5 spaces per service bay (18 stalls) Stalls Required: 18</p> <p><u>Storage</u> Min: 2 spaces per 1000/sf (89,200sf = 178 stalls) Max: 1 per 20 storage units (617 units = 31 stalls) Stalls Required: 31 stalls</p> <p>Total stalls required (site wide): 49 Total stalls provided (site wide): 43-44 stalls</p> <p>A letter was provided with the application by Westcoast Self Storage to reduce the storage facility parking to 0.16 stalls/1000 GSF per published ITE ratio. The letter cites other stores for which Westcoast Self Storage operates along the west coast and state that indicates that they normally plan for 10 stalls for a store of this size. By comparison, Westcoast's 103,000sf store on Rainier Ave provides 14 stalls.</p> <p>As the parking for the entire project is shy of the</p>	<p>Condition 6: Prior to submittal of any Building permits, provide a Transportation Demand Study that shows that the amount of parking provided for the project is adequate based comparable projects in the Pacific Northwest. The Transportation Demand Study shall be performed by a parking professional that is jointly selected by the Applicant and the City.</p> <p>Condition 7: Prior to issuance of any Building permits, the Applicant shall provide a parking agreement, acceptable to the Director, which demonstrates that sufficient parking is available (in perpetuity) for both of the proposed uses. The agreement shall be recorded on title and filed with the King County Recorder's Office.</p>

ATTACHMENT 1 ASDP15-00002 – Gilman Point: CIDDS review

August 27, 2015

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
						<p>required minimum by approximately 5-6 stalls, a reduction will be necessary per the criteria of Section 8.13.8. See this section below for additional discussion and condition of approval.</p> <p>The Applicant has indicated that a private agreement between the two uses will be recorded which grants shared use of all parking stalls within the project. This agreement shall be recorded on title and filed with the King County Recorder's Office.</p>	
Table 8.10-1	Table of Vehicular Parking Spaces		X			<p>44 Stalls provided See information in Section 8.8.9 above.</p>	
8.11	Bicycle parking		X			<p>Applicant seeks to reduce bike parking to two stalls per 8.11.B. Based upon the proposed uses, the Director grants this request subject to Condition 8 to require separate, covered bicycle parking for each use independently.</p> <p>Bicycle parking quantity and location to be determined with construction permits. Bike racks shall be provided with both facilities.</p>	<p>Condition 8: At least 2 bicycle parking spaces shall be provided for each use (4 total at a minimum). The bicycle parking shall be covered and shall be provided within a close proximity to the primary entrance to each building. This will be reviewed with the Site Work and Landscape permits.</p>
8.12	Motorcycle parking		X			<p>None provided. Motorcycle parking is specifically exempted per Section 8.12.A.3 for storage and automobile facilities.</p>	
8.13	Tools & Flexibility						
8.13.B.1	Transit Access	X					
8.13.B.2	Enhanced Pedestrian Routes to Issaquah Transit Center	X					
8.13.B.3	Small Business Waiver	X					
8.13.B.4	Retail in MXD Bldg.	X					
8.13.B.5	On-Street Parking Credit	X					
8.13.B.6	Off-site Parking	X					
8.13.B.7	Shared Parking	X				<p>The applicant has indicated that an agreement between both businesses will be executed in order to allow all parking within the parking lot to be</p>	

ATTACHMENT 1 ASDP15-00002 – Gilman Point: CIDDS review

August 27, 2015

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
						utilized by both businesses which will be separate commercial condominiums.	
8.13.B.8	TDM study	X				The applicant provided a letter by Westcoast Self Storage to reduce Storage parking to 0.16 stalls/1000 GSF per published Institute of Transportation Engineers (ITE) ratio. The letter cites other stores along west coast and state that they would normally plan for 10 stalls for a store of this size. Their 103,000sf store on Rainier Ave provides 14 stalls. Additional documentation will be required to substantiate the TDM. See Section 8.8-9 and Condition 6..	
8.13.B.9	Tandem Parking	X				No tandem parking is proposed.	
8.13.B.10	Delay and Reserved Parking	X					
8.13.B.11	Electric Vehicle	X				Not proposed.	
8.13.B.12	Shuttle	X					
8.13.B.13	Valet	X					
8.13.B.14	Other Parking Measures	X					
8.13.B.15	Administrative Adjustment of Parking Standards	X					
8.14	Parking District	X					
8.15	Barrier-free				X	To be reviewed with site work and building permit review.	
8.16	Loading spaces						
Table 8.16-1	Required Loading Spaces		X		X	Complies. Large loading area provided at north end of Tire Center. Three large loading stalls provided in front of storage facility to accommodate U-haul style trucks. Tire – 1 required Storage – 3 required	
8.17	Drive Thru Stacking Spaces	X					
8.18	Structured & Surface Parking Standards						
8.18.B.1	General Design & Construction Standards		X		X	Complies. Some parking stalls are shown in excess of the allowed parking stall dimensions. All stalls shall meet the adopted stall size dimensions	Condition 9: Parking stall dimensions shall be the minimum necessary per the requirements. For

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
						per Section 8.20-1 – see Condition 9. Final stall and driveways dimensions to be confirmed with Site Work permit.	standard stalls, the maximum dimension shall be 9 x 18.5 feet. If compact stalls are used, they shall be 8 x 16 feet. Where feasible, use of the 2 foot overhang into planted areas is encouraged.
8.18.B.2	Mix of stall sizes		X			All stalls are proposed to be Standard size.	
8.18.B.3	Location for compact & micro stalls	X				No compact or micro parking stalls are proposed.	
8.18.B.4	Drive aisle width			X	X	In general, drive aisle widths throughout the parking lot are consistent with the standards. At the east side of the side (Pogacha side) the driveway area exceeds the allowed width in order to provide access fire truck access and access by larger delivery vehicles. Where the drive aisles exceed the allowed width (20-24 feet), surficial treatments (e.g. concrete) shall be used to reduce the visual presence of the drive aisle and to define the drive aisle for passenger vehicles.	Condition 10: Where parking lot drive aisle widths must be wider than the standardized dimension in order to provide emergency and large vehicle access, special techniques, e.g. concrete, shall be used where the drive aisle exceeds the allowed width (e.g., the area to the east of the tire facility and adjacent to the loading area).
8.18.B.5	Surface Lot Materials				X	To be reviewed at site work permit.	
8.18.B.6	Marking		X		X	No Parking – Fire Lane striping is shown on Sheet C1.0 for use in several locations within the parking lot. Striping (paint) should not be used except at the discretion of the Fire Marshal. Signage shall be used to indicate No Parking – Fire Lane area.	Condition 11: “No Parking – Fire Lane” zones shall be indicated by the use of signs. Painted curbs shall not be used unless expressly directed by the Fire Marshal.
8.18.B.7	Driveways	X				No driveway (ramps) are proposed.	
8.18.B.8	Wheelstops	X				Not proposed.	
8.18.B.9	Stall Length in Surface Pkg	X		X		Some surface stalls are too long. All stalls shall conform to dimension standards per Section 8.18.B.4 and Condition 9.	
8.18.B.10	Inner circulation		X				
8.19	Administrative Adjustmt. Of Standards	X					
8.20	Stall/Aisle Dimension					See 8.18.B.4 for discussion pertaining to aisle dimensions. See 8.20.A below for discussion	

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
						pertaining to stall size.	
	A. Stall sizes		X			Standard stall sizes are shown at 9 x 20 feet. Standard parking stalls shall be reduced to the allowed standard dimension of 9 x 18.5 feet. See Section 8.18.B.1 and Condition 9.	
	B. Bumper Overhang	X	X			No bumper overhang is proposed though its use is encouraged to reduce the amount of driveway paving.	
	C. Drive aisles not used for backing from parking spaces		X		X	See 8.18.B.4 above.	

Chap 15 PARKING DESIGN

15.2	General Standards						
15.2.A	Location		X		X	Complies.	
15.2.B	Minimize Parking Appearance		X		X	Complies.	
15.2.C	Driveway Access		X			Complies.	
15.2.D	Pedestrian Priority		X			Complies.	
15.2.E	Pedestrian Friendly					Complies.	
15.2.F	Multi-Functional	X					
15.2.G	Natural Ventilation & Lighting		X			Complies.	
15.3	Structured Parking	X				No structured parking is proposed.	
15.4	Surface Parking						
15.4.A	Internal Circulation to replicate character of public street and connect to surrounding vehicular circulation		X			Complies.	
15.4.B	Break up Large Lots	X				Nothing that would be considered a 'large lot' is proposed as part of this project.	
15.4.C	Pedestrian Connections		X			Complies.	
15.4.D	Buffer Pedestrian Routes		X			Complies. Pedestrian routes are buffered from moving traffic by tree planters.	
15.4.E	Shade Pavement		X			Complies. A minimum of one tree is provided for every 6 parking spaces; however, not all surface parking areas are provided with trees as much of the parking lot is covered by the building overhang of the storage facility.	
15.4.F	Landscape Screening		X			Landscape screening, in the form of trees and shrubs, are proposed along the internal street and	

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
						within planter islands. Landscaping is also provided around the perimeter of the property.	
15.4.G	Sustainability		X		X	Details not provided at this time. Consider using pervious pavers for the site entry adjacent to the tire facility and throughout the parking lot. To be reviewed at construction permit phase.	
15.5	Bike Parking						
15.5.A	General		X		X		
15.5.B	Location – w/in 50 ft. of building entrance; covered		X			Uncovered bike parking for the tire facility is shown near the primary building entrance. No bike parking is shown for the storage facility but is required. See Section 8.11 and Condition 8.	
15.5.C	Secured parking/lockers	X					
15.5.D	Accessible racks		X		X	To be reviewed at construction permit phase.	
15.5.E	Decorative bike parking				X	This is optional, but may help with creating a sense of place and serving as intuitive wayfinding elements.	
15.5.F	Anchor bike racks to ground or wall				X	To be reviewed at construction permit phase.	
15.5.F	Supporting facilities: showers, maintenance equipment	X					
Chap 09 SIGNS							
9.0					X	No information pertaining to signage is provided. Signage will be reviewed with the Sign permits.	
Chap 10 LANDSCAPE							
10.2.A.1	Landscape Plan				X		
10.2.A.2	Tree Plan		X		X	Tree plan to be provided with Landscape permit. See Section 10.10-10.14 for discussion pertaining to Minimum Tree Density, Tree Retention and Tree Replacement.	
10.2.A.3	Irrigation Plan				X		
10.2.A.4	Planting and Irrigation Details				X		
10.2.A.5	Landscape Plan Revision				X		

ATTACHMENT 1 ASDP15-00002 – Gilman Point: CIDDS review

August 27, 2015

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
10.2.A.6	Landscape to be Maintained by Applicant				X		
10.2.A.7	Certificate of Occupancy				X		
10.3	Circulation Elements & Community Space						
10.3.A.1	Trees under utility lines	X				No trees are proposed under utility lines.	
10.3.A.2	Required for R.O.W. and other circulation facilities				X	Tree type, caliper, pit size, etc. to be reviewed with Landscape permit.	
10.3.A.3	30-feet o.c.				X	Street trees are to be planted at 30-feet on-center.	
10.3.A.4	Root barrier				X	Details to be reviewed with Landscape Permit.	
10.3.A.5	Tree wells 4 ft. by 6 ft. min.				X	No tree wells are proposed with ASDP.	
10.3.A.6	Raised planters alternative	X					
10.3.A.7	Removal of Plants and Trees in ROW	X				No removal of plants or trees is proposed in the Right-of-Way.	
10.3.B	Community Spaces	X				There are no required Community Spaces.	
10.4	Parking Areas						
10.4.A.1	Small Parking Lots	X				There are no small parking lots on this project.	
10.4.A.2	Interior Landscape						
10.4.A.2.a	1 tree/6 stalls		X				
10.4.A.2.b	Min.10% of parking lot should be landscaped			X	X	Information pertaining to the square footage quantity of provided parking lot landscape is not provided. Based upon initial review, it appears that sufficient landscape area is provided (including any adjustments) in order to meet the 10 percent requirement. This will be verified with the Landscape and Site Work permits.	Condition 12: The total of all interior landscaping for the parking lot, excluding Edge Landscaping, shall be equal to or greater than 10 percent of the total parking lot area. The portion of parking lot that is underneath the building overhang of the storage facility shall not be counted toward the overall parking lot area calculation.
10.4.A.2.c	100% coverage of evergreen in 3 years				X	To be reviewed at construction permit phase.	
10.4.A.2.d	Landscape area reqd. at end of aisles				X	To be reviewed at construction permit phase.	
10.4.A.2.e	Clustering permitted				X	To be reviewed at construction permit phase.	
10.4.A.2.f	Landscape island width		X		X	The landscaped islands shown are dimensioned at 5 feet in width. This will be verified with the Landscape and Site Work permits.	

ATTACHMENT 1 ASDP15-00002 – Gilman Point: CIDDS review

August 27, 2015

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
10.4.A.3	Edge Landscape		X		X	Edge landscaping is shown on Sheet L1.2. Plant details and specifications will be verified with the Landscape permit to ensure that the desired edge landscaping treatment is achieved.	
10.4.A.4	Alternatives to Parking Lot Landscaping	X			X	No parking lot landscaping alternatives are proposed. They may be considered with future construction permits at the Applicant's discretion.	
10.4.B	Parking Structures	X					
10.7	Vegetation adjacent to Critical Areas				X	Critical areas plantings will be reviewed with the Landscape permit to ensure their suitability for the wetland critical areas.	
10.8	Fences, Waste Enclosures, Mechanical Equipment						
10.8.A	Fencing – planting reqd. on side with greatest public use	X					
10.8.B	Hedges – min. 1 ft. above area to be screened				X		
10.8.C	Waste Enclosures: min. 6 ft. and 1 ft. above container; 100% sight obscuring; softened with landscaping				X		
10.8.D	Mechanical Equipment				X	The location of ground-mounted mechanical and utility equipment is not shown on the plans. This will be reviewed at construction permit phase.	Condition 13: All utility vaults, cabinets, switchgear, pull boxes, meters, equipment, and appurtenances are assumed to be shown on the Permit submittal. Anything not shown on the Permit submittal (location, relative height, presence above ground) is assumed to be located within the buildings. Any revisions or additions to what the Permit has shown and approved outside of the buildings requires a modification to the Permit, except for fire hydrants.
10.9	Blank & Retaining Walls						
10.9.A	Blank Walls					See Section 14.2.B .	
10.9.B	Retaining Walls above 4 ft.	X			X	No retaining walls above 4 ft. in height are proposed. Rockeries are shown adjacent to the in-	Condition 14: Any retaining walls shall not be constructed as rock-

ATTACHMENT 1 ASDP15-00002 – Gilman Point: CIDDS review

August 27, 2015

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
						ternal sidewalk and behind the tire facility. Rockeries are specifically prohibited in the CIDDS. Where retaining walls are necessary, they shall not be designed as rockeries.	eries and shall be block or other allowed materials. This will be reviewed with the Site Work and Landscape permits.
10.10	Min. Tree Density			X	X	<p>Min. tree density required: 4 trees/5000 s.f. Based on a total developable area of 74,869 s.f., the total number of trees required is 60.</p> <p>Based upon a count of the trees provided on Sheet L1.02, there are approximately 45 replacement trees provided with redevelopment.</p> <p>If it is not possible to locate all of the replacement trees on-site, an alternate (off-site) location shall be determined. If a suitable location relocation site is not available, payment shall be made to the City Tree Fund. See CIDDS Section 10.14.A.C for specific details concerning tree replacement and/or payment in-lieu of.</p> <p>The minimum tree density will be further reviewed at construction permit review and final quantity and location will be ensured at landscape inspection. See Sections 10.13, 10.14 and Condition 15.</p>	
10.11	Tree Removal (vacant/developed properties)	X				Tree removal permit does not apply to redevelopment of a site. Instead, this project is required to meet the tree retention and tree density requirements. See Sections 10.10 and 10.13.	
10.12	Tree Removal	X				This section is applicable to existing developments. It does not apply to new development.	
10.13	Tree Retention			X	X	Sheet L1.1 shows that 12 significant trees are present on the site and have an aggregate diameter (dbh) of 257 inches. The requirement states that 25 percent of the total dbh of trees must be retained (64" dbh).	

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
						<p>Based upon the location of the existing trees, all of the trees are proposed to be removed and therefore Section 10.13.B (Modification of Tree Retention Requirements) is applicable. The proposal to remove the tree is consistent with the criteria for a reduction of the tree retention requirements as follows:</p> <ol style="list-style-type: none"> 1. The modification is consistent with the purpose and intent of the Landscape chapter and the Central Issaquah Plan goals and policies. 2. The modification does not retain the retention of groupings of smaller trees as such groupings do not exist on the property. As a result, compliance with this requirement is not possible for this property. A large grouping 11 replacement evergreen trees will be provided to create the grouping of trees that is intended by this criteria. 3. The modification is necessary because the shape and location of the subject property may jeopardize reasonable use of the property and reasonable alternatives do not exist. 4. The modification is necessary to allow for site features including: buildings, utilities, storm drainage, circulation facilities, etc. 5. Not applicable. 6. The applicant will be required to provide trees on or off-site or pay a fee-in-lieu of to the City Tree Fund per Sections 10.14.C-D of the CIDDS. See Section 10.14 and Condition 15. 	
10.14	Replacement Trees				X	<p>Replacement trees are required for this project up to the minimum tree density required for the lot of 4 trees per 5,000 square feet of developable area. Based on a total developable area of 74,869 s.f.,</p>	<p>Condition 15: In order to mitigate for the reduction in tree retention; and, to meet the minimum tree density for the lot, replacement trees must be provided at a rate</p>

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
						<p>the total number of replacement trees required is 60.</p> <p>25 percent of the total caliper of existing trees on the property must be replaced per the criteria of parts C and D of this Section.</p>	<p>of 5 trees per 5,000 square feet of Developable Site Area and per Section 10.13 and 10.14 of the CIDDS. The value of the existing trees shall be established with the Site Work permit and prior to the trees removal.</p>
10.15	Tree Maintenance				X		
10.16	Maintenance, Bond				X		
10.17	Req'ments, Specs		X		X	Plant spacing generally complies. Additional review will occur with the Site Work and Landscape permits.	
Chap 16 LANDSCAPE							
16.2.A	Integrate with Nature and Surroundings		X			The proposed landscape will successfully integrate with the surrounding natural environment.	
16.2.B	Context		X		X	The height and type of trees, shrubs and groundcover are selected to complement the various functions of the site.	
16.2.C	Soften Development		X		X	Generally complies. Landscaping is provided in order to provide greenness and to buffer buildings and hardscape surfaces.	
16.2.D	Key Landscape Elements		X		X	Trees are located at key focal points in order to establish lush, verdant landscape. The parking lot landscape island adjacent to the primary entrance to the tire facility is located in a high-visibility area which should be reflected in the planting palette with prominent and lush landscaping.	
16.2.E	Green Edge along I-90		X			Approximately 80-100 feet of WSDOT Right of Way will remain undisturbed between the property and the interstate. The wetland located at the back of the site and adjacent to I-90 will be enhanced via wetland mitigation planting which includes the addition of 11 new conifer trees at the back of the storage facility.	

ATTACHMENT 1 ASDP15-00002 – Gilman Point: CIDDS review

August 27, 2015

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
16.2.F	Use Accent Plantings -sense of place				X	Accent plantings will be provided throughout the development in key locations. Accent plantings will occur at the entrances to buildings, at the project entry, and along the internal street.	
16.2.G	Wildlife Habitat near critical areas		X			Wetland mitigation planting will be required for this project.	
16.2.H	Design Unity: repetition of plant varieties and other materials				X	The planting palette will be reviewed with the Landscape plan for compliance with this standard.	
16.2.I	Green Walls				X	Consider installing greenwalls on the waste enclosures when visible from pedestrian circulation.	
16.2.J	Trees on Site		X			Trees are located at parking lots, along sidewalks, and around the outside perimeter of the buildings. Trees planted in the wetland buffers shall be appropriate for riparian habitats. The largest canopy trees will be planted along the entry drive to provide a dramatic canopy at the entrance to the development.	
16.2.K	Setback Treatment		X			Trees are located within the building setbacks where possible including a deciduous tree located at the project entry near the south corner of the tire facility.	
16.2.L	Pedestrian Buffer		X			Trees, shrubs and groundcover are provided within the planting strip provided with the internal street.	
16.2.M	Native Plants				X	This can be accommodated and will be reviewed with the Landscape permit.	
16.2.N	Right Plant, Right Place		X		X	Generally complies. Final plant species, size, quantity, spacing, etc. will be reviewed with the Landscape permit review.	
16.2.O	Site Furnishings		X		X	The primary pedestrian facility for the project is provided with the adjacent East Lake Sammamish Trail. The trailhead, located adjacent to the site entrance, contains benches, kiosk, waste receptacles, pet waste pickup station, etc.	
16.2.P	Circulation					See comments in Chaps.6, 10 and 12	
16.2.Q	Surface Parking					See comments in Chap.10 and Chap.15	

ATTACHMENT 1 ASDP15-00002 – Gilman Point: CIDDS review

August 27, 2015

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
16.2.R	Parking Structures					See comments in Chap.10 and Chap.15	
16.2.S	Integrate Stormwater Facilities and Critical Areas		X				
16.3	Fences	X					
16.3.A	Appropriate Fence design	X				No fences are proposed.	
16.3.B	Fence height	X				No fences are proposed.	
16.3.C	Avoid canyon effect	X				No fences are proposed.	
16.3.D	Visual relief from bulk and size	X				No fences are proposed.	
16.3.E	Articulation and artwork					Please see comments and conditions for blank walls in 14.2.B.	
16.3.F	Preferred Materials				X	Where retaining walls are proposed, they shall not be constructed as rockeries. See Section 10.9.B and Condition 14.	
16.3.G	Chain Link fences	X				No chain link fences are proposed.	
16.3.H	Compliance with IMC 18.07.120	X			X		

Chap 11 SITE DESIGN

11.2.A	Integrate with Nature		X			The site integrates successfully with the surrounding physical and natural environment. The Green Necklace vision is fulfilled with the adjacent East Lake Sammamish Trail and wetland preservation and mitigation at the back of the site. The view corridor that exists along the trail will be preserved.	
11.2.B	Circulation Priorities		X			Circulation facilities are appropriate given the adjacency of East Lake Sammamish Trail (external) and given the nature of the two proposed. Additionally, a new trail connection will be provided to add an additional pedestrian connection link to the core of the site. See Condition 4.	
11.2.C	Sense of Place					The project will enhance the 'green edge' provided along Interstate 90 with the addition of wetland plantings and 11+ new evergreen trees that will be planted at the back of the storage facility. The project entrance will be integrated with the trail-	

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
						head of the East Lake Sammamish Trail and the building and site design will complement the adjacent trail and preserve axial views of Mt. Rainier.	
11.2.D	Sustainable Site Design					<p>Through the initial site planning and engineering processes, the stormwater system was changed from a detention pond to underground detention via a vault and piping.</p> <p>Under-building parking beneath the storage facility reduces the amount of impervious area needed for surface parking and additional wetland mitigation and tree planting will be provided at the back of the property.</p>	
11.2.E	Sense of Arrival		X			The site features two predominate elements which define the entrance. To the east, the tire facility is located within the build-to-line setback area and will have a prominent presence at the entrance to the overall property. An enlarged area of decorative concrete paving will be included to create a sense of arrival and transition point between the tire facility, site entrance and the trailhead of the East Lake Sammamish Trail located just west of the internal street.	
11.2.F	Existing Features & Context		X			<p>The site integrates successfully with the existing surrounding features. Improvements to the site include a trail connection and wetland mitigation including shrub and tree planting.</p> <p>See additional discussion under Chapter 14 for information on building design and compatibility with adjacent regional trail, street frontage and adjacent properties.</p>	
11.2.G	Views & Vistas		X			The storage facility is adjacent to the East Lake Sammamish Trail and has an approximate setback from the trail of 40 feet. Views of Mt. Rainier should not be impacted.	

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
11.2.H	Intuitive Wayfinding		X		X	Design details and materials will be reviewed with the Construction permits to implement intuitive wayfinding.	
11.2.I	Universal Design				X	The plan-set shows ADA ramps and parking stalls. Final review and approval of all ADA project elements Project to be reviewed for ADA compliance and universal design with construction permits. ASDP plans show ADA parking and access.	
11.2.J	Multi-functionality		X		X	See Section 12.2.E.	
11.2.K	Site Amenities & Street Furniture			X		The proposal does not sufficiently address this Design Standard as only one bench and 2 bike racks are proposed. Additional amenities, e.g., waste receptacles, fountains, wall art, etc. should be provided.	Condition 16: Additional site furnishings, including but not limited to waste receptacles, benches, sculptures, artwork, etc. shall be provided. This condition will be reviewed and approved with the Site Work and Landscape permits.
11.2.L	Special Paving		X			No special paving is shown with this application. Decorative paving shall be provided at the project entrance and at pedestrian crossings within the site. Additionally, strategies (e.g. beveled curbs and use of concrete instead of asphalt) shall be used to reduce the perceived width of the street and parking lot (while still allowing for adequate fire turning radius movements). For example, the planter islands and the end of the parking rows shall be treated with concrete aprons or other techniques.	Condition 17: Decorative concrete shall be provided for a minimum of 30 feet in length at the project entrance (between the primary entrance to the tire facility and the sidewalk connection on opposite side of internal street). Paving treatment may include, but is not limited to, stamping, scoring and variations in color and texture.
11.3	Standards for all Uses						
11.3.A	Pedestrian Connections		X			The plans show a new sidewalk connection linking the project to the existing sidewalk on Gilman Boulevard and a connection to the East Lake Sammamish Trail.	Condition 18: Any existing non-conforming ADA crosswalks and ramps that provide immediate access to circulation facilities within the project shall be recon-

ATTACHMENT 1 ASDP15-00002 – Gilman Point: CIDDS review

August 27, 2015

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
						<p>At the June 2, 2015 meeting of the River and Streams Board, a recommendation to add a trail connection from near the entrance of the storage facility to the East Lake Sammamish Trail was recommended. Staff concurs with this recommendation. See Section 6.2.A and Condition 4.</p> <p>The existing ADA ramps at the entrance off of Gilman Boulevard are non-conforming and must be replaced to meet current ADA standards, perpendicular to the sidewalk. This will be reviewed with the Site Work permit. See Condition 18.</p>	<p>structured so that they are in compliance with ADA standards in effect at the time of permit issuance.</p>
11.3.B	Connections to surrounding...		X			<p>The project has sufficient pedestrian and vehicular connection to surrounding circulation facilities and properties. One additional pedestrian connection will also be provided via a new connection from the storage facility to the East Lake Sammamish Trail.</p>	
11.3.C	Emphasize Landscaping		X		X	<p>A concept landscape plan has been provided with this application; see Attachment 6, Sheet L1.2. In general, the plans show sufficient landscape coverage and planting variety. Landscaping is not reviewed at a technical level with the ASDP review and detailed review will occur with the Landscape Permit to ensure compliance with the Development and Design Standards.</p>	
11.3.D	Community Space and Site Design		X		X	<p>Provision of Community Space is not a requirement for this project, however its inclusion is “highly encouraged”. Opportunities to enhance the project should be emphasized near the entrance to the site from Gilman Boulevard. This area will be enhanced per Condition 17 and will serve to enhance the entrance to the project as a Community Space. Additional streetscape elements/furniture should be considered to create a cohesive outdoor space with existing features</p>	

ATTACHMENT 1 ASDP15-00002 – Gilman Point: CIDDS review

August 27, 2015

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
						near the terminus of the East Lake Sammamish Trail.	
11.3.D.8	Prohibited		X			Complies. This will also be reviewed with construction permits.	
11.3.E	Parking, Drive-thru	X					
11.3.F	Streetwall		X		X	<p>The Build-to-Line for the proposal is very narrow (approximately 80' in length). This frontage also serves as the sole point of vehicle entry into the site. As a result of this tight configuration, there is only a limited portion of the tire facility building frontage which can meet the Streetwall and still permit access by a fire truck.</p> <p>In function, the building frontage of the tire facility is located (to the greatest extent feasible) near to the entry street (appx. 5 feet from back of sidewalk), giving a strong building presence at the entrance to the site and visibility from NW Gilman Boulevard.</p> <p>See Administrative Adjustment of Standards – Attachment 9.</p>	
11.3.G	Min. Bldg. Frontage		X			<p>Because of the limited and irregular configuration of the front property line; and, the necessity to provide a circulation facility (vehicular access) to the site, the project cannot meet the minimum building frontage threshold and thus an AAS is required.</p> <p>This standard is evaluated as an Administrative Adjustment of Standards. See Attachment 9.</p> <p>Also see Section 11.3.F.</p>	
11.3.H	Corner Bldg. Frontage					Complies. The building (tire facility) is located at the intersection of circulation facilities and serves to amplify the importance of the corner.	
11.3.I	Community Space as building frontage	X					

ATTACHMENT 1 ASDP15-00002 – Gilman Point: CIDDS review

August 27, 2015

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
11.3.J	Alternative Bldg Frontage	X					
11.3.K	Above-ground Utilities				X	Location of above-ground utilities is not shown at this time. Since mechanical and utility equipment were not shown on the site plan, it is assumed that they are located within buildings or in locations that do not impact achieving the vision for the project and compliance with CIDDS, such as within parking areas. See Section 10.8.D and Condition 13.	See Condition 13.
11.3.M	Res'l Front Door Orientation	X					
11.3.N	Res'l Garage Setbacks and Max. width	X					
11.4	Environmentally Critical Areas						
11.4.A	Minimize Impacts to Critical Areas		X			The project meets the requirements of the Critical Areas Regulations; see SEPA MDNS, Attachment 4. Outdoor lighting was not reviewed with this ASDP application and will be reviewed for compliance with applicable regulations with the construction permits.	
11.4.B	Bldg. Orientation to natural areas		X			The purpose of this design standard is to ensure that buildings are sited to recognize and acknowledge natural areas. This primarily comes into play with the adjacent Lake Sammamish Trail and wetland adjacent to the storage facility. Buildings are sited with respect to the natural areas of the site.	
11.5	Service, Loading, Waste						
11.5.A	Consolidate Facilities	X				Consolidation between the two facilities isn't necessary with this project as the storage facility will store all of its waste functions inside of the building.	
11.5.B	Locate within buildings or use roof cover			X	X	This standard encourages waste areas to be located in the building when possible. The applicant has stated that the storage facility will secure-	

ATTACHMENT 1 ASDP15-00002 – Gilman Point: CIDDS review

August 27, 2015

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
						ly store its waste area inside the building. The tire facility will use an outdoor (covered) waste enclosure. During construction permit review, the external waste enclosure serving the tire facility will be required to be covered. See Section 11.5.E and Condition 19.	
11.5.C	Waste enclosures and receptacles, keep wildlife out				X	The waste enclosure for the tire facility will be designed to keep wildlife out. See 11.5.E and Condition 19.	
11.5.D	Integrate screening with overall landscape and architectural design				X	See discussion under Section 11.5.C and Condition 19.	
11.5.E	Location and Size.		X		X	The waste enclosure for the tire facility is proposed to be located at the east end of the building, adjacent to the loading area. The size of the pad for the enclosure is shown at 12 x 12 feet in area. The proposed sizing has been reviewed and approved by Recology, the City's waste purveyor to ensure that is the correct size to accommodate the three waste streams (garbage, recycling, food waste). Final sizing will be determined through a joint review with Recology/Cleanscapes during the Site Work permit review.	Condition 19: The external waste enclosure shall be wildlife proof, covered and sized appropriately in order to accommodate three waste streams (trash, recycling and food waste). Sizing of the enclosure shall be confirmed prior to issuance of the Site Work permit. Screening of the enclosure shall also be provided.
	Sized per IMC18.07.130				X	Generally complies but sizes will be further reviewed during construction permit.	
11.5.F	Screening				X	The outside waste enclosure for the tire facility shall be screened appropriately including the use of landscape screening and appropriate structure design and materials. This will be reviewed with the Building permit for the tire center. See Section 11.5.E and Condition 19.	
11.5.F.1	Arch'l solid walls, landscaping/fencing: 6 feet				X		
11.5.F.2	Screening effective in winter and summer				X		

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
11.5.F.3	Materials compatible with overall development				X		
11.5.F.4	Place in alleys	X					
11.5.F.5	Do not locate on the same face as residential uses	X					
11.5.F.6	Delivery and garbage trucks conflicts		X			The exterior waste facility is located away from pedestrian and vehicular travel lanes to the greatest extent feasible.	

Chap 14 BUILDINGS

Chap 14 BUILDINGS							
14.2	General Standards						
14.2.A	Building design to accommodate change of use in future		X			Given the nature of the proposed businesses, future accommodation of different uses, especially for the storage facility may be difficult. With tenant alterations of the building interiors, it is possible that the buildings could be converted or repurposed for alternative uses.	
14.2.B	Blank walls			X		<p>Tire Facility Building Elevations - Sheet AB3.1</p> <ol style="list-style-type: none"> 1. Front elevation (southeast) – complies 2. Showroom elevation (southwest) – complies 3. Loading elevation (northeast) – complies 4. Rear elevation (northwest) – does not comply <p>The rear elevation appears very blank in nature and is designed as the “back” of the building. Per Section 14.2.A, “Generally, buildings should have no “back side”. As such, additional wall treatments and detailing are necessary for this wall. Design alternatives include, but are not limited to: piers, modulation, varied materials and detailing, trellises, applied elements and art. See Condition 20.</p> <p>Sheet A3.2 (wing wall extension for loading area)</p>	<p>Condition 20: The rear (east) elevation of the tire facility (including the wing wall extension for the loading area) shall incorporate additional wall treatments, including but not limited to canopies, building modulation and detailing, varied materials and textures, applied elements, trellises, etc., in order to reduce its blank appearance.</p> <p>Condition 21: The following walls of the storage facility shall incorporate additional wall treatments, including but not limited to canopies, building modulation and detailing, varied materials and</p>

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
						<p>The screen wall that extends beyond the corner of the loading area is very blank in nature. Given its adjacency to the neighboring restaurant, this wall will have a prominent presence and shall be treated using the techniques identified above in order to minimize its presence when viewed from offsite.</p> <p>Storage Facility Revised Building Elevations – Sheet A3.1, A3.2 (Attachment 7)</p> <p><u>South elevation</u> – The storefront and upper levels of the building meet the standard by providing windows and wall detailing which serve to break up the presence of blank walls.</p> <p>The section of wall underneath the building overhang (facing parking lot) has acceptable variation near the retail store entrance and where the overhead garage doors are located. The middle section, however, shows long expanse of blank wall, broken up only by an automatic sliding glass door. <u>This wall qualifies as a blank wall and additional treatment is required.</u></p> <p><u>West elevation</u> – The west elevation faces the East Lake Sammamish Trail, a key pedestrian circulation facility. As such, its design shall correlate with the trail appropriately.</p> <p>In general, the wall provides an inadequate level of detailing to break up the walls blank appearance, especially the middle section between the retail store front and the bay of windows near the back.</p>	<p>textures, applied elements, trellises, etc., in order to reduce its blank appearance:</p> <ol style="list-style-type: none"> 1. Wall on south elevation, underneath building overhang between the retail store entrance and the overhead garage doors; 2. West elevation: the untreated wall section between and above the retail store front and the tri-color block wall located at the back of the building; 3. North elevation: the north wall elevation shall be designed with dark, muted tones and materials in order to reduce its visibility from off-site. 4. East elevation: the east wall elevation shall be designed with colors and materials to achieve balance between the presence of the rear of the building with the lush landscape that is present along Interstate 90. This includes the addition of varied color blocking, modulation or other techniques that serve to partially obscure the wall while still making it attractive to vehicles traveling at freeway speeds.

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
						<p>Given the walls adjacency to the East Lake Sammamish Trail, three of the larger panels of wall (identified below) appear very blank, showing only small horizontal panels to break up the single plane/finish wall section. These wall sections will require additional treatment in order to reduce their blank presence.</p> <p>Sheet A3.2 <u>North Elevation</u> – The north wall is located on the back of the building and faces Interstate 90 (and to a lesser extent, the East Lake Sammamish Trail). In general, the wall appears very blank except for a bank of windows located on the NE side of the elevation. These windows would be more appropriately suited for the west side of the wall as this portion will be more visible from the East Lake Sammamish Trail.</p> <p>As part of the wetland mitigation for the project, approximately 11 evergreen trees will be planted in front of the northeast portion of the wall and this will help to further screen the wall from view. Additionally, there is also a grove of trees located within the WSDOT right-of-way between the interstate and the property which will provide effective screening as viewed from the interstate.</p> <p>Because this wall does not front onto a primary circulation facility, the primary design focus should be to obscure the wall from view of vehicles on Interstate 90. Dark, muted tones and wall detailing should be used to limit its presence.</p>	

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
						<p><u>East elevation</u> The level of detailing for the east elevation is suitable as it will have limited visibility from off site. The east elevation faces Interstate 90 and is separated from the freeway by an approximately 80-100 feet of vegetated landscape area that contains evergreen and deciduous trees and shrubs. A variety of dark colors and materials should be used to further reduce its visibility from off-site.</p>	
14.2.C	Consider internal and external views, solar access with taller buildings		X			<p>The buildings have been sited to reinforce the adjacent circulation facilities. The tallest building has been located at the back of the site, north and east of the East Lake Sammamish Trail.</p>	
14.2.C.1	Sunlight at street level		X				
	Maximize sunlight on sidewalks		X				
14.2.C.2	Shade re req'd Community Space	X					
14.2.D	Continuous streetwall					See Sections 11.3.F and 11.3.G	
14.2.E	Incorporate informal gathering spaces		X			The project compliments the adjacent informal gathering area provided at the trailhead of the East Lake Sammamish Trail.	
14.2.F	Buildings to encroach into public realm	X					
14.2.G	Green buildings	X			X	Green buildings are not proposed but will be further discussed and reviewed with the applicant during construction permit review.	
14.3	Building Mass and Design						
14.3.A.1	Setback buildings above the third story: change in bldg. materials, articulation and modulation				X	The storage facility does not comply with this standard as the upper floors of the building do not differentiate from the lower floors.	Condition 22: Exterior walls located above the 3 rd floor level of the storage facility shall be designed so as to create the sense of a building setback through changes in building materials, articulation and modulation that

ATTACHMENT 1 ASDP15-00002 – Gilman Point: CIDDS review

August 27, 2015

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
							differs from the first three floors.
14.3.A.2	Break up larger buildings			X	X	The purpose of this standard is to break up the presence of larger buildings so that they appear as a series of smaller buildings. For the storage facility, this will be achieved through treatments to the exterior walls. See Section 14.2.B and Condition 20.	
14.3.A.3	Provide surface relief, depth and shadows		X		X	The buildings, especially at the pedestrian level and where they front onto circulation facilities, provide an adequate level of surface relief and depth.	
14.3.A.4	Bldgs. > 45,000 s.f. shall be broken into 2 or more buildings	X				The building footprint of each building does not meet this threshold.	
14.3.A.5	Setbacks for commercial and retail uses		X			The proposed building setbacks meet the intent of this standard and to the extent feasible and given the proximity of adjacent uses and parcel configuration.	
14.3.A.6	Windows: divided light, operable, trimmed; recessed or projecting from building facade		X		X	The elevations for both buildings show windows that possess trim around their borders. The retail portions of both the storage facility and the tire facility have large banks of windows that should allow for good visibility both inward and outward.	
14.3.A.7	Tripartite articulation of facade		X		X	Compliance with this condition will be confirmed with future review of the Construction permits.	
14.3.A.8	Views re high rise buildings	X				High rise buildings are defined as being 10 or more stories.	
14.3.A.9	Special treatment of building corners adjacent to Public Spaces				X		
14.3.A.10	Other techniques to enhance building design and break up mass				X		
14.4	Ground Level Details						
14.4.A	Standards for All Uses						
14.4.A.1	Retail windows facing Circulation facilities – clear view of activity within; large windows		X		X	Buildings for the retails portions of each building will be reviewed with the Building permits to ensure that a clear view is provided both into and out of the retail area.	
14.4.A.2	Delineate public vs. private space	X					

ATTACHMENT 1 ASDP15-00002 – Gilman Point: CIDDS review

August 27, 2015

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
14.4.A.3	Active ground floor uses that are visible from Circulation facilities		X			Both buildings feature retail store fronts which have primary entrances that front onto, and are visible from, circulation facilities.	
14.4.A.4	Numerous entrances		X			The storage provides a primary entrance off of a circulation facility. An additional public entrance provided from the parking lot and provides access for individuals to move items in and out of the storage units. The tire facility has two entrances for its retail store front.	
14.4.A.5	Primary bldg. entrances shall be accessible and visible from Circulation Facilities		X			Primary building entrances are accessible and visible from Circulation Facilities.	
14.4.A.6	Primary bldg. entrance visually more prominent than secondary entrances; emphasize through arch'l modulation and articulation, lighting, weather protection			X		Both retail storefront entrances of the tire facility shall be designed and operated so that they are available for patron use during regular business hours. Additionally, weather protection shall be provided above both doors. See Section 14.5.A.1.	Condition 23: Both of the retail storefront (customer) doors of the tire facility shall be open and available so that customers may both enter and exit during regular business hours. The doors are intended for public use and shall not be blocked or otherwise prevent in/out access by patrons.
14.4.A.7	For Bldg. with multiple frontage along a Circulation Facility – each frontage shall be designed to complement the Circulation Facility abutting it				X		
14.4.A.8	50% of building frontage shall be windows; 75% of window area shall use clear glazing		X		X	Complies. This will be verified with the Building permit.	
14.4.A.9	Mirrored or Reflective Glass not allowed				X		
14.4.A.10	Ground level retail and entrance lobby located on a Pedestrian Oriented Circulation Facility shall have a first floor height of at least 15 feet		X		X	The building elevations appear to meet this requirement. This will be confirmed with the Building permit review.	
14.4.A.11	Landscaping as transition between property line and building face					Complies. Instead of the property line, the back of the sidewalk is used as the baseline for applying	

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
						this standard. The landscape treatment will be reviewed at the Landscape Permit phase.	
14.4.A.12	Elements to enhance ground floor: clerestories over storefront windows, projecting window sills, medallions, benches and seat walls along 25% of façade; decorative masonry						
14.4.B1-B5	Ground Level Residential Uses	X				Not applicable to commercial/retail projects.	
14.5	Weather Protection						
14.5.A	Standards for All Uses						
14.5.A.1	Weather protection required at primary entries and circulation facilities			X		<p><u>Meets standard</u> The building elevation for both the storage facility shows weather protection at the primary entrance to the building.</p> <p><u>Does not meet standard</u> The standard states that “weather protection shall be required over entrances and across 75% of the building façade length, where the building is located at the property line or the development extends the sidewalk onto the site and against the building”. This provision is applicable to both the tire and storage facilities and therefore weather protection must be provided for at least 75% of the portion of the building that is serving as the building frontage onto the internal street.</p>	Condition 24: Weather protection shall be provided for at least 75 percent of the tire and storage facility building that fronts onto the internal street. For the tire facility, the weather protection shall generally extend from the intersection of the crosswalk to the secondary building entrance (parking lot side). Weather protection must conform to height and projection dimensions as required by the Standards of CIDDS Section 14.5.A.
14.5.A.2	Height of weather protection				X	See Section 14.5.A.1 and Condition 24.	
14.5.A.3	Materials/projection length of weather protection				X	See Section 14.5.A.1 and Condition 24.	
14.5.B	Residential building entrance size	X					
14.6	Roofs & Parapets						
14.6.A.1	Rooftops as active amenities	X					
14.6.A.2	Alternatives to active amenities: green roofs, solar panels	X					
14.6.A.3	Consider public access of rooftops	X				It is impractical to provide public access to rooftops of the commercial/retail buildings.	

ATTACHMENT 1 ASDP15-00002 – Gilman Point: CIDDS review

August 27, 2015

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
14.6.A.4	Nonresidential buildings shall have parapets and projecting cornices to create a prominent edge; Sloping roof elements may be approved by Director.			X		With the exception of the portion of roof above the retail portion of the storage facility, the buildings do not have any parapets or cornices to create a prominent edge when viewed against the sky. Treatment to the rooflines will be necessary in order to meet the intent of this standard.	Condition 25: The tire and storage facility shall utilize parapets and/or projecting cornices to create a prominent edge when viewed against the sky.
14.6.A.4	Residential parapets	X					
14.6.A.5	False parapets prohibited.		X			False or excessively large parapets are not proposed.	
14.6.A.6	Parapets shall not exceed 25% of the height of the supporting wall and shall not exceed 8 feet.			X	X	No parapets are provided except for above the retail storefront of the storage facility. See Section 14.6.A.4 and Condition 24.	
14.6.A.7	Penthouse design	X					
14.6.A.8	Sloped roofs	X				No sloped roofs are proposed.	
14.6.A.9	Roof surfaces shall use a "white roof" with a Solar Reflectance Index of 78 or greater				X		
14.6.A.10	Rooftop equipment screening		X		X	The building elevations show mechanical screening for equipment located on the rooftops of the buildings.	
14.6.A.11	Wireless communications equip.	X				No telecommunication equipment is proposed.	
14.6.A.12	Rooftop terrace or garden equipment	X					

Chap 17 LIGHTING

17.2	General					Photometric plans (Sheet A0.3) were provided with the application but have not been reviewed for compliance with any applicable codes. To be reviewed for compliance at construction permit phase	
17.2.A	Safety at night				X	To be reviewed with Construction permits.	
17.2.B	Intuitive				X		
17.2.C	Sustainable				X		
17.2.D	Contribute to the public realm				X		
17.2.E	Dark sky elements				X		

ATTACHMENT 1 ASDP15-00002 – Gilman Point: CIDDS review

August 27, 2015

CIDDS Standard #	Name	Not Appl.	Meets Standard?		Review At Constn.	Staff Analysis	Conditions of Approval
			Yes	No			
17.2.F	Appropriate for type of activities				X		
17.2.G	Use as architectural elements				X		
17.2.H	Volume of Space				X		
17.2.I	Illumination levels				X		
17.3	BUG				X		
17.4 A-E	Design and Fixture Standards			X		A design detail for interior site lighting is provided on Sheet A0.3 that does not comply with the standards as it is too tall. The proposed light is 25 feet high which exceeds the allowed light pole height by 10 feet. The proposed light pole shall not be used.	Condition 26: Light poles shall not exceed 15 feet in height.
17.5.	Circulation Standards: Streets				X		
17.6	Circulation Standards: Pedestrian, Bicycle and Trail				X		
17.7	Community Space Standards				X		
17.7.A	Lighting for walkways min. of 6 feet height				X		
17.7B	Contributes to urban character				X		
17.7.C	Festive or special lighting				X		
17.7.D	Varying levels				X		
17.7.E	Children's play area	X					
17.7.F	Recreation areas	X					
17.8	Parking				X		
17.9	Building Design: Entry				X		
17.10	Landscape Standards				X		

Attachment 2: ASDP15-00002 Gilman Point Construction Conditions

The following conditions apply to constructions permits for the Gilman Point project, file number ASDP15-00002, which include but are not limited to the clearing and grading, utility, including roads, landscape and building permits. All of the conditions listed below are meant to assist the applicant through the transition from the land use permit to construction permits. In addition to land use permit condition compliance, each phase of this development will have these conditions applied to applicable, associated construction permits. Note that while the assembly of these conditions is meant to assist both the City and Applicant with the transition from land use to construction permit, this list is not meant to be exhaustive. The Applicant is required to meet all standards in the Central Issaquah Development and Design Standards checklist for this project that has an “X” in the “Review at Construction” column, regardless of no mention of the requirements in this list.

The Applicant will also be required to comply with other relevant codes, regulations, and agreements.

Critical Areas, Clearing and Grading

1. Look for opportunities to reuse construction materials and to purchase locally-produced products.
2. Appropriate measures, as determined by the Director, shall be taken to ensure that construction operations do not result in erosion and sedimentation impacts on water quality and on nearby drainage courses. In addition, the applicant shall comply with the City’s TESC construction requirements.
3. The applicant shall minimize impacts to existing roads during clearing and grading activities. Prior to issuance of grading permits that would require the import or export of soils, the applicant shall be required to submit a grading worksheet demonstrating that the import/export of soil will be minimized through on-site reuse and a haul plan to minimize impacts to the existing residents and the local road network. This information shall be considered by the City prior to approval of associated permits.
4. Provide a summary, on the face of the plans, of each net grading impacts of each Utility Permit. Each permit must balance cut and fill or provide SEPA analysis and mitigate any non-balanced grading activities.
5. The applicant shall provide geotechnical analyses prior to any grading activities demonstrating soils are compatible for the proposed development. Information from geotechnical analyses shall be considered and incorporated, as determined by the Director, into related permits.

6. The applicant will monitor wetland buffers and common edges of forested open space for tree blow downs following clearing for 3 years. Should it be found that these areas have been impacted from blow downs, the applicant will plant evergreen trees at a ratio consistent with the City's Tree Replacement Code (IMC 18.12.1390), subject to approval by the property owner.
7. Any cleared land that sits idle for 6 months shall be revegetated, consistent with the phasing plan; however at no time shall situations exist which might contribute erosion or off-site sedimentation. Any revegetated areas shall be maintained for 3 years.
8. Critical Areas Boundaries:
 - a. Temporary Marking: The location of the outer extent of the critical area buffer and building setback line pursuant to an approved Development or Land Use Permit shall be marked in the field with orange construction fencing and/or other appropriate apparatus, as determined by the Director during critical area review. The location and presence of such markings in the field shall be approved by the Director, prior to the commencement of permitted activities. Such field markings shall be maintained throughout the duration of the construction activities.
 - b. Survey Markers: Permanent survey stakes using iron or cement markers as established by current survey standards shall be set delineating the boundaries between adjoining properties and the critical areas tracts.
 - c. Signs: Boundaries between critical area tracts and/or areas with conservation easements and adjacent lands shall be identified using permanent signs explaining the type and value of the critical area, except the portions, if any, of a critical area that are adjacent to natural or wild areas. Whenever a trail enters a critical area buffer, the boundary shall be identified using permanent signs explaining the type and value of the critical area. The number of signs required by the Director will be dependent upon the size of the critical areas and the use of the property.

Neighborhoods, Site and Building Design

9. All exterior staircases and walkways shall be at least 5 feet wide, clear of intruding handrails, mature landscape, car overhangs, light poles, tables and chairs, etc..., except in those portions of the sidewalk where the standard is greater than 5 ft; then the width will be equal that of the standard. Where narrow planter beds are adjacent to walkways, plants shall be selected whose mature size will not impact the walkway width.
10. Retaining walls shall be keystone, stone, or other appropriate materials, not rockeries unless they aren't visible.

11. Locations of above-ground utility boxes, mechanical equipment and fire department appurtenances must be identified on site work permit plans. No utility boxes, mechanical equipment and other appurtenances, other than what is shown on approved Site Work Permits shall be installed on site.

Streets

12. Construction plans shall show the locations of proposed fire hydrants.
13. Use techniques during road design that will slow vehicles, facilitate pedestrian friendliness, and encourage vehicles to make appropriate choices. These might include installing curb bulbs at intersections, driveways, and other entrances, as well as at hydrants and other “no parking” locations.
14. If raised pedestrian crossings are used they must be designed in conjunction with the limitations of EF&R vehicles, e.g. will not high center the vehicles, while also drawing drivers’ attention to pedestrian facilities and slowing traffic. Designated pedestrian paths and trails which cross vehicular routes, drives, access routes, etc... shall be designed to draw the driver’s attention to the possible presence of pedestrians. This condition would be met, for example, through the use of pedestrian tables, changes in material (e.g. concrete, pavers; not solely paint or striping), etc... Raised crosswalks shall not be installed on designated emergency vehicle routes unless they are designed in a way that will not “high center“ the emergency vehicles.
15. Prior to the issuance of any construction permit, existing trees to be retained shall be protected throughout construction by erecting temporary fencing.
16. All curbs must be vertical, unless otherwise approved by the Director, such as for fire access or some other unique circumstance. No extruded curbs are allowed.
17. All curb ramps must direct the user into the crosswalk (not the intersection or travel lanes, and whether the crosswalk is striped or implied) and generally point toward the curb ramp on the opposing side.
18. Driveway widths shall be reduced to the minimum for fire truck and dumpster truck access. Use mountable curbs to accommodate required turning radii where feasible.
19. Where stairs must be used in the sidewalks or the path system, the Applicant shall avoid single steps and all steps should be level and of even height per the International Building Code.
20. Private roads shown shall have access easements to the City for public access (vehicular and pedestrian), emergency service, and public utilities.

Buildings

21. An accessible route of travel must be provided to all portions of the building, to accessible building entrances, and connecting the building to the public way per WA State Code 1104, 2012 IBC.
22. Where pedestrian walkways are within 3 feet of a 2 ½ foot drop near the retaining walls being constructed for the planted swale, fall protection shall be provided.
23. The roof colors shall be a light color with a Solar Reflective Index (SRI) of 78 or greater.

Utilities

24. The applicant shall minimize slope and surface disturbances for the construction of any necessary discharge pipes for stormwater. Project Stormwater improvements shall be permitted or in place prior to approval of land use permits.
25. Show all critical area boundaries and building setback lines on the plans and include, on the face of the plans, an attestation by a Registered Surveyor or Engineer that all Critical Area Boundaries are accurately shown on the plans.
26. Provide a summary, on the face of the plans, of the utility capacity impacts of each Utility Permit including, impervious area, Q₂, Q₁₀, Q₁₀₀, Max day Demand, Average Day Demand, Peak Wet Weather Flow, and Average Dry Weather Flow.
27. The applicant shall incorporate the use of Low Impact Development (LID) techniques to reduce the quantity of site stormwater runoff and minimize dependence on the publicly-engineered stormwater facilities.
28. Backflow shall be reviewed by Public Works Operations Department and provided for all water services.
29. Move domestic water meter for storage facility prior to hydrant (for water quality purposes).
30. Consider cutting and capping water service at main for 240 NW Gilman Blvd (Caboose). Install new tap with service from new 12" main, perpendicular to main and shortest distance between main and meter. Replace meter and upgrade to radio-read. (City of Issaquah will perform work, but fee for radio read upgrade.) The existing service line location is unknown, but most likely it's tapped near the hydrant where this project is connecting to the existing 8" main on west side of off-site improvements.
31. Sewer is available to the site and service will be provided via a new manhole connection.

Parking

32. Vertical clearances for the fire truck bucket shall be maintained within the parking spaces abutting the fire lanes. Trellises and other architectural projections of the buildings shall be sized appropriately and trees planted in the parking areas of the 3-story buildings shall be at a height, and spaced appropriately to prevent obstructing the fire truck extensions.
33. Handicapped parking is required per Table 1106.1 2003 IBC. One of every 6 accessible spaces or fraction thereof must be van accessible, per Section 1106.5 2012 IBC and requires an 8' stall with an 8' access aisle. The slope of both the stall and the aisle may not exceed 2% and must be paved to provide a hard, stable surface. A van accessible parking sign is required and must be mounted at 60" minimum to the bottom of the sign.

Landscape, Trails and Parks

34. No Temporary Certificate of Occupancy shall be issued for any structure prior to approval of the Landscape Permit.
35. No Final Certificate of Occupancy shall be issued prior to inspection and acceptance of the landscape installation for the entire site, and provision of a Performance Bond.
36. Landscape construction permit plans shall identify which trees are provided as replacement trees. These trees shall meet the requirements per CIDDS 10.14
37. Evergreen shrubs shall be planted around the waste and utility equipment enclosures for additional screening. Evergreen shrubs shall be of a size at planting and a variety that will achieve a 100% sight obscuring functions within 3 years or prior to the release of the landscape maintenance bond.
38. Provide a minimum of 12" of topsoil to planted areas (e.g. streetscapes, private parks, private yards) to facilitate localized infiltration.
39. Any tree located within 8 ft. of a public street, curb, sidewalk, or similar publicly-owned and maintained paving or public utilities must have at least 10 lineal feet of root barrier placed adjacent to pavement.
40. Drought-resistant shrubs and hardy groundcover shall be used for the tree planter areas, the landscaping along the building foundations, and the surface parking areas.
41. Plants should be selected and spaced based on their mature size. To facilitate review, plants will be shown on landscape plans at 85% maturity.

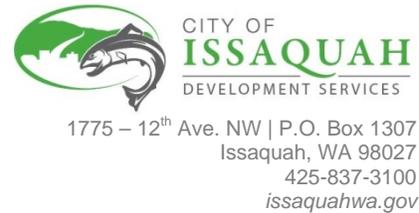
42. All proposed groundcover for parking lot landscapes shall be evergreen and drought-resistant (shall be of a type that will survive the heat from surrounding impervious surfaces) and planted at distances that will allow 100% coverage in 3 years.
43. Any lighting provided ~~with trails~~ shall be designed in a manner that does not negatively affect the surrounding uses.
44. Pedestrian-scale lighting shall be located with consideration of tree canopies at maturity. Tree foliage should not block the light.
45. The TESC or site clearing and grading permit plans shall show the tree to be retained and the tree protection fencing locations. Fencing location shall follow the City of Issaquah's standards. Prohibited activities within the tree protection area shall be followed at all times during construction.

Sustainability

46. All structures shall incorporate green building/sustainability components.
47. Select plants that do not require irrigation following establishment and which reduce the heating and cooling needs of buildings. Also select plants that enhance wildlife.
48. Select low water use plants and employ low water use technologies in all landscape design.
49. Consider using LED lamps for exterior light fixtures.
50. All street lights and walkway lights shall use full cut-off LED light fixtures.

Service areas

51. The dumpster enclosures' structural frame shall be designed with a foundation or anchored to the ground. The frame shall be a solid metal that does not corrode easily. Doors shall be designed to automatically close and hinges shall be industrial grade.
52. Dumpster enclosures shall be of a durable material such as masonry, cement fiber board or metal that is not easily dented.



LAND USE PERMIT APPLICATION

This Section For Staff Use Only	
Permit Number: _____ Staff Contact: _____	Date Received: _____

Type of Application: ASDP 2 New and Major

PROJECT INFORMATION

Name of Project (if applicable): GILMAN POINT

Project Site Address: 160 NW GILMAN BOULEVARD

Parcel Number: 884350-0238

OWNER

Name: MarSi LLC

Address: 160 NW Gilman Boulevard

Phone: 425-269-4204 Email: bigdad39@comcast.net

APPLICANT

Name: Gilman Point LLC; Contact: Bob Power

Address: 165 NE Juniper Street, Suite 100, Issaquah, WA 98027

Phone: 425-837-9720 Email: bpower@seaconllc.com

CONTACT

Name: MAGELLAN ARCHITECTS; Attn: Dirk McCulloch

Address: 8383 158th Ave NE, Suite 280, Redmond, WA 98052

Phone: 425-885-43600 Email: dirk@magellanarchitects.com

PROPOSED PROJECT DESCRIPTION

Please provide a brief description of the project. (Use an additional sheet of paper, if necessary.)

Please see attached

I certify (or declare) under penalty of perjury under the laws of the State of Washington that all application information, including plans and reports, are true and complete to the best of my knowledge. I understand the lead agency is relying on them to make its decision.

Signature: _____ Date: _____

PROJECT SITE INFORMATION

Legal Description: (Use an additional sheet of paper, if necessary.)

Please see attached

Zoning Designation: **MU Mixed Use**

Land Use Designation: **Commercial**

Subarea Designation: **Gilman**

Shoreline Designation, if applicable: _____

Existing Land Use: Commercial Office Building

Adjacent Land Uses North: Interstate 90 Freeway

 South: Restaurant (Pogacha) and Gilman Boulevard

 East: Interstate 90 Freeway

 West: East Lake Sammamish Trail corridor

Area in square feet: 82,134 sf (1.89 ac)

Does the site contain any of the following environmentally critical areas? Check all that apply.

- | | |
|--|--|
| <input type="checkbox"/> Flood Hazard Area | <input type="checkbox"/> Landslide Hazard Area |
| <input type="checkbox"/> Streams | <input type="checkbox"/> Wetlands |
| <input type="checkbox"/> Steep Slope Hazard Area | <input type="checkbox"/> Coal Mine Hazard Area |

PROPOSED DEVELOPMENT STATISTICS

Proposed Land Use: **Self Storage (fully enclosed) and Tire Shop**

Density (multifamily only): n/a

Impervious Surface Ratio: 75.3%

Pervious/Landscaping/Open Space Provided (in square feet): 61,858 sf

Maximum Proposed Building or Structure Height: 54 feet

Total Proposed Building Square Footage (Gross Area): 99,972

Proposed Setbacks Front: 10'

 Rear: 11' 2"

 Side: 5'+ & 10' 5"

Parking Spaces Provided: 44

LEGAL DESCRIPTION:

PROJECT: **Gilman Point**
PROJECT NO.: **14-140**
ADDRESS: **160 Northwest Gilman Boulevard**
Issaquah, WA 98027

PARCEL #: **884350-0238**

Legal Description:

THAT PORTION OF LOTS 1 THROUGH 4, INCLUSIVE, BLOCK 6, HERBERT S. UPPER'S FIRST ADDITION TO ISSAQUAH, ACCORDING TO THE PLAT THEREOF RECORDED IN VOLUME 12 OF PLATS, PAGE 13, IN KING COUNTY, WASHINGTON, DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF SAID BLOCK 6;
THENCE SOUTH 16°41 '00" EAST 414.26 FEET TO THE NORTHERLY MARGIN OF STATE HIGHWAY NO.2;
THENCE SOUTH 48°25'00" EAST ALONG SAID NORTHERLY MARGIN 80 FEET TO THE MOST WESTERLY POINT OF A TRACT OF LAND AS DESCRIBED IN DEED RECORDED UNDER RECORDING NUMBER 8904141132;
THENCE NORTH 41°35'00" EAST 125.7 FEET;
THENCE SOUTH 48°48'30" EAST 4.5 FEET;
THENCE NORTH 42°45'03" EAST 120.98 FEET TO THE SOUTHWESTERLY MARGIN OF PRIMARY STATE HIGHWAY NO.2 (SR90) AS CONDEMNED IN KING COUNTY SUPERIOR COURT CAUSE NUMBER 691461;
THENCE NORTHWESTERLY ALONG SAID SOUTHWESTERLY MARGIN TO THE NORTH LINE OF LOT 1 IN SAID BLOCK 6;
THENCE WEST ALONG SAID NORTH LINE TO THE POINT OF BEGINNING;

TOGETHER WITH THAT PORTION OF THE NORTHEAST QUARTER OF THE NORTHEAST QUARTER OF SECTION 28, TOWNSHIP 24 NORTH, RANGE 6 EAST, W.M., IN KING COUNTY, WASHINGTON, LYING NORTH OF LOCUST STREET, EASTERLY OF NORTHERN PACIFIC RAILROAD RIGHT-OF-WAY AND SOUTHERLY OF PRIMARY STATE HIGHWAY NO.2 (SR90);

AND TOGETHER WITH THAT PORTION OF LOCUST STREET LYING SOUTHERLY OF THE ABOVE DESCRIBED PORTION OF SECTION 28, AND NORTHERLY OF THE ABOVE DESCRIBED PORTION OF BLOCK 6, HERBERT S. UPPER'S FIRST ADDITION TO ISSAQUAH.

**CITY OF ISSAQUAH
MITIGATED DETERMINATION OF NONSIGNIFICANCE (MDNS)**

Description of Proposal: Construct a 4-story 88,000 square foot (SF) fully-enclosed self-storage building, and a 12,000 SF retail/service tire shop with 7 service bays and a retail/office area on a 1.89 acre site. Site improvements include 44 parking spaces, associated landscaping and utility improvements. There is a Category III off-site wetland, located in adjacent the East Lake Sammamish Trail corridor, and the wetland buffer extends onto the north corner of the subject site. The 50-foot wetland buffer would be reduced by 1,064 SF to a minimum buffer width of 37.5 feet (25% buffer reduction), replaced with 1,075 SF of added buffer area, and the entire on-site buffer area (6,163 SF) would be enhanced with native trees and shrubs. The site would be accessed from a private driveway off NW Gilman Blvd.

Proponent: Bob Power
Gilman Point LLC
165 NE Juniper St, Suite 100
Issaquah, WA. 98027

Permit Number: ASDP15-00002

Location of Proposal: 160 NW Gilman Blvd

Lead Agency: City of Issaquah

Determination: The lead agency for this proposal has determined that it does not have a probable significant adverse impact on the environment. An environmental impact statement is not required under RCW 43.21C.030(2)(c). This decision was made after review of a completed environmental checklist and other information on file with the lead agency. This information is available to the public on request.

Comment/Appeal Period: This MDNS is issued under WAC 197-11-340(2) and 197-11-680(3)(a)vii. There is a 21-day combined comment/appeal period for this determination, between **July 9, 2015 and July 30, 2015**. Anyone wishing to comment may submit written comments to the Responsible Official. The Responsible Official will reconsider the determination based on timely comments. Any person aggrieved by this determination may appeal by filing a Notice of Appeal with the City of Issaquah Permit Center. Appellants should prepare specific factual objections. Copies of the environmental determination and other project application materials are available from the Issaquah Development Services Department, 1775 12th Avenue NW.

Appeals of this SEPA determination must be consolidated with appeal of the underlying permit, per IMC 18.04.250.

Notes:

- 1) This threshold determination is based on review of the construction plans received March 10, 2015; environmental checklist received March 10, 2015; and other documents in the file.
- 2) Issuance of this threshold determination does not constitute approval of the permit. The proposal will be reviewed for compliance with all applicable City of Issaquah codes, which regulate development activities, including the Land Use Code, Critical Area Regulations, Building Codes, Clearing and Grading Ordinance, and Surface Water Design Manual.

Findings:

1. Critical Areas - There is a Category III off-site wetland, located in the adjacent the East Lake Sammamish Trail (ELST) corridor, and the wetland buffer extends onto the north corner of the subject site. The off-site wetland has been enhanced as part of mitigation for improvements to the

ELST and the current wetland rating considered the enhancement. The 50-foot wetland buffer extending on the site would be reduced by 1,064 SF to a minimum buffer width of 37.5 feet (25% buffer reduction), replaced with 1,075 SF of added buffer area. The entire on-site wetland buffer area (6,163 SF) would be enhanced with native trees and shrubs. The buffer reduction is limited to only the south end of the wetland buffer, minimizing the extent of buffer impacts. The proposed building would not actually encroach into the wetland buffer, but would be constructed adjacent to the buffer and the required building setback would extend into the buffer area. The proposed buffer averaging is consistent with the City's Critical Areas Regulations; limiting buffer reductions/averaging to 25% of the standard buffer width and providing an equal buffer replacement area. The proposed buffer reduction area currently consists of pasture vegetation and does not provide any significant protective buffer functions (i.e. water quality or habitat). The buffer enhancement plan would significantly increase the plant species and structural diversity of buffer vegetation, thereby increasing the habitat quality and also providing a physical and visual screen between the development and the wetland and ELST. The buffer enhancement would be adjacent to and complement the previously enhanced off-site wetland and wetland buffer located in the ELST corridor.

Final wetland/wetland buffer enhancement plans are required for approval by the Issaquah Development Services Department (DSD) prior to issuing construction permits. Final plans shall include a planting plan and a 5-year monitoring/maintenance plan with performance standards for monitoring success of the enhancement planting. The plans shall meet standards of the King County Critical Areas Mitigation Guidelines for the planting density and monitoring performance standards.

2. Traffic – A Traffic Impact Study (TSI, March 9, 2015) was provided to document and evaluate the traffic impacts related to the development proposal. The study concludes the proposal would generate 17 new PM peak hour trips based on trip generation from independent studies of similar projects.

The traffic study evaluated the existing level of service (LOS) and the 2016 LOS with the proposed development at the intersections of NW Gilman Blvd and Front St N, NW Gilman Blvd and NW Juniper St, NW Juniper St and Rainier Blvd N, and NW Gilman Blvd and the east driveway entrance into the development site. The analysis concluded the proposal would not impact the LOS at these intersections, with the exception of the stop-controlled, northbound left turn movement from NW Juniper St onto NW Gilman Blvd. This turn movement currently operates at LOS F and the proposal would increase the left turn delay by approximately 18 seconds. Improvements and signalization of this intersection are identified in Issaquah's Transportation Improvement Program (TIP). The TIP improvement will address the LOS deficiency.

For the City's new concurrency standards (adopted by Ordinance #2733, effective February 2, 2015), a system-wide transportation concurrency assessment for future planned growth was completed. Road improvements to mitigate for the corresponding planned growth were identified, and a transportation impact fee calculated to fund these road improvements. According to the City's traffic model, adopted level of service (LOS) standards would be maintained and development projects would be concurrent provided the identified road improvements are constructed.

Under the City's new concurrency standards, individual development applications are not required to address their traffic impacts on the local street system, provided a proposal is consistent with the City's planned growth that was previously evaluated in the traffic concurrency model. The subject proposal is consistent with the growth assumptions in the traffic concurrency model. Therefore, the proposed development can withdraw trips from the "trip bank" that was calculated for concurrency and can mitigate their traffic impacts by payment of the traffic impact fee. The traffic impact fee will be used by the City to fund transportation improvements identified in the concurrency model and on the City's Transportation Improvement Program (TIP). Improvements and signalization of the NW

Gilman Blvd and NW Juniper St intersection is identified in Issaquah's Transportation Improvement Program (TIP).

However, the concurrency assessment doesn't address traffic operations and safety at the project site driveway access or at non-concurrency intersections. Currently, during the PM peak hour the eastbound left-turn queue at the intersection of NW Gilman Blvd and N Front St backs up beyond NW Juniper St. Also, the westbound left-turn queue at the intersection of NW Gilman Blvd and NW Juniper St extends beyond the existing left-turn pocket and blocks westbound through traffic flow on NW Gilman Blvd. Due to the existing traffic conditions on NW Gilman Blvd in the immediate vicinity of the site, and to address the safety and operations of the site access, the driveway access shall be restricted to right-in/right-out turn movements only. The applicant shall install C-curb on NW Gilman Blvd to limit turn movements. Final plans for the driveway access onto NW Gilman Blvd shall be approved prior to issuance of construction permits.

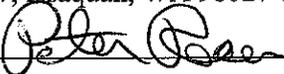
Due to the configuration of the access driveway, turning movements of large delivery trucks may require both lanes on NW Gilman Blvd. Therefore, the site access for trucks and truck trailers 35-feet and longer shall be restricted to the hours between 10:00 PM and 6:00 AM.

3. Bicycle and Pedestrian Facilities – The *Nexus Study for Bicycle and Pedestrian Facilities Mitigation Fees* (Henderson Young & Company, December 10, 2014) was adopted by the City Council, Ordinance #2733, effective February 2, 2015. The study quantifies the direct impact of new development on the current system of bicycle and pedestrian facilities and the additional demands from future growth to maintain the adopted level of service. The report uses trip generation rates based on the different land use types to quantify the impacts of new development. It also identifies 16 specific bicycle and pedestrian projects that are needed to support the City's level of service standard. Payment of mitigation fees as determined in the study may satisfy a development's requirement to mitigate their project impacts on the level of service standard. If the developer doesn't voluntarily use the methodology and mitigation fees as determined in the report, the developer may choose other methods to quantify and mitigate their impact including conducting a study of its impacts and identifying alternate means of mitigating impacts to achieve the adopted standards. Applicant objections to the voluntary payment should be made during the SEPA comment period.
4. Public Services - The proposal would have a potential impact on public services, including police and general government buildings. IMC Chapter 3.74, Methods to Mitigate Development Impacts, provides alternatives to mitigate for direct impacts of proposed development. The City may approve a voluntary payment in lieu of other mitigation. Rate studies for police facilities and general government buildings are included in IMC 18.10.260 as the City's SEPA policy base. The rate studies present the methodology and formulas for determining the amount of the mitigation fee commensurate with the proposed land use and project impacts. Applicant objections to the voluntary payment should be made during the SEPA comment period.

Mitigation Measures: The Mitigated Determination of Nonsignificance is based on the checklist received March 10, 2015 and supplemental information in the application. The following SEPA mitigation measures shall be deemed conditions of the approval of the licensing decision pursuant to Chapter 18.10 of the Issaquah Land Use Code. All conditions are based on policies adopted by reference in the Land Use Code.

1. Final wetland/wetland buffer enhancement plans are required for approval by the Issaquah Development Services Department (DSD) prior to issuing construction permits. Final plans shall include a planting plan and a 5-year monitoring/maintenance plan with performance standards for monitoring success of the enhancement planting. The plans shall meet standards of the King County Critical Areas Mitigation Guidelines for the planting density and monitoring performance standards.

2. To address the safety and operations of the site access, the driveway access shall be restricted to right-in/right-out turn movements only. The applicant shall install C-curb on NW Gilman Blvd to limit turn movements. Final plans for the driveway access onto NW Gilman Blvd shall be approved prior to issuance of construction permits.
3. Due to the configuration of the access driveway, turning movements of large delivery trucks may require both lanes on NW Gilman Blvd. Therefore, the site access for trucks and truck trailers 35-foot and longer shall be restricted to the hours between 10:00 PM and 6:00 AM.
4. The applicant shall mitigate for potential impacts on public services and bicycle and pedestrian facilities. The City may approve a voluntary payment in lieu of other mitigation. The current fees based on the rate studies are \$49.32 per 1,000 SF of new building area for the General Government Buildings Mitigation Fee and the Police Mitigation Fee is \$123.29 per 1,000 SF of new office area and \$912.37 per 1,000 SF for new retail building area. The Bicycle-Pedestrian Facility Mitigation Fee is \$225.00 per 1,000 SF for a tire store and a self-storage facility is not a listed use and will need to be determined based on similar trip generation uses. Applicant objections to the voluntary mitigation fee payments should be made during the SEPA comment period. The impact fee cost will be determined based on the new building area approved in the building permit application and the impact fee in effect at permit issuance. The applicant should pay the voluntary contribution prior to issuance of building permits.

Responsible Official: Peter Rosen
Position/Title: Environmental Planner
Address/Phone: P.O. Box 1307, Issaquah, WA 98027-1307 (425) 837-3094
Date: 7/9/2015
Signature:  _____

cc: Washington State Department of Ecology
Muckleshoot Indian Tribe
U.S. Army Corps of Engineers
Washington State Department of Fish and Wildlife
Issaquah Development Services Department
Issaquah Public Works Engineering and Parks and Recreation Departments



Notice of Application

Project Name: GILMAN POINT

Public Comment Period Ends: April 30, 2015

Application: March 16, 2015
Application Complete: March 26, 2015
Notice of Application: April 15, 2015

(See Public Comment below for more information)

PROJECT INFORMATION

File Number(s): ASDP15-00002

Project Description: Construction of (1) 4-story, fully enclosed self-storage building of appx. 88,000 sq. ft. and (1) tire shop of appx. 12,000 sq. ft. with (7) service bays and a retail/office space. Site improvements consist of grading and associated improvements including 44 parking spaces, landscaping, utility improvements and buffer enhancements across a total site area of 82,134 sq. ft (1.89 acres) **(See attached plans)**

Project Location: 160 NW GILMAN BLVD **(See Map)**

Size of Subject Area in Acres: 1.89 **Sq. Ft.:** 82,134

Applicant: Bob Power, 165 NW Juniper ST, Suite 100, Issaquah, WA 98027
425-837-9720 bpower@seaconllc.com

Required Permits: SDP, SEPA

Required Permits, Not Part of this Application: Building Permit, Site Work Permit, Landscape Permit, AAS

Required Studies: Wetlands, Traffic

Existing Environmental Documents Relevant to this Application: Environmental Checklist, Geotechnical, Drainage

REGULATORY INFORMATION

Zoning: MU – Mixed Use

Comprehensive Plan Designation: Mixed Use

Consistent With Comprehensive Plan: Yes

Preliminary Determination of the Development Regulations that will be used for Project Mitigation and Consistency: **Central Issaquah Development & Design Standards, Central Issaquah Plan, Comprehensive Plan** (Online at: [issaquahwa.gov/codes and plans](http://issaquahwa.gov/codes%20and%20plans))

PUBLIC COMMENT

The application, with full size plans, is available for review at the Permit Center, City Hall Northwest, 1775 12th Avenue NW (next to Holiday Inn and behind Lowe's), 9 am – 5 pm. An appointment with the Project Planner is suggested.

Although comments are accepted up until the decision is issued, submittal of comments during the Public Comment Period will ensure comments are considered prior to issuing a decision and will allow staff and/or the applicant to address comments as early in the process as possible.

Written comments are due by 5:00 pm on the Public Comment Period date noted above to:

Development Services Department
P.O. Box 1307, Issaquah, WA 98027

Or by e-mail to the Project Planner noted below.

To receive further public notices on this project please provide your name, address, and e-mail to the Project Planner and request to become a Party of Record.

Notice is required to be provided to property owners within 300 feet of the site and to Parties of Record. Please share this notice with others in your neighborhood who may be interested in this project. Property owner, Mortgagee, Lien Holder, Vendor, Seller, etc., please share this notice with tenants and others who may be interested in this project.

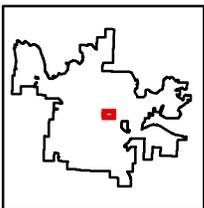
CITY CONTACT INFORMATION

Project Planner: Mike Martin
Phone Number: 425-837-3103
E-Mail: mikem@issaquahwa.gov

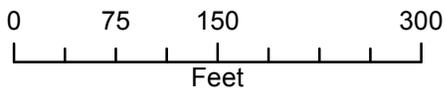
Development Services Department:
Phone Number: 425-837-3100
E-Mail: DSD@issaquahwa.gov



City of Issaquah, WA GIS



ASDP15-00002: Vicinity Map - Gilman Point - 160 NW Gilman Blvd.



DISCLAIMER: These maps and other data are for informational purposes and have not been prepared for, nor are they suitable for legal, surveying, or engineering purposes. Users of this information should review or consult the primary data and information sources to ascertain the usability of the information. The City of Issaquah makes no warranty or guarantee as to the content, accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained herein.



4/14/2015



Copyright © 2015 Magellan Architects PLLC
 Architect for this project are instruments of the
 this project service for use solely with respect to
 to transmitted in any form or by any means,
 electronic or mechanical (including photocopying,
 without the express written approval of Magellan
 may be made without prior written permission. The
 purchaser to use this set of plans for the
 additional set(s) of plans shall not entitle the
 purchaser to construct more than one building.

Magellan
 ARCHITECTS
 8383 158th Avenue Northeast, Suite 280
 Redmond, Washington 98052
 Tel (425) 885-4300 Fax (425) 885-4303
 www.magellanarchitects.com
 Copyright © 2015 Magellan Architects PLLC

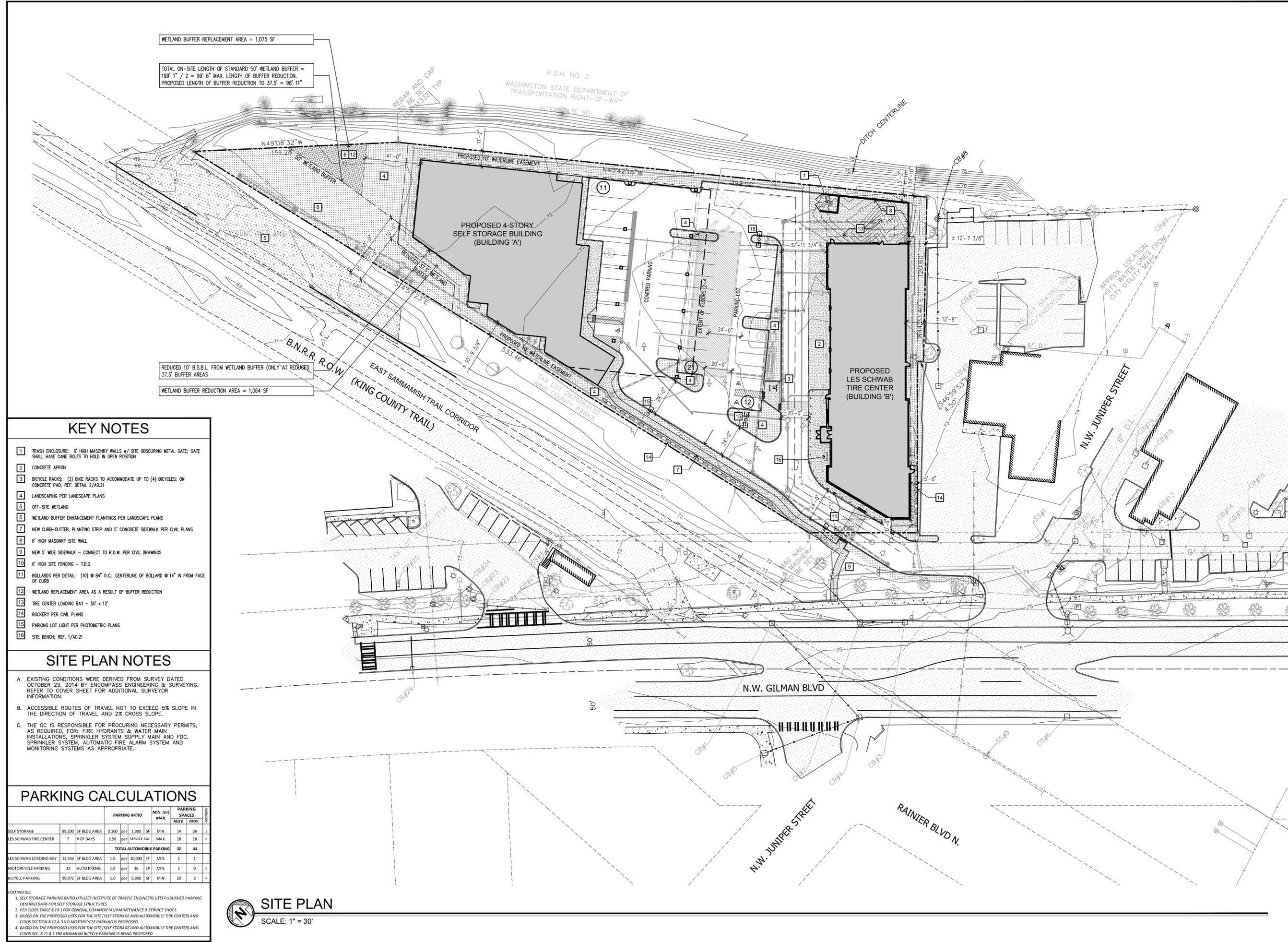
GILMAN POINT
 SELF-STORAGE and LES SCHWAB TIRE CENTER
 160 NW GILMAN BOULEVARD
 ISSAQUAH, WASHINGTON 98027

REVISIONS		
NO.	DATE	BY
1		
2		
3		
4		
5		
6		

ISSUE DATES
 DESIGN APPROVAL:
 PERMIT SUBMITTAL:
 PERMIT RECEIVED:
 BID DOCS:
 CONSTR. DOCS:

24"x36" SCALE: 1" = 30'
 PLOT DATE: 03-10-2015
 CAD FILE: 14-140_A02_ASDP
 JOB NUMBER: 14-140
 CHECKED:
 DRAWN: DYM
 STATUS: ISSUE FOR ASDP

ARCHITECTURAL SITE PLAN
A0.2



WETLAND BUFFER REPLACEMENT AREA = 1,075 SF
 TOTAL ON-SITE LENGTH OF STANDARD 50' WETLAND BUFFER = 199' 1" / 2 = 99' 6" MAX. LENGTH OF BUFFER REDUCTION, PROPOSED LENGTH OF BUFFER REDUCTION TO 37.5' = 98' 11"

REDUCED 10' B.S.B.L. FROM WETLAND BUFFER (ONLY AT REDUCED 37.5' BUFFER AREAS)
 WETLAND BUFFER REDUCTION AREA = 1,064 SF

KEY NOTES

- 1 TRASH ENCLOSURE: 6' HIGH MASONRY WALLS w/ SITE OBTUSCURING METAL GATE; GATE SHALL HAVE CANE BOLTS TO HOLD IN OPEN POSITION
- 2 CONCRETE APRON
- 3 BICYCLE RACKS: (2) BIKE RACKS TO ACCOMMODATE UP TO (4) BICYCLES; ON CONCRETE PAD; REF. DETAIL 2/A0.21
- 4 LANDSCAPING PER LANDSCAPE PLANS
- 5 OFF-SITE WETLAND
- 6 WETLAND BUFFER ENHANCEMENT PLANTINGS PER LANDSCAPE PLANS
- 7 NEW CURB-GUTTER, PLANTING STRIP AND 5' CONCRETE SIDEWALK PER CIVIL PLANS
- 8 6' HIGH MASONRY SITE WALL
- 9 NEW 5' WIDE SIDEWALK - CONNECT TO R.O.W. PER CIVIL DRAWINGS
- 10 6' HIGH SITE FENCING - T.B.D.
- 11 BOLLARDS PER DETAIL: (10) Ø 84" O.C.; CENTERLINE OF BOLLARD Ø 14" IN FROM FACE OF CURB
- 12 WETLAND REPLACEMENT AREA AS A RESULT OF BUFFER REDUCTION
- 13 TIRE CENTER LOADING BAY - 50' x 12'
- 14 ROCKERY PER CIVIL PLANS
- 15 PARKING LOT LIGHT PER PHOTOMETRIC PLANS
- 16 SITE BENCH; REF. 1/A0.21

SITE PLAN NOTES

- A. EXISTING CONDITIONS WERE DERIVED FROM SURVEY DATED OCTOBER 29, 2014 BY ENCOMPASS ENGINEERING & SURVEYING. REFER TO COVER SHEET FOR ADDITIONAL SURVEYOR INFORMATION.
- B. ACCESSIBLE ROUTES OF TRAVEL NOT TO EXCEED 5% SLOPE IN THE DIRECTION OF TRAVEL AND 2% CROSS SLOPE.
- C. THE GC IS RESPONSIBLE FOR PROCURING NECESSARY PERMITS, AS REQUIRED, FOR: FIRE HYDRANTS & WATER MAIN INSTALLATIONS, SPRINKLER SYSTEM SUPPLY MAIN AND FDC, SPRINKLER SYSTEM, AUTOMATIC FIRE ALARM SYSTEM AND MONITORING SYSTEMS AS APPROPRIATE.

PARKING CALCULATIONS

		PARKING RATIO	MIN. (or) MAX.	REQ'D	PROV.	PARKING SPACES	COMMENTS
SELF STORAGE	89,200 SF BLDG AREA	0.160 per 1,000 SF	MIN.	14	26		
LES SCHWAB TIRE CENTER	7 # OF BAYS	2.50 per SERVICE BAY	MAX.	18	18		
TOTAL AUTOMOBILE PARKING				32	44		
LES SCHWAB LOADING BAY	12,556 SF BLDG AREA	1.0 per 10,000 SF	MIN.	1	1		
MOTORCYCLE PARKING	32 AUTO PRKNG	1.0 per 36 SP	MIN.	1	0		
BICYCLE PARKING	99,972 SF BLDG AREA	1.0 per 5,000 SF	MIN.	20	2		

FOOTNOTES:
 1. SELF STORAGE PARKING RATIO UTILIZES INSTITUTE OF TRAFFIC ENGINEERS (ITE) PUBLISHED PARKING DEMAND DATA FOR SELF STORAGE STRUCTURES
 2. PER CIDDS TABLE 8.10-1 FOR GENERAL COMMERCIAL/MAINTENANCE & SERVICE SHOPS
 3. BASED ON THE PROPOSED USES FOR THE SITE (SELF STORAGE AND AUTOMOBILE TIRE CENTER) AND CIDDS SECTION 8.12.A.3 NO MOTORCYCLE PARKING IS PROPOSED.
 4. BASED ON THE PROPOSED USES FOR THE SITE (SELF STORAGE AND AUTOMOBILE TIRE CENTER) AND CIDDS SEC. 8.11.B.1 THE MINIMUM BICYCLE PARKING IS BEING PROPOSED.

SITE PLAN
 SCALE: 1" = 30'

ADMINISTRATIVE SITE DEVELOPMENT PERMIT (ASDP) SUBMITTAL

GILMAN POINT

SELF-STORAGE and LES SCHWAB TIRE CENTER

160 NW GILMAN BOULEVARD

ISSAQUAH, WASHINGTON 98027

S.E. 1/4 OF N.E. 1/4 OF SECTION 28, T. 24 N., R. 06 E., W.M.



Copyright © 2015, Magellan Associates PLLC. All rights reserved. This document is the property of Magellan Associates PLLC and is intended for the use of the purchaser. No part of this document may be reproduced, stored in a retrieval system, or transmitted in any form or by any means, electronic or mechanical, including photocopying, recording, or by any information storage and retrieval system, without the express written approval of Magellan Associates PLLC. The purchaser acknowledges that the use of this document is subject to the terms and conditions set forth in the purchase order and that the purchaser is responsible for obtaining all necessary permits and approvals from the appropriate authorities. The purchaser agrees to hold Magellan Associates PLLC harmless from and against all claims, damages, and expenses, including reasonable attorneys' fees, arising out of or from the use of this document, whether or not such claims, damages, and expenses are caused in whole or in part by the negligence, active or passive, of Magellan Associates PLLC. The purchaser's use of this document is limited to the specific project and site identified in the purchase order and does not constitute a contract. The purchaser is advised that this document is not intended to constitute a contract and that the purchaser should consult with an attorney for more information. The purchaser's use of this document is limited to the specific project and site identified in the purchase order and does not constitute a contract. The purchaser is advised that this document is not intended to constitute a contract and that the purchaser should consult with an attorney for more information.

Magellan
ARCHITECTS
8883 158th Avenue Northeast, Suite 280
Redmond, Washington 98052
Tel (425) 885-4300 Fax (425) 885-4303
www.magellanarchitects.com
Copyright © 2015 Magellan Associates PLLC

GILMAN POINT
SELF-STORAGE and LES SCHWAB TIRE CENTER
160 NW GILMAN BOULEVARD
ISSAQUAH, WASHINGTON 98027

VICINITY MAP



PROJECT TEAM

APPLICANTS:
GILMAN POINT, LLC
165 NE JUNIPER ST, STE 100
ISSAQUAH, WA 98027
TEL: 425-837-9720
MOB: 425-677-2111
Contact: BOB POWER
BPOWER@SEACONLLC.COM

LES SCHWAB TIRES
20900 COOLEY ROAD
BEND, OR 97701
TEL: 541-416-5238
MOB: 559-974-0517
Contact: MATT HANNIGAN
MATTHEW.T.HANNIGAN@LESSCHWAB.COM

CIVIL ENGINEER:
PACLAND
11400 SE 8th ST, STE 345
BELLEVUE, WA 98004
TEL: 425-453-9501 x 1511
MOB: 206-251-2842
Contact: JENELLE TAFLIN
JTAFLIN@PACLAND.COM

LANDSCAPE ARCHITECT:
ALTMANN-OLIVER
PO BOX 578
CARNATION, WA 98014
TEL: 425-333-4535
MOB: 206-251-2842
Contact: SIMONE OLIVER
SIMONE@ALTOLIVER.COM

ARCHITECT (SITE & STORAGE BLDG):
MAGELLAN ARCHITECTS
8383 158th AVE NE, STE 280
REDMOND, WA 98052
TEL: 425-885-4300
MOB: 206-617-3716
Contact: DIRK McCULLOCH
DIRK@MAGELLANARCHITECTS.COM

ARCHITECT (LES SCHWAB BLDG):
NRG FIRE CONSULTING
8050 N. PALM AVE, STE 300
FRESNO, CA 93711
TEL: 303-770-8884
MOB: 559-974-0517
Contact: TERRA MORTENSEN
TERRAMORTENSEN@GALLOWAYUS.COM

CRITICAL AREAS CONSULTANT:
ALTMANN-OLIVER
PO BOX 578
CARNATION, WA 98014
TEL: 425-333-4535
MOB: 206-251-2842
Contact: JOHN ALTMANN
JOHN@ALTOLIVER.COM

GEOTECHNICAL ENGINEER:
KRAZAN & ASSOCIATES
922 VALLEY AVE NW, STE 101
PUYALLUP, WA 98371
TEL: 253-939-2500
MOB: 253-355-5638
Contact: JAY MARTIN
JAYMARTIN@KRAZAN.COM

TRAFFIC ENGINEER:
TSI TRANSPORTATION SOLUTIONS
8250 165th AVE NE, STE 100
REDMOND, WA 98052
TEL: 425-833-4134 x 114
MOB: 206-612-5658
Contact: DAVID MARKLEY
DAVIDM@TSINW.COM

FIRE PROTECTION CONSULTANT:
NRG FIRE CONSULTING
650 NW 54th ST
SEATTLE, WA 98107
TEL: 206-789-0165
MOB: 206-354-3310
Contact: SEAN PISONI
NRGFIRECONSULT@GMAIL.COM

PROJECT & SITE DATA

SITE ADDRESS: 160 NW GILMAN BOULEVARD
ISSAQUAH, WA 98027

LEGAL DESCRIPTION:
THAT PORTION OF LOTS 1 THROUGH 4, INCLUSIVE, BLOCK 6, HERBERT S. UPPER'S FIRST ADDITION TO ISSAQUAH, ACCORDING TO THE PLAT THEREOF RECORDED IN VOLUME 12 OF PLATS, PAGE 13, IN KING COUNTY, WASHINGTON, DESCRIBED AS FOLLOWS:
BEGINNING AT THE NORTHWEST CORNER OF SAID BLOCK 6;
THENCE SOUTH 16°41'00" EAST 414.26 FEET TO THE NORTHERLY MARGIN OF STATE HIGHWAY NO.2;
THENCE SOUTH 48°25'00" EAST ALONG SAID NORTHERLY MARGIN 80 FEET TO THE MOST WESTERLY POINT OF A TRACT OF LAND AS DESCRIBED IN DEED RECORDED UNDER RECORDING NUMBER 890411132;
THENCE NORTH 41°35'00" EAST 125.7 FEET;
THENCE SOUTH 48°48'30" EAST 4.5 FEET;
THENCE NORTH 42°45'03" EAST 120.98 FEET TO THE SOUTHWESTERLY MARGIN OF PRIMARY STATE HIGHWAY NO.2 (SR90) AS CONDEMNED IN KING COUNTY SUPERIOR COURT CAUSE NUMBER 691461;
THENCE NORTHWESTERLY ALONG SAID SOUTHWESTERLY MARGIN TO THE NORTH LINE OF LOT 1 IN SAID BLOCK 6;
THENCE WEST ALONG SAID NORTH LINE TO THE POINT OF BEGINNING;
TOGETHER WITH THAT PORTION OF THE NORTHEAST QUARTER OF THE NORTHEAST QUARTER OF SECTION 28, TOWNSHIP 24 NORTH, RANGE 6 EAST, W.M., IN KING COUNTY, WASHINGTON, LYING NORTH OF LOCUST STREET, EASTERLY OF NORTHERN PACIFIC RAILROAD RIGHT-OF-WAY AND SOUTHERLY OF PRIMARY STATE HIGHWAY NO.2 (SR90);
AND TOGETHER WITH THAT PORTION OF LOCUST STREET LYING SOUTHERLY OF THE ABOVE DESCRIBED PORTION OF SECTION 28, AND NORTHERLY OF THE ABOVE DESCRIBED PORTION OF BLOCK 6, HERBERT S. UPPER'S FIRST ADDITION TO ISSAQUAH.

PARCEL NUMBER: 884350-0238
JURISDICTION: CITY OF ISSAQUAH
WATER & SEWER DISTRICT: CITY OF ISSAQUAH
SECTION/TOWNSHIP/RANGE: NE-28-24-06
PROPERTY ZONING: MU (MIXED USE)
OVERLAY: GILMAN
OCCUPANCY CLASSIFICATION: S-2 (SELF-STORAGE)
S-1/M (TIRE CENTER)

SITE AREA CALCULATIONS
GROSS SITE AREA: 82,134 S.F. (1.89 ACRES)
BASE SITE AREA: 82,134 S.F.
DEVELOPABLE SITE AREA: 74,896 S.F.
BASE F.A.R. @ 1.25: 93,620 S.F.
PROPOSED BLDG AREA (GFA): 99,972 S.F. (F.A.R.=1.33)
ADDITIONAL BUILDING AREA SUBJECT TO CDD'S SEC. 5.0 DENSITY BONUS PROGRAM: 6,352 S.F.
PROPOSED IMPERVIOUS AREA: 61,858 S.F. (75.3%)
PROPOSED PERVIOUS AREA: 20,276 S.F.

LANDSCAPE REQUIREMENTS
PARKING LOT LANDSCAPING:
10% OF PARKING AREA x 19,005 SF + EDGE LANDSCAPING: 1,900 2,117 SF
1 TREE PER 6 STALLS x 44 STALLS: 7.33 7 TREES

GENERAL LANDSCAPING:
TYPE II PERIMETER LANDSCAPING

MINIMUM TREE DENSITY:
4 SIGNIFICANT TREES (OR CALIPER EQUIV)
PER 5,000 SF DEVELOPABLE AREA x 74,896 SF: 60 XX TREES

REFER TO LANDSCAPE PLANS FOR TREE REPLACEMENT AND LANDSCAPE AREAS

SHEET INDEX

GENERAL:
A0.1 COVER SHEET AND PROJECT INFORMATION
A0.2 ARCHITECTURAL SITE PLAN
A0.21 SITE AMENITIES, CONTEXT PHOTOS AND DESIGN NARRATIVE
A0.3 PHOTOMETRIC SITE PLAN
A0.4 PHOTOMETRIC DETAILS

SURVEY:
1 of 1 ALTA/ASCM LAND TITLE SURVEY

CIVIL:
CVR-1 COVER SHEET
C-1.0 CIVIL SITE PLAN
C-2.0 CIVIL CIRCULATION PLAN
C-3.0 CIVIL FIRE AND RESCUE PLAN
C-4.0 STORMWATER AND GRADING PLAN
C-5.0 WATER, SEWER AND OTHER UTILITY PLAN
C-6.0 OFF-SITE IMPROVEMENT PLAN

LANDSCAPE:
L1.1 PRELIMINARY TREE PLAN
L1.2 PRELIMINARY LANDSCAPE PLAN
W1.1 PRELIMINARY WETLAND BUFFER MITIGATION PLAN

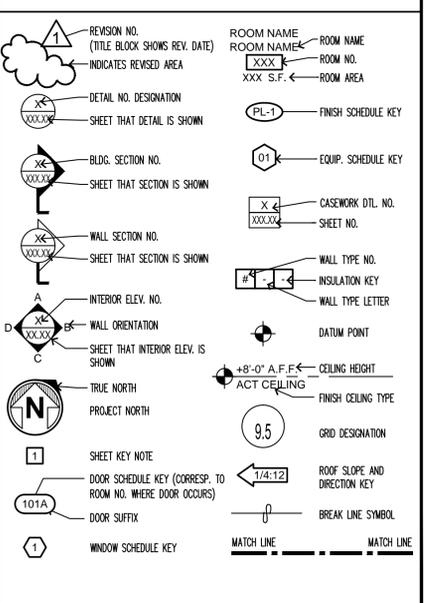
ARCHITECTURAL - SELF STORAGE BUILDING:
AA2.1 STORAGE BUILDING FIRST FLOOR PLAN
AA2.2 STORAGE BUILDING SECOND FLOOR PLAN
AA2.3 STORAGE BUILDING THIRD FLOOR PLAN
AA2.4 STORAGE BUILDING FOURTH FLOOR PLAN
AA2.5 STORAGE BUILDING ROOF PLAN
AA3.1 STORAGE BUILDING EXTERIOR ELEVATIONS & FINISHES
AA3.2 STORAGE BUILDING EXTERIOR ELEVATIONS & FINISHES
AA3.3 STORAGE BUILDING COLOR PERSPECTIVES

ARCHITECTURAL - LES SCHWAB TIRE CENTER:
AB2.1 LES SCHWAB FLOOR PLANS
AB3.1 EXTERIOR ELEVATIONS & FINISH SCHEDULE
AB3.2 COLOR PERSPECTIVES
AB3.3 LES SCHWAB COLOR & MATERIALS BOARD

ARCHITECTURAL ABBREVIATIONS

SYMBOLS:	AT AND ANGLE CENTER LINE COPYRIGHT DEGREE DIAMETER EQUAL NUMBER GREATER THAN LESS THAN PERCENTAGE PLUS/MINUS PLATE	COL CONC CONT CONSTR CONTR CPT CT DF DS DWC EA ELEV ELEC EQ EQS EJ EXP EXT F.C.I.C.	COLUMN CONCRETE CONTINUOUS CONSTRUCTION CONTRACTOR CERAMIC TILE DRAINING EACH ELEVATION, ELEVATOR ELECTRICAL EXPANSION JOINT EQUAL EXISTING EXPANSION JOINT EXPANSION EXTERIOR FURNISH BY CONTRACTOR INSTALL BY CONTRACTOR FOUNDATION FINISH FLOOR FURNISHED BY OWNER INSTALLED BY OWNER FIRE EXTINGUISHER CABINET FURNISHED BY OWNER INSTALLED BY CONTRACTOR FROST PROOF HOSE BIBB FIBER REINFORCED PANEL(S) FOOTING GAUZE GALVANIZED GENERAL CONTRACTOR GYPSUM LATH & PLASTER GYPSUM WALLBOARD HIGH, HEIGHT HOSE BIBB HOLLOW METAL HORIZ HEIGHT HEATER INSULATION	JT/JTS L LAM L.F. LW LVL MAS MAX M.B.S. MCT MECH MEZZ MTL MFG MFR MNL MNM MNSC M.O. M.R. MTD MTC NIC NOM NTS OC O.H. OPP O/P P PERP PL P-LAM PLUMB PML PND PRV PNT PT QTR R RMB RL R.C'D REF	JOINT, JOINTS LONG, LENGTH LAMINATE, LAMINATED LINEAR FOOT, LINEAL FOOT LEVEL MASONRY MAXIMUM METAL BUILDING SUPPLIER MARBLE MECHANICAL MEZZANINE METAL MANUFACTURING MANUFACTURER MANHOLE MINIMUM MISCELLANEOUS MASONRY OPENING MOISTURE RESISTANT MOUNTING NOMINAL NOT IN CONTRACT NOT TO SCALE ON CENTER OVERHEAD OPPOSITE O/P PERPENDICULAR POINT QUARRY TILE QUARTER RADIUS RAOF, RAIN RAIN LEADER RECEIVED REFRIGERATOR, REFERENCE	REINFORCING REQUIRED RUBBER FLOORING RESTROOM ROUGH OPENING PUBLIC RIGHT OF WAY SELF ADHESIVE MEMBRANE SCHEDULE SOLID CORE SQUARE FOOTAGE SAFETY GLASS SHEET SIMILAR SPECIFICATION SQUARE STAINLESS STEEL SANITARY SEWER STANDARD STEEL STRUCTURE, STRUCTURAL SUSP SYS T TAB TEL TEMP TAG TEMPERED GLASS T.O. TYP UTIL U.O.N. VCT VERT W/ W/O W-MAT WR WRB WT WMM WVF Y.C.
-----------------	---	---	---	--	---	--

SYMBOLS LEGEND



PARKING

STANDARD SIZE	PARKING RATIO	# OF BAYS	MIN. (or) MAX.	PARKING SPACES	
				REQ'D	PROV.
SELF STORAGE	89,200 SF BLDG AREA	0.160	per 1,000 SF	MIN.	14 26 2
LES SCHWAB TIRE CENTER	7 # OF BAYS	2.50	per SERVICE BAY	MAX.	18 18 2
TOTAL AUTOMOBILE PARKING				32	44
LES SCHWAB LOADING BAY	12,556 SF BLDG AREA	1.0	per 10,000 SF	MIN.	1 1 1
MOTORCYCLE PARKING	32 AUTO PARKING	1.0	per 36 SP	MIN.	1 0 1
BICYCLE PARKING	99,972 SF BLDG AREA	1.0	per 5,000 SF	MIN.	20 2 4

FOOTNOTES:
1. SELF STORAGE PARKING RATIO UTILIZES INSTITUTE OF TRAFFIC ENGINEERS (ITE) PUBLISHED PARKING DEMAND DATA FOR SELF STORAGE STRUCTURES
2. PER CDD'S TABLE 8.10-1 FOR GENERAL COMMERCIAL/MAINTENANCE & SERVICE SHOPS
3. BASED ON THE PROPOSED USES FOR THE SITE (SELF STORAGE AND AUTOMOBILE TIRE CENTER) AND CDD'S SECTION 8.2.2.3 NO MOTORCYCLE PARKING IS PROPOSED
4. BASED ON THE PROPOSED USES FOR THE SITE (SELF STORAGE AND AUTOMOBILE TIRE CENTER) AND CDD'S SEC. 8.11.1 THE MINIMUM BICYCLE PARKING IS BEING PROPOSED.

PARKING SPACE TYPE	SIZE (MINIMUM)		REQ'D	PROV.
	STANDARD	COMPACT		
STANDARD	18.5' x 9'	15' x 8'	1	37
COMPACT	18.5' x 9' w/ ACCESS AISLE	15' x 8'	1	4
VAN ACCESSIBLE	18.5' x 9' w/ ACCESS AISLE	18.5' x 9' w/ ACCESS AISLE	1	3
ACCESSIBLE	18.5' x 9' w/ ACCESS AISLE	18.5' x 9' w/ ACCESS AISLE	1	2
LOADING SPACE	25' x 10' (TYPE 'A')		1	3
TOTALS			32	44

SCOPE OF WORK

THE PROJECT PROPOSES CONSTRUCTION OF (1) 4-STORY, FULLY ENCLOSED SELF-STORAGE BUILDING OF APPROXIMATELY 88,000 SQUARE FEET AND (1) TIRE SHOP OF APPROXIMATELY 12,000 SQUARE FEET WITH (7) SERVICE BAYS AND A RETAIL/OFFICE AREA. SITE IMPROVEMENTS CONSIST OF GRADING AND ASSOCIATED IMPROVEMENTS INCLUDING 44 PARKING SPACES, LANDSCAPING, UTILITY IMPROVEMENTS AND BUFFER ENHANCEMENTS ACROSS A TOTAL SITE AREA OF 82,134 SQUARE FEET (1.89 ACRES).

REQUIRED PERMITS

- ADMINISTRATIVE SITE DEVELOPMENT PERMIT - LEVEL 2 MAJOR
- CLEARING AND GRADING PERMIT
- UTILITY AND ROW PERMITS
- BUILDING PERMIT (FOR EACH OF TWO BUILDINGS)
- PLUMBING AND MECHANICAL PERMITS (FOR EACH OF TWO BUILDINGS)
- ELECTRICAL PERMITS (FOR EACH OF TWO BUILDINGS)
- FIRE SPRINKLER PERMITS (FOR EACH OF TWO BUILDINGS)
- FIRE ALARM PERMITS AS APPROPRIATE

REVISIONS		
NO.	DATE	BY
1		
2		
3		
4		
5		

ISSUE DATES
DESIGN APPROVAL:
PERMIT SUBMITTAL:
PERMIT RECEIVED:
BID DOCS:
CONSTR. DOCS:

24"x36" SCALE: AS NOTED
PLOT DATE: 03-10-2015
CAD FILE: 14-140_A01_ASDP
JOB NUMBER: 14-140
CHECKED:
DRAWN: DYM
STATUS: ISSUE FOR ASDP

COVER SHEET & PROJECT DATA
A0.1



Copyright © 2015 Magellan Architects PLLC
 Architect for this project are instruments of the
 this project service for use solely with respect to
 to transmitted in any form or by any means,
 electronic or mechanical (including photocopying,
 without the express written approval of Magellan
 may be made without prior written permission. The
 purchaser to use this set of plans for the
 additional set(s) of plans shall not entitle the
 purchaser to construct more than one building.

Magellan
 ARCHITECTS
 8383 158th Avenue Northeast, Suite 280
 Redmond, Washington 98052
 Tel (425) 885-4300 Fax (425) 885-4303
 www.magellanarchitects.com
 Copyright © 2015 Magellan Architects PLLC

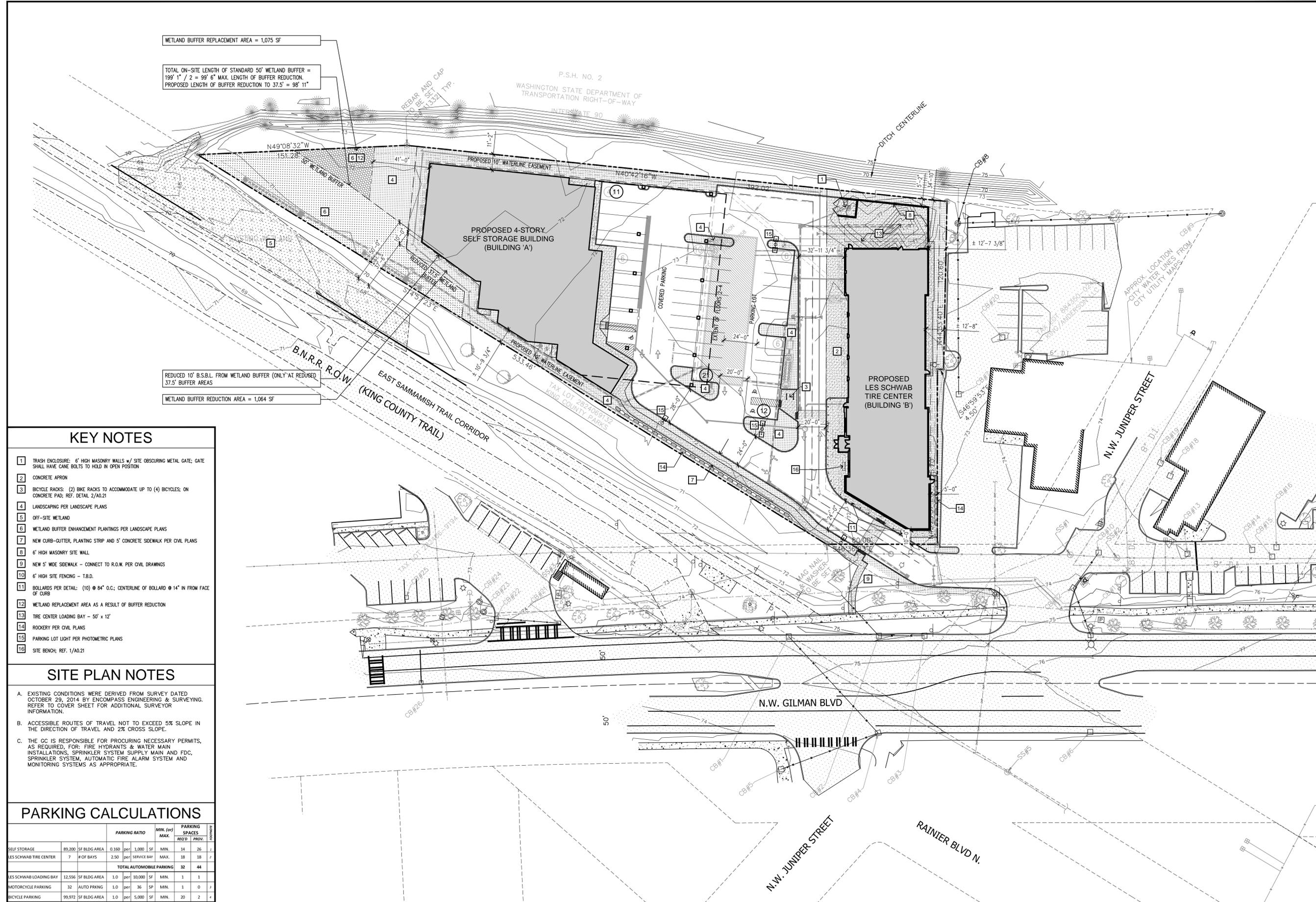
GILMAN POINT
 SELF-STORAGE and LES SCHWAB TIRE CENTER
 160 NW GILMAN BOULEVARD
 ISSAQUAH, WASHINGTON 98027

REVISIONS		
NO.	DATE	BY
1		
2		
3		
4		
5		
6		

ISSUE DATES
 DESIGN APPROVAL:
 PERMIT SUBMITTAL:
 PERMIT RECEIVED:
 BID DOCS:
 CONSTR. DOCS:

24"x36" SCALE: 1" = 30'
 PLOT DATE: 03-10-2015
 CAD FILE: 14-140_A02_ASDP
 JOB NUMBER: 14-140
 CHECKED:
 DRAWN: DYM
 STATUS: ISSUE FOR ASDP

ARCHITECTURAL SITE PLAN
A0.2



KEY NOTES

- 1 TRASH ENCLOSURE: 6' HIGH MASONRY WALLS w/ SITE OBTUSCURING METAL GATE; GATE SHALL HAVE CAME BOLTS TO HOLD IN OPEN POSITION
- 2 CONCRETE APRON
- 3 BICYCLE RACKS: (2) BIKE RACKS TO ACCOMMODATE UP TO (4) BICYCLES; ON CONCRETE PAD; REF. DETAIL 2/A0.21
- 4 LANDSCAPING PER LANDSCAPE PLANS
- 5 OFF-SITE WETLAND
- 6 WETLAND BUFFER ENHANCEMENT PLANTINGS PER LANDSCAPE PLANS
- 7 NEW CURB-GUTTER, PLANTING STRIP AND 5' CONCRETE SIDEWALK PER CIVIL PLANS
- 8 6' HIGH MASONRY SITE WALL
- 9 NEW 5' WIDE SIDEWALK - CONNECT TO R.O.W. PER CIVIL DRAWINGS
- 10 6' HIGH SITE FENCING - T.B.D.
- 11 BOLLARDS PER DETAIL: (10) Ø 84" O.C.; CENTERLINE OF BOLLARD Ø 14" IN FROM FACE OF CURB
- 12 WETLAND REPLACEMENT AREA AS A RESULT OF BUFFER REDUCTION
- 13 TIRE CENTER LOADING BAY - 50' x 12'
- 14 ROCKERY PER CIVIL PLANS
- 15 PARKING LOT LIGHT PER PHOTOMETRIC PLANS
- 16 SITE BENCH; REF. 1/A0.21

SITE PLAN NOTES

- A. EXISTING CONDITIONS WERE DERIVED FROM SURVEY DATED OCTOBER 29, 2014 BY ENCOMPASS ENGINEERING & SURVEYING. REFER TO COVER SHEET FOR ADDITIONAL SURVEYOR INFORMATION.
- B. ACCESSIBLE ROUTES OF TRAVEL NOT TO EXCEED 5% SLOPE IN THE DIRECTION OF TRAVEL AND 2% CROSS SLOPE.
- C. THE GC IS RESPONSIBLE FOR PROCURING NECESSARY PERMITS, AS REQUIRED, FOR: FIRE HYDRANTS & WATER MAIN INSTALLATIONS, SPRINKLER SYSTEM SUPPLY MAIN AND FDC, SPRINKLER SYSTEM, AUTOMATIC FIRE ALARM SYSTEM AND MONITORING SYSTEMS AS APPROPRIATE.

PARKING CALCULATIONS

			PARKING RATIO	MIN. (sq) MAX.	REQ'D	PROV.	PARKING SPACES	COMMENTS	
SELF STORAGE	89,200	SF BLDG AREA	0.160	per 1,000 SF	MIN.	14	26	1	
LES SCHWAB TIRE CENTER	7	# OF BAYS	2.50	per SERVICE BAY	MAX.	18	18	1	
TOTAL AUTOMOBILE PARKING							32	44	
LES SCHWAB LOADING BAY	12,556	SF BLDG AREA	1.0	per 10,000 SF	MIN.	1	1		
MOTORCYCLE PARKING	32	AUTO PRKNG	1.0	per 36 SP	MIN.	1	0	1	
BICYCLE PARKING	99,972	SF BLDG AREA	1.0	per 5,000 SF	MIN.	20	2	1	

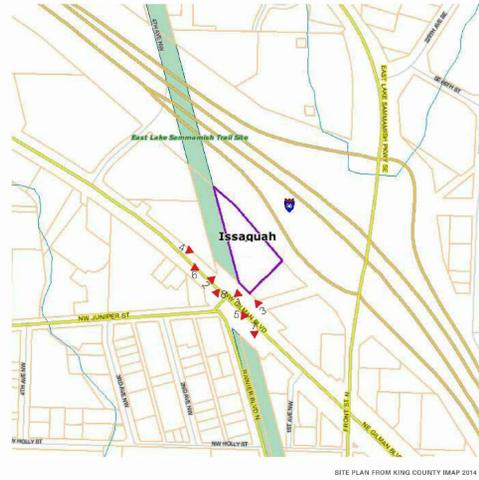
FOOTNOTES:
 1. SELF STORAGE PARKING RATIO UTILIZES INSTITUTE OF TRAFFIC ENGINEERS (ITE) PUBLISHED PARKING DEMAND DATA FOR SELF STORAGE STRUCTURES
 2. PER CIDDS TABLE 8.10-1 FOR GENERAL COMMERCIAL/MAINTENANCE & SERVICE SHOPS
 3. BASED ON THE PROPOSED USES FOR THE SITE (SELF STORAGE AND AUTOMOBILE TIRE CENTER) AND CIDDS SECTION 8.12.A.3 NO MOTORCYCLE PARKING IS PROPOSED.
 4. BASED ON THE PROPOSED USES FOR THE SITE (SELF STORAGE AND AUTOMOBILE TIRE CENTER) AND CIDDS SEC. 8.11.B.1 THE MINIMUM BICYCLE PARKING IS BEING PROPOSED.

SITE PLAN
 SCALE: 1" = 30'



CURRENT SITE

SITE CONTEXT



SITE PLAN FROM KING COUNTY MAP 2014

CONTEXT IMAGES

GILMAN POINT

160 NW GILMAN BLVD, ISSAQUAH, WA 98027

LES SCHWAB BUILDING

1. Les Schwab Building: The CIDDS requires a minimum building frontage of 60% (CIDDS 11.3.G) which means that there's building at the 0 ft setback line, i.e. property line (CIDDS Table 4.4). A setback of up to 10 ft is allowed in some circumstances, but you've exceeded that 10 ft max and may not meet the standards for when a setback is appropriate. While the plaza and pedestrian table are good tools, we have to start with 'are you meeting the requirement' and 'is the adjustment consistent with vision and intent of the rules (CIDDS 1.1.E)'? As we understand it, this is a preference from Les Schwab regarding their building design.

Magellan Architects (MA): The geometric constraints of the site are very challenging to meet the letter of the code. Based on subsequent discussions with City staff and analysis of the CIDDS requirements the Les Schwab Tire Center building is now providing the maximum feasible frontage at the 10' build-to line with additional frontage façade at a slight angle based on providing minimum reasonable vehicular and pedestrian access to the site. Based on discussions with City staff and further analysis of the CIDDS requirements it is our understanding that the plaza requirement does not apply to this project and those elements have been removed from the proposal.

LES SCHWAB ENTRY

2. Les Schwab entry: The entry is facing the parking lot not the adjacent Circulation Facility, i.e. Gilman Blvd. Again this is a preference of Les Schwab's not something that the site precludes which would not support an adjustment of standards.

Magellan Architects (MA): An entry has been added to the frontage façade of the Les Schwab building. Additionally, a clear pedestrian connection from the sidewalk along the NW Gilman Boulevard right-of-way to the site and, subsequently, to the Les Schwab building have been provided.

STORAGE FACILITY ENTRY

3. Storage facility entry: the change to the entry and provision of a walkway are good ways to make this more compliant. The walkway and adjacent drive must meet a Circulation Facility standard. To use this configuration, the walk (and possibly the auto area) will need to meet a standard in CIDDS Chap 6.

Magellan Architects (MA): Sidewalk and planter strip have been added per in an effort to configure the circulation route to the self-storage facility to be compliant with CIDDS guidelines. Additional provisions towards conforming to Circulation Facility standards include the modulation and narrowing, where appropriate while maintaining appropriate vehicular circulation, of drive-aisle widths and the inclusion of lighting and appropriate landscaping.

PARKING

4. Parking: it appears you may have used a parking generation code rather than the CIDDS Chap 8. Please provide calcs using CIDDS showing both the min and max parking allowed and where you fall in that range.

Magellan Architects (MA): The parking requirements have been updated and are a combination of CIDDS standards and published Institute of Traffic Engineers (ITE) parking ratios. Based on conversations with City staff and interpretation of the CIDDS it is our understanding that there is discretion available for the calculation of parking demand for the self-storage facility. This is represented by the ITE parking ratios and further supported by a letter (attached to this application) from Mr. Steve Tangney, Vice President of West Coast Self-Storage. The parking associated with the proposed Les Schwab Tire Center is within the maximum allowed per the CIDDS standards. The quantity of stalls associated with the self-storage facility – approaching 0.30 stalls/1,000 sf – is well in excess of the 0.160 stalls/1,000 sf ratio recommended by the ITE Manual or the 0.136 stalls/1,000 sf recommended by West Coast Self-Storage.

BUILDING HEIGHT

5. Height: please provide height info on the storage building for review relative to CIDDS Table 4.4.

Magellan Architects (MA): Height information has been provided on the cover sheet and exterior elevations for the respective buildings.

Project Description:

We are proposing redevelopment of this underutilized site with an 87,416 square foot self-storage facility paired with a 12,556 square foot, 7 service bay Les Schwab Tire Center. The site's previous use was a small mixed office and retail building that included one apartment and an informal animal pasture.

The proposed self-storage building would extend to the 54 foot maximum height (above average existing grade) per zoning regulations. The Les Schwab facility is lower at a proposed 25' 4" above finished grade. While no building setbacks are required per code the proposed building setbacks vary around the site based on a combination of critical areas buffering, site infrastructure requirements and design guidelines.

The project proposes 44 at grade parking spaces of which approximately half are under a portion of the self-storage structure. Based on parking analysis from past self-storage projects in conjunction with zoning and development requirements we believe that the proposed parking is adequate and appropriate for the proposed uses.

Generally this project aims to maximize buildable area while respecting critical areas buffers situated to the north-west of the site, minimizing impacts to adjacent rights-of-way and uses.

Site Location:

The essentially triangular site is located on the north side of NW Gilman Boulevard at the intersection of NW Juniper Street. Access is via a driveway at NW Juniper Street shared with adjacent businesses. Adjacent uses include the East Sammamish Trail corridor abutting the property to the west, the Interstate-90 corridor to the east and Pogacha restaurant to the south.

Topography:

The existing site topography is quite flat with a swale off-site to the east in the WSDOT Interstate-90 corridor and a small wetland depression off-site to the north and west of the site. It is expected that, due to existing soil conditions, grade will be raised slightly in order to provide appropriate structural basis for buildings and site improvements.

Design Standard 11.0 Site Design

11.2 Existing Site Features and Context: Despite odd shapes and proportions and challenging site access this proposal respects the existing wetland and wetland boundaries just off-site to the northwest and provides ample setback from the west property boundary in an effort to honor the East Sammamish trail corridor alignment. The site is shielded from the adjacent I-90 corridor and Front Street off-ramp by existing, off-site trees and vegetation and development is oriented towards the NW Gilman Boulevard corridor and adjacent pedestrian facilities.

11.3 Pedestrian Connections: Pedestrian connectivity and circulation is facilitated by the inclusion of accessible and obvious sidewalk and crosswalk connections from the NW Gilman Boulevard right-of-way to the site and to the proposed structures. Minimal conflict with parking areas and vehicular circulation are incorporated into the layout of pedestrian circulation elements.

11.4 Environmentally Critical Areas: The self-storage facility honors the existing wetland and buffers by stepping around buffer areas. Buffer replacement areas and enhancement plantings are proposed in a further effort to respect these natural areas.

11.5 Service, Loading and Waste Enclosures: The fully screened solid waste collection area (i.e. trash enclosure) is combined for joint use by both businesses on the site. Further, the trash enclosure is situated adjacent to, and integral with, the tire center loading area to consolidate these functions and position them away from pedestrian access areas to further minimize visual impact.

Design Standard 12.0 Circulation Design

12.3 Motorized Facility Standards: Apparent vehicular circulation areas in front of the tire center service bays have been visually reduced in order to mitigate driving speeds and create clear lanes of function and travel. Likewise, drive aisle widths have been modulated throughout the parking lot areas to strike a balance of functional widths for site access and deliveries while seeking to reduce travel speeds on the site. The proposed development makes use of a single existing driveway alignment and an existing connection to NW Gilman Boulevard.

12.4 Nonmotorized Facility Standards: Pedestrian walkways are separated, wherever possible, from vehicular travel areas by landscaping and clear crosswalks and changes of material and texture. Clear and direct routes for pedestrian travel are provided throughout the site and to the adjacent right-of-way.

Design Standard 14.0 Buildings

While previous design concepts considered combining both functions into a single structure it was ultimately decided to distinguish each function into a separate structure thus providing distinct and visually interesting building massing within the site.

The proposed designs have sought to introduce appropriate levels of glazing and transparency with an emphasis on retail and public access areas of each facility. Attention was also paid to providing building modulation and transparency for the self-storage facility adjacent to the East Sammamish Trail corridor and, at the Les Schwab showroom area, along the albeit limited street frontage of this site including significant transparency at the southwest corner of the site and wrapping around to the south property boundary.

The taller and larger self-storage facility has been situated to the northern portion of the site in order to minimize visual impact to NW Gilman Boulevard, preserve solar exposure for the site in not casting shade and shadows across adjacent structures or circulation areas and to buffer the site from traffic noise from the adjacent Interstate-90 corridor.

Views and Exposure:

The site has relatively good visual exposure from NW Gilman Boulevard punctuated partially by mature existing street trees along the NW Gilman Boulevard corridor. Along its western boundary the site is relatively open with respect to the East Sammamish Trail alignment. While it is immediately adjacent to the Interstate-90 corridor and Front Street off-ramp the site is screened by dense mature trees and vegetation within the I-90 right-of-way.

Noise:

Situated between two primary thoroughfares and adjacent commercial uses the site is subject to typical traffic noise. The proposed self-storage generates little to no operational noise impacts. In that the majority of parking serving the self-storage facility is located under the structure any potential noise during customer activity will be partially contained. The proposed tire sales center does have noise associated with operational activities (e.g. impact wrenches for removing & installed lug nuts, etc.). In that the bulk of these activities occur within the interior service bays and the bays are oriented inwards to the center of the site this will serve to further contain and mitigate noise impacts.

Architecture:

Adjacent structures include, to the west, a two-story, mixed retail building familiarly known as "The Clock Tower" building, separated from the site by the East Lake Sammamish Trail corridor. Also immediately west is the nostalgic presence of a barbershop contained within a train caboose. Southeast of the site is situated the one-story Pogacha Restaurant & Bar. Southwest and west of the site, across NW Gilman Boulevard, is the Gilman Village complex and a financial institution which are set back some distance from the NW Gilman right-of-way.

Nearby architectural styles are varied with the structures of Gilman Village representing a variety of small scale commercial buildings with eclectic traditional styling. Likewise the Clock Tower building is of conventional retail design with the addition of a more traditional clock tower element prominent on its western façade. The Pogacha Restaurant & Bar presents similarly traditional forms and cladding materials.

objectives and existing site conditions

design standards and response

Public access and building entrance areas have been made visually apparent through orientation on the site and with respect to both pedestrian access routes and parking functions. It is important to recognize that access to both of the proposed functions is primarily vehicular in nature. Thus clear and proximal vehicular parking has also been established on the site.

Both vertical and horizontal building modulation, appropriate to the respective uses, is introduced within each building design while pedestrian scale canopies at each building serve to identify public access and provide a sense of space and entry.

Design emphasis in the form of glazing, canopy details and divided glazing mullions has been provided at prominent building corners oriented towards the site access point and NW Gilman Boulevard.

Design Standard 15.0 Parking

Vehicular parking has been oriented towards the center of the property in order to minimize impacts to adjacent uses while maintaining useful proximity to these business types. Similarly vehicular circulation is separated from pedestrian areas by changes in materials and texture and through the use of landscaping.

A single driveway access on to the site is utilized as well as the existing, shared connection to NW Gilman Boulevard in an effort to minimize impacts to the public right-of-way.

Appropriately configured bicycle parking is provided in a clear and prominent area while keeping it protected from vehicles and pedestrian conflicts. Given the uses for this site it is expected that bicycle use will be minimal.

Design Standard 16.0 Landscaping

Increased building setbacks, particularly for the self-storage building, provide increase landscape buffering opportunities to soften building presence and add visual interest.

While a range of plant materials are proposed consistency and repetition within site landscaping provides continuity of design. Complementary and varied native plant materials as well as downed logs are proposed within the buffer replacement and enhancement areas to provide habitat and distinguish this environmental asset while providing visual interest and a comprehensive overall design.

The heavily planted northern portion of the site provides additional buffering for the wetland, the trail corridor and the Interstate-90 right-of-way.

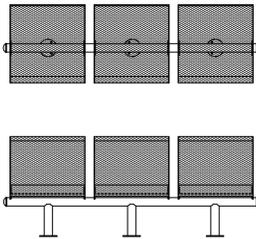
Design Standard 17.0 Lighting

The proposed site lighting has been configured in order to mitigate impacts on adjacent properties, uses and roadways. Light fixture locations are distributed in an effort to provide consistent, uniform lighting throughout the drive aisles and parking areas while preventing light pollution to off-site areas and ensuring the safety and security of customers and employees.

NOTE:
SEE FFE SCHEDULE REFERENCE, ARCHITECTURAL SHEET A151, FOR PROCUREMENT INFORMATION. FOLLOW ALL MANUFACTURERS RECOMMENDATIONS FOR INSTALLATION.

BENCH SPECIFICATIONS:

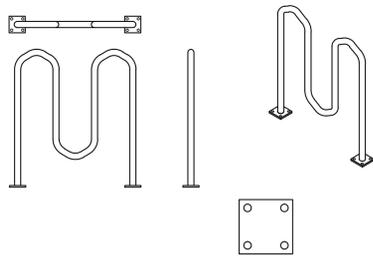
1. WILKASH VALLEY MANUFACTURING INC. "CITY LIMITS STRAIGHT BENCH"
2. PERFORATED 3 SEAT W/ BACK
3. SURFACE MOUNT
4. DARK BRONZE
5. MODEL #DL321 BENCH, WITH MC103 PLATE COVERS
6. UNLESS OTHERWISE SPECIFIED, ALL DIMENSIONS ARE IN INCHES AND PER ASME Y14.5M-1994.
7. ALL DIMENSIONS ARE AFTER PLASTISOL COATING.



1 SITE BENCH

SCALE: NONE

NOTE:
SEE FFE SCHEDULE REFERENCE, ARCHITECTURAL SHEET A151, FOR PROCUREMENT INFORMATION. FOLLOW ALL MANUFACTURERS RECOMMENDATIONS FOR INSTALLATION.



2 BIKE RACK

SCALE: NONE



Copyright © 2015, Magellan Architects PLLC. All rights reserved. This project is the property of Magellan Architects, PLLC. No part of this project may be reproduced, stored in a retrieval system, or transmitted in any form or by any means, electronic or mechanical, including photocopying, recording, or by any information storage and retrieval system, without the express written approval of Magellan Architects, PLLC. This approval does not constitute a warranty or guarantee of any kind, and the purchaser to use this set of plans for the project at their own risk. The purchaser shall be responsible for obtaining all necessary permits and approvals from the appropriate authorities. The purchaser shall be responsible for obtaining all necessary permits and approvals from the appropriate authorities. The purchaser shall be responsible for obtaining all necessary permits and approvals from the appropriate authorities.

Magellan
ARCHITECTS
8383 158th Avenue Northeast, Suite 280
Redmond, Washington 98052
Tel (425) 885-4300 Fax (425) 885-4303
www.magellanarchitects.com
Copyright © 2015 Magellan Architects PLLC

GILMAN POINT
SELF-STORAGE and LES SCHWAB TIRE CENTER
160 NW GILMAN BOULEVARD
ISSAQUAH, WASHINGTON 98027

REVISIONS		
NO.	DATE	BY
1		
2		
3		
4		
5		
6		

ISSUE DATES

DESIGN APPROVAL:

PERMIT SUBMITTAL:

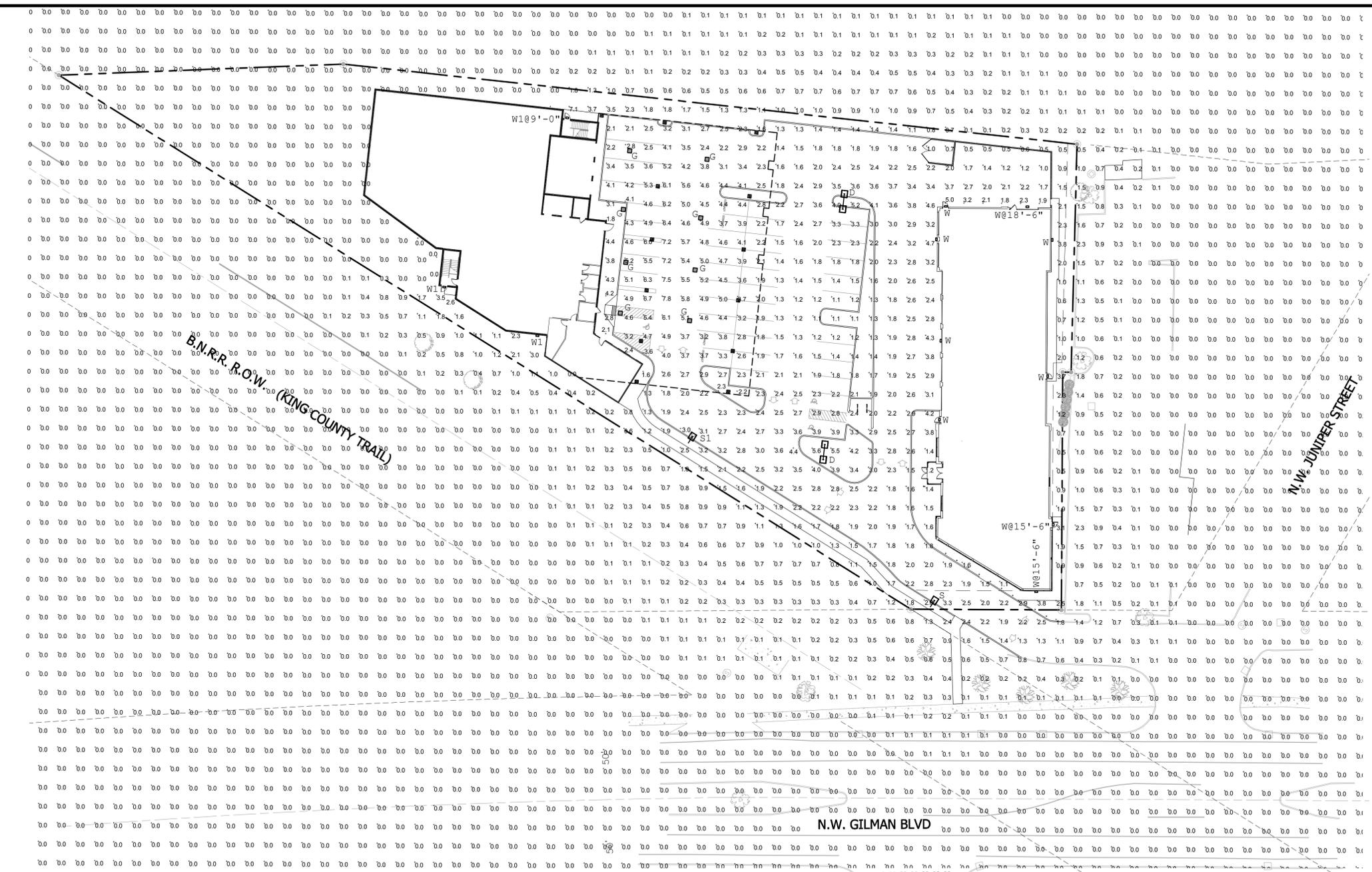
PERMIT RECEIVED:

BID DOCS:

CONSTR. DOCS:

24"x36" SCALE:	
PLOT DATE:	03-10-2015
CAD FILE:	14-140_A021_ASDP
JOB NUMBER:	14-140
CHECKED:	
DRAWN:	DYM
STATUS:	ISSUE FOR ASDP

SITE AMENITIES, CONTEXT PHOTOS AND NARRATIVE
A0.21



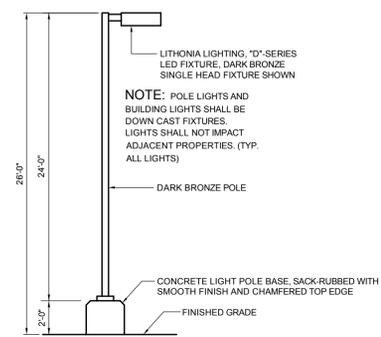
1 SITE PHOTOMETRIC PLAN
SCALE: 1"=30'-0"

LUMINAIRE SCHEDULE						
TIRE STORE						
SYMBOL	QTY	LABEL	TYPE	WATTS	LUMENS	MODEL NUMBER
☐	2	S	SINGLE	131	13289	DSX1 LED 60C 700 40K TFTM MVOLT SPA DDBXD
☐	1	D	D180"	262	26578	DSX1 LED 60C 700 40K TFTM MVOLT SPA DDBXD
☐	9	W	SINGLE	47	4055	WST LED 2 10A700_40K SR3 MVOLT ELCW DDBXD
STORAGE						
☐	1	S1	SINGLE	131	13289	DSX1 LED 60C 700 40K TFTM MVOLT SPA DDBXD
☐	3	W1	SINGLE	47	4055	WST LED 2 10A700_40K SR3 MVOLT ELCW PIR DMG DDBXD
☐	8	G	SINGLE	86	6270	KACM LED 1 49B350/40K SR5 MVOLT DDBXD

NOTE:
ALL PROPOSED LIGHT FIXTURES ARE FULL CUT-OFF AND DOWNCAST.

POLE SCHEDULE				
TIRE STORE				
SYMBOL	QTY	LABEL	ARRANGEMENT	MODEL NUMBER
☐	1	S	SINGLE	SSS-244C-DM19AS-DOB
☐	2	D	2 @ 180"	SSS-244C-DM28AS-DOB
CAR WASH				
☐	1	S-1	SINGLE	SSS-244C-DM19AS-DOB

CALCULATION SUMMARY						
LABEL	UNITS	AVG	MAX	MIN	UNIFORMITY	
					AUGSMIN	MAXIMIN
PARKING	FC	2.05	3.9	1.1	1.85	3.55
PARKING-STORAGE	FC	4.38	6.7	2.1	2.09	3.19
PARKING-UNDER	FC	2.91	5.0	1.3	2.24	3.85
DRIVE	FC	2.38	4.9	1.1	2.16	4.45
DRIVE-UNDER	FC	4.25	7.8	1.4	3.04	5.57
DRIVE-TRASH	FC	1.76	3.7	0.5	3.52	7.40
ENTRY	FC	2.04	3.3	1.5	1.36	2.20
BUILDING SURROUNDS-TIRE	FC	3.74	4.4	3.0	1.25	1.47



2 AREA LIGHT DETAIL
SCALE: NOT TO SCALE

DATE	DES. BY	DRWN. BY

Galloway
Planning, Architecture, Engineering
6162 S. Willow Drive, Suite 320
Greenwood Village, CO 80111
303.770.8884
www.gallowayus.com

LES SCHWAB
Tires

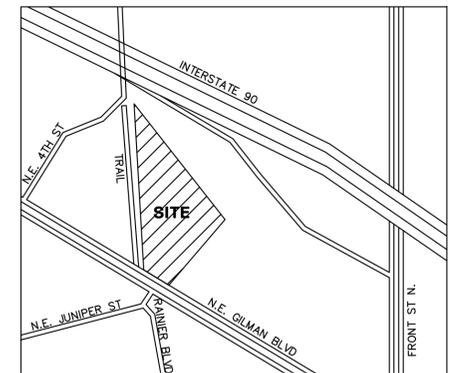
LES SCHWAB TIRE CENTER
160 NW GILMAN BOULEVARD
ISSAQUAH, WA 98027

Project No:	LST700027
Sheet Scale:	1"=30'
Designed By:	JMG
Drawn By:	JMG
Date:	March, 2015
Plot File:	Lst7007_P_A03-Lite

PHOTOMETRIC SITE PLAN

A0.3

S.E. 1/4 OF N.E. 1/4 OF SECTION 28, T. 24 N., R. 06 E., W.M.
CITY OF ISSAQUAH, STATE OF WASHINGTON



VICINITY MAP
N.T.S.

LEGAL DESCRIPTION
N.T.S.
THAT PORTION OF LOTS 1 THROUGH 4, INCLUSIVE, BLOCK 6, HERBERT S. UPPER'S FIRST ADDITION TO ISSAQUAH, ACCORDING TO THE PLAT THEREOF RECORDED IN VOLUME 12 OF PLATS, PAGE 13, IN KING COUNTY, WASHINGTON, DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF SAID BLOCK 6; THENCE SOUTH 16°41'00" EAST 414.26 FEET TO THE NORTHERLY MARGIN OF STATE HIGHWAY NO.2; THENCE SOUTH 48°25'00" EAST ALONG SAID NORTHERLY MARGIN 80 FEET TO THE MOST WESTERLY POINT OF A TRACT OF LAND AS DESCRIBED IN DEED RECORDED UNDER RECORDING NUMBER 8904141132; THENCE NORTH 41°35'00" EAST 125.7 FEET; THENCE SOUTH 48°48'30" EAST 4.5 FEET; THENCE NORTH 42°45'03" EAST 120.98 FEET TO THE SOUTHWESTERLY MARGIN OF PRIMARY STATE HIGHWAY NO.2 (SR90) AS CONDEMNED IN KING COUNTY SUPERIOR COURT CAUSE NUMBER 691461; THENCE NORTHWESTERLY ALONG SAID SOUTHWESTERLY MARGIN TO THE NORTH LINE OF LOT 1 IN SAID BLOCK 6; THENCE WEST ALONG SAID NORTH LINE TO THE POINT OF BEGINNING; TOGETHER WITH THAT PORTION OF THE NORTHEAST QUARTER OF THE NORTHEAST QUARTER OF SECTION 28, TOWNSHIP 24 NORTH, RANGE 6 EAST, W.M., IN KING COUNTY, WASHINGTON, LYING NORTH OF LOCUST STREET, EASTERLY OF NORTHERN PACIFIC RAILROAD RIGHT-OF-WAY AND SOUTHERLY OF PRIMARY STATE HIGHWAY NO.2 (SR90); AND TOGETHER WITH THAT PORTION OF LOCUST STREET LYING SOUTHERLY OF THE ABOVE DESCRIBED PORTION OF SECTION 28, AND NORTHERLY OF THE ABOVE DESCRIBED PORTION OF BLOCK 6, HERBERT S. UPPER'S FIRST ADDITION TO ISSAQUAH.

DATUM
NAVD 88

BENCHMARK
3" BRASS DISK WITH SCRIBED "X" IN DISK, SET 0.3' ABOVE GRADE, LOCATED IN THE CENTER OF THE TURN LANE ON FRONT STREET NORTH AT N.W. HOLLY STREET
ELEVATION=83.62 FEET

INSTRUMENTATION
INSTRUMENT USED: 5 SECOND TOTAL STATION.
FIELD SURVEY WAS BY CLOSED TRAVERSE LOOP, MINIMUM CLOSURE OF LOOPS WAS 1:22,000, IN ACCORDANCE WITH WAC 331-130-090.

REFERENCE
RECORD OF SURVEY, REC. NO. 9605319001

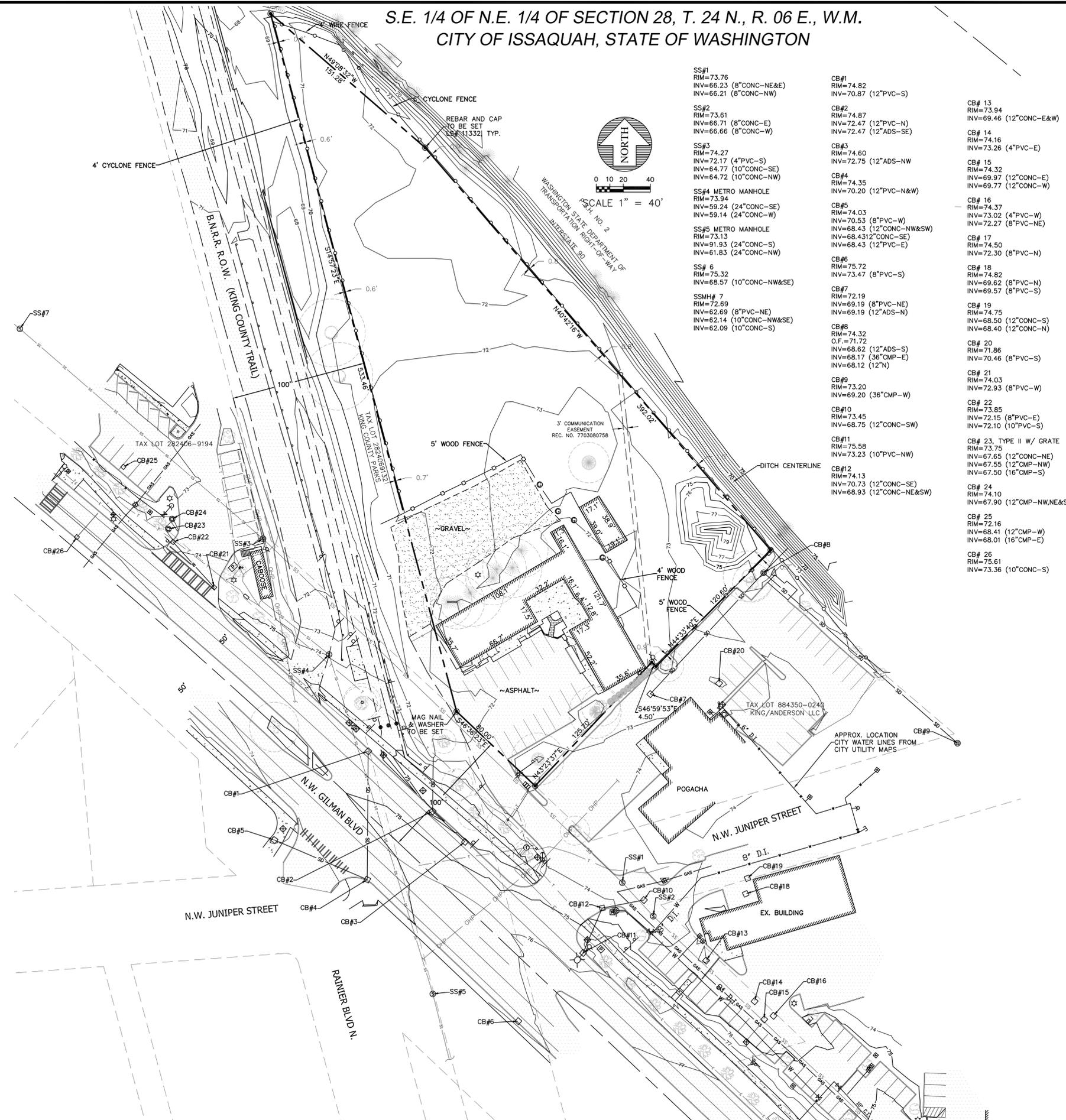
TABLE A NOTES
FLOOD ZONE DESIGNATION IS ZONE X (UNSHADED) PER PANEL 691 OF 1725, MAP NO. 53033C0691H
GROSS LAND AREA OF SITE = 82,134 SQ. FT. - 1.89 ACRES
THE LAND USE UNDER THE CENTRAL ISSAQUAH PLAN IS MIXED USE.
SIDE AND REAR SETBACKS = 0 FEET
BUILD TO LINE = 0 FEET TO 10 FEET
MAXIMUM IMPERVIOUS SURFACE = 90%
BUILDING HEIGHT
BASE = 48' MAX. = 85'
FLOOR AREA RATIO
RESIDENTIAL = 1.7 TO 3.5 MAX.
COMMERCIAL = 1.25 TO 3.5 MAX.

THERE IS NO VISIBLE EVIDENCE OF EARTH MOVING WORK, BUILDING CONSTRUCTION OR BUILDING ADDITIONS WITHIN RECENT MONTHS.
THERE ARE NO CHANGES IN STREET RIGHT OF WAY LINES, EITHER COMPLETED OR PROPOSED, FROM THE CITY OF ISSAQUAH.
THERE IS NO OBSERVABLE EVIDENCE OF SITE USE AS A SOLID WASTE DUMP, SUMP OR SANITARY LANDFILL.
PUGET SOUND ENERGY MARKED PROPERTY "NO ELECTRIC" AND "NO GAS"
EXISTING BUILDING HEIGHT IS 14.5± FEET
GROSS BUILDING AREA IS 6,049 SQ. FT.

SS#1 RIM=73.76 INV=66.23 (8"CONC-NE&E) INV=66.21 (8"CONC-NW)	SS#2 RIM=73.61 INV=66.71 (8"CONC-E) INV=66.66 (8"CONC-W)	SS#3 RIM=74.27 INV=72.17 (4"PVC-S) INV=64.77 (10"CONC-SE) INV=64.72 (10"CONC-NW)	SS#4 METRO MANHOLE RIM=73.94 INV=59.24 (24"CONC-SE) INV=59.14 (24"CONC-W)	SS#5 METRO MANHOLE RIM=73.13 INV=91.93 (24"CONC-S) INV=61.83 (24"CONC-NW)	SS#6 RIM=75.32 INV=68.57 (10"CONC-NW&SE)	SSMH#7 RIM=72.69 INV=62.69 (8"PVC-NE) INV=62.14 (10"CONC-NW&SE) INV=62.09 (10"CONC-S)	CB#1 RIM=74.82 INV=70.87 (12"PVC-S)	CB#2 RIM=74.87 INV=72.47 (12"PVC-N) INV=72.47 (12"ADS-SE)	CB#3 RIM=74.60 INV=72.75 (12"ADS-NW)	CB#4 RIM=74.35 INV=70.20 (12"PVC-N&W)	CB#5 RIM=74.03 INV=70.53 (8"PVC-W) INV=68.43 (12"CONC-NW&SW) INV=68.43(12"CONC-SE) INV=68.43 (12"PVC-E)	CB#6 RIM=75.72 INV=73.47 (8"PVC-S)	CB#7 RIM=72.19 INV=69.19 (8"PVC-NE) INV=69.19 (12"ADS-N)	CB#8 RIM=74.32 O.F.=71.72 INV=68.62 (12"ADS-S) INV=68.17 (36"CMP-E) INV=68.12 (12"N)	CB#9 RIM=73.20 INV=69.20 (36"CMP-W)	CB#10 RIM=73.45 INV=68.75 (12"CONC-SW)	CB#11 RIM=75.58 INV=73.23 (10"PVC-NW)	CB#12 RIM=74.13 INV=70.73 (12"CONC-SE) INV=68.93 (12"CONC-NE&SW)	CB#13 RIM=73.94 INV=69.46 (12"CONC-E&W)	CB#14 RIM=74.16 INV=73.26 (4"PVC-E)	CB#15 RIM=74.32 INV=69.97 (12"CONC-E) INV=69.77 (12"CONC-W)	CB#16 RIM=74.37 INV=73.02 (4"PVC-W) INV=72.27 (8"PVC-NE)	CB#17 RIM=74.50 INV=72.30 (8"PVC-N)	CB#18 RIM=74.82 INV=69.62 (8"PVC-N) INV=69.57 (8"PVC-S)	CB#19 RIM=74.75 INV=68.50 (12"CONC-S) INV=68.40 (12"CONC-N)	CB#20 RIM=71.86 INV=70.46 (8"PVC-S)	CB#21 RIM=74.03 INV=72.93 (8"PVC-W)	CB#22 RIM=73.85 INV=72.15 (8"PVC-E) INV=72.10 (10"PVC-S)	CB#23, TYPE II W/ GRATE RIM=73.75 INV=67.65 (12"CONC-NE) INV=67.55 (12"CMP-W) INV=67.50 (16"CMP-S)	CB#24 RIM=74.10 INV=67.90 (12"CMP-NW,NE&S)	CB#25 RIM=72.16 INV=68.41 (12"CMP-W) INV=68.01 (16"CMP-E)	CB#26 RIM=75.61 INV=73.36 (10"CONC-S)
---	---	--	--	--	--	---	---	--	--	---	--	--	---	---	---	--	---	---	---	---	--	---	---	--	--	---	---	---	--	--	--	---

- LEGEND**
- FIRE HYDRANT
 - MAILBOX
 - ◇ IRRIGATION CONTROL VALVE
 - ▽ SIGN POST
 - TELEPHONE RISER
 - ⊠ GAS METER
 - ◇ POWER VAULT
 - ◇ STREETLIGHT
 - TELEPHONE MANHOLE
 - UTILITY POLE
 - JUNCTION BOX
 - ◆ GUARD POST
 - ⊙ GATE
 - ☆ YARD LIGHT

- TREE LEGEND**
- CEDAR TREE
 - MAPLE TREE
 - COTTONWOOD TREE
 - OAK TREE
 - WILLOW TREE
 - TREE (UNSPECIFIED)
 - DRIPLINE
 - LAUREL HEDGE
 - CHERRY TREE
 - ALDER TREE



REVISIONS	DESCRIPTION	BY	DATE



TAX PARCEL 884350-0238
ISSAQUAH, WASHINGTON
BOUNDARY/TOPOGRAPHY PLAN

Encompass
ENGINEERING & SURVEYING
Western Washington Division
165 NE Juniper Street, Suite 201 • Issaquah, WA 98027 • Phone: (425) 397-0250 • Fax: (425) 391-3055
108 East 2nd Street • Cle Elum, WA 98922 • Phone: (509) 674-7433 • Fax: (509) 674-7419

JOB NO.	14670
DATE	10/29/14
SCALE	JOB#
DESIGNED	WSH
DRAWN	JEF
CHECKED	WSH
APPROVED	WSH

SHEET 1 OF 1

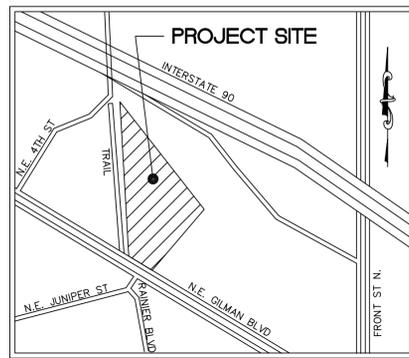
GILMAN POINT

ADMINISTRATIVE SITE DEVELOPMENT PERMIT (ASDP) SUBMITTAL

SELF-STORAGE AND LES SCHWAB TIRE CENTER

160 NW GILMAN BOULEVARD
ISSAQUAH, WASHINGTON 98027

SE 1/4 OF NE 1/4 OF SECTION 28, TOWNSHIP 24N, RANGE 6E. W.M.



VICINITY MAP
NOT TO SCALE

CONSULTANTS

CIVIL:
PACLAND
11400 SE 8TH STREET
SUITE 345
BELLEVUE, WA 98004
(425) 453-9501
CONTACT: JENELLE TAFLIN, P.E., LEED AP
EMAIL: JTAFLIN@PACLAND.COM

ARCHITECT:
MAGELLAN ARCHITECTS
8383 158TH AVE
SUITE 280
REDMOND, WA 98052
(425) 885-4300
CONTACT: DIRK MCCULLOCH

LANDSCAPE ARCHITECT:
ALTMANN OLIVER ASSOCIATES, LLC
PO BOX 578
CARNATION, WA 98014
(425)333-4535
CONTACT: SIMONE OLIVER, LA
EMAIL: SIMONE@ALTOLIVER.COM

SURVEYOR:
ENCOMPASS
165 NE JUNIPER STREET
SUITE 201
ISSAQUAH, WA 98027
(425) 392-0250
CONTACT: WILLIAM HOLMBERG

DEVELOPER

LES SCHWAB
20900 COOLEY ROAD
BEND, OR 97701
(541) 416-5206
CONTACT: MATTHEW HANNIGAN

SEA CON LLC
165 NE JUNIPER STREET
ISSAQUAH, WA 98027
(425) 837-9720
CONTACT: J.J. ENGLER

OWNER

MARSI, LLC
160 NEW GILMAN BLVD.
ISSAQUAH, WA 98027
CONTACT: KIRK A. CATTERALL

EXISTING LEGEND

⊗	WATER VALVE	P.A.	PLANTED AREA
⊕	HYDRANT	VL T # 1	450 GALLON UTILITY VAULTS W/ OIL & WATER SEPARATOR
⊙	WATER METER	VL T # 2	RECORD
⊖	MANHOLES (SS/SD)	(R)	DEED
□	CB	(D)	DEED
⊖	POWER/UTILITY POLE	⊕	FOUND CASED MONUMENT
⊖	GUY ANCHOR	⊕	FOUND REBAR & CAP *LDC LS 37536* UNLESS OTHERWISE NOTED
⊖	POWER TRANSFORMER	⊕	FOUND IRON PIPE W/CAP *LS 2534* UNLESS OTHERWISE NOTED
⊖	POWER/TELEPHONE VAULT		
⊖	POWER/TELEPHONE VAULT		
⊖	POWER METER		
⊖	TELEPHONE/TV RISER		
⊖	GAS VALVE		
⊖	GAS METER		
⊖	STREET LIGHT		
⊖	TRAFFIC SIGNAL POLE		
⊖	PEDESTRIAN CROSSING POST		
⊖	CONIFEROUS TREE		
⊖	DECIDUOUS TREE		
⊖	CLEAN OUT		
⊖	SIGN		
⊖	CONCRETE WALK		
⊖	CONCRETE		

---	CENTER LINES
---	PROPERTY LINES
---	RIGHT-OF-WAY LINES
---	LOT LINES
---	DITCH LINE
---	FLOW LINE
---	WATER LINE
---	SANITARY SEWER LINE
---	STORM DRAIN LINE
---	GAS LINE
---	UNDERGROUND POWER LINES
---	UNDERGROUND TELEPHONE LINES
---	UNDERGROUND CABLE TV LINES
---	UNDERGROUND FIBER OPTIC LINES
---	OVERHEAD POWER LINES
---	OVERHEAD UTILITY LINES
---	CHAIN LINK FENCE
---	WIRE FENCE
---	WOOD FENCE

GOVERNING AGENCY

DSD - ISSAQUAH
1775 12TH AVE NW
ISSAQUAH, WA 98027
(425) 837-3414

SITE DATA

PARCEL NUMBERS = 884350-0238
EXISTING USE = MIXED USE
PROPOSED USE = MIXED USE
ZONING DESIGNATION = MIXED USE (MU)

TOTAL PROPERTY AREA = ±82,136 SF (±1.89 ACRES)
EXISTING IMPERVIOUS = 20,342 SF
PERVIOUS = 61,794 SF

PROVIDED IMPERVIOUS: 62,272 SF (75.8% OF 82,136 SF)
NPGIS: 35,461 SF (43.2% OF 82,136 SF)
PGIS: 26,811 SF (32.6% OF 82,136 SF)
PERVIOUS: 19,864 SF (24.2% OF 82,136 SF)

PERMITS REQUIRED:

- ADMINISTRATIVE SITE DEVELOPMENT PERMIT
- COMMERCIAL BUILDING PERMIT
- COMMERCIAL SITE WORK PERMIT

LEGAL DESCRIPTION BY SURVEYOR:

THAT PORTION OF LOTS 1 THROUGH 4, INCLUSIVE, BLOCK 6, HERBERT S. UPPER'S FIRST ADDITION TO ISSAQUAH, ACCORDING TO THE PLAT THEREOF RECORDED IN VOLUME 12 OF PLATS, PAGE 13, IN KING COUNTY, WASHINGTON, DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF SAID BLOCK 6;
THENCE SOUTH 16°41'00" EAST 414.26 FEET TO THE NORTHERLY MARGIN OF STATE HIGHWAY NO.2;
THENCE SOUTH 48°25'00" EAST ALONG SAID NORTHERLY MARGIN 80 FEET TO THE MOST WESTERLY POINT OF A TRACT OF LAND AS DESCRIBED IN DEED RECORDED UNDER RECORDING NUMBER 8904141132;
THENCE NORTH 41°35'00" EAST 125.7 FEET;
THENCE SOUTH 48°48'30" EAST 4.5 FEET;
THENCE NORTH 42°45'03" EAST 120.98 FEET TO THE SOUTHWESTERLY MARGIN OF PRIMARY STATE HIGHWAY NO.2 (SR90) AS CONDEMNED IN KING COUNTY SUPERIOR COURT CAUSE NUMBER 691461;
THENCE NORTHWESTERLY ALONG SAID SOUTHWESTERLY MARGIN TO THE NORTH LINE OF LOT 1 IN SAID BLOCK 6;
THENCE WEST ALONG SAID NORTH LINE TO THE POINT OF BEGINNING;

TOGETHER WITH THAT PORTION OF THE NORTHEAST QUARTER OF THE NORTHEAST QUARTER OF SECTION 28, TOWNSHIP 24 NORTH, RANGE 6 EAST, W.M., IN KING COUNTY, WASHINGTON, LYING NORTH OF LOCUST STREET, EASTERLY OF NORTHERN PACIFIC RAILROAD RIGHT-OF-WAY AND SOUTHERLY OF PRIMARY STATE HIGHWAY NO.2 (SR90);

AND TOGETHER WITH THAT PORTION OF LOCUST STREET LYING SOUTHERLY OF THE ABOVE DESCRIBED PORTION OF SECTION 28, AND NORTHERLY OF THE ABOVE DESCRIBED PORTION OF BLOCK 6, HERBERT S. UPPER'S FIRST ADDITION TO ISSAQUAH.

SURVEYOR NOTES

DATUM: NAVD 88

BENCHMARK: 3" BRASS DISK WITH SCRIBED "X" IN DISK, SET 0.3' ABOVE GRADE, LOCATED IN THE CENTER OF THE TURN LANE ON FRONT STREET NORTH AT N.W. HOLLY STREET
ELEVATION=83.62 FEET

INSTRUMENTATION: INSTRUMENT USED: 5 SECOND TOTAL STATION. FIELD SURVEY WAS BY CLOSED TRAVERSE LOOP, MINIMUM CLOSURE OF LOOPS WAS 1:22,000, IN ACCORDANCE WITH WAC 331-130-090.

REFERENCE: RECORD OF SURVEY, REC. NO. 9605319001

TABLE A NOTES

FLOOD ZONE DESIGNATION IS ZONE X (UNSHADED) PER PANEL 691 OF 1725, MAP NO. 53033C0691H

GROSS LAND AREA OF SITE = 82,134 SQ. FT. - 1.89 ACRES

THE LAND USE UNDER THE CENTRAL ISSAQUAH PLAN IS MIXED USE.

SIDE AND REAR SETBACKS = 0 FEET
BUILD TO LINE = 0 FEET TO 10 FEET
MAXIMUM IMPERVIOUS SURFACE = 90%
BUILDING HEIGHT
BASE = 48' MAX. = 85'
FLOOR AREA RATIO
RESIDENTIAL = 1.7 TO 3.5 MAX.
COMMERCIAL = 1.25 TO 3.5 MAX.

THERE IS NO VISIBLE EVIDENCE OF EARTH MOVING WORK, BUILDING CONSTRUCTION OR BUILDING ADDITIONS WITHIN RECENT MONTHS.

THERE ARE NO CHANGES IN STREET RIGHT OF WAY LINES, EITHER COMPLETED OR PROPOSED, FROM THE CITY OF ISSAQUAH.

THERE IS NO OBSERVABLE EVIDENCE OF SITE USE AS A SOLID WASTE DUMP, SUMP OR SANITARY LANDFILL.

PUGET SOUND ENERGY MARKED PROPERTY "NO ELECTRIC" AND "NO GAS"

EXISTING BUILDING HEIGHT IS 14.5± FEET

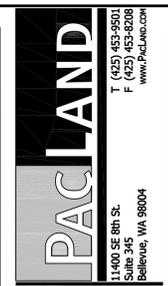
GROSS BUILDING AREA IS 6,049 SQ. FT.

PACLAND NOTES

- ALL WORK AND MATERIALS SHALL COMPLY WITH ALL CITY/COUNTY REGULATIONS AND CODES AND O.S.H.A. STANDARDS.
- THE DESIGN SHOWN IS BASED UPON THE ENGINEER'S UNDERSTANDING OF THE EXISTING CONDITIONS. THIS PLAN DOES NOT REPRESENT A DETAILED FIELD SURVEY. THE EXISTING CONDITIONS SHOWN ON THIS PLAN SHEET ARE BASED UPON SURVEY PREPARED BY ENCOMPASS ENGINEERING AND SURVEY, DATED OCTOBER 21, 2014. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING FIELD CONDITIONS PRIOR TO BIDDING THE PROPOSED SITEWORK IMPROVEMENTS. IF CONFLICTS ARE DISCOVERED, THE CONTRACTOR SHALL NOTIFY THE OWNER PRIOR TO INSTALLATION OF ANY PORTION OF THE SITEWORK WHICH WOULD BE AFFECTED. IF CONTRACTOR DOES NOT ACCEPT EXISTING SURVEY, INCLUDING TOPOGRAPHY AS SHOWN ON THE PLANS, WITHOUT EXCEPTION, HE SHALL HAVE MADE, AT HIS OWN EXPENSE, A TOPOGRAPHIC SURVEY BY A REGISTERED LAND SURVEYOR AND SUBMIT IT TO THE OWNER FOR REVIEW.
- CAUTION - NOTICE TO CONTRACTOR**
THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES AND EXISTING IMPROVEMENTS WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS ON THE PLANS.

SHEET INDEX

CVR-1	COVER SHEET
C-1.0	CIVIL SITE PLAN
C-2.0	CIRCULATION PLAN
C-3.0	FIRE AND RESCUE PLAN
C-4.0	STORMWATER AND GRADING PLAN
C-5.0	WATER, SEWER, AND OTHER UTILITY PLAN
C-6.0	OFFSITE WATER AND IMPROVEMENTS PLAN



GILMAN POINT
SELF-STORAGE and LES SCHWAB TIRE CENTER
160 NW GILMAN BOULEVARD
ISSAQUAH, WASHINGTON 98027

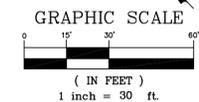
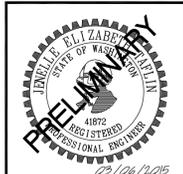
REVISIONS		
NO.	DATE	BY
1		
2		
3		
4		
5		
6		

24"x36" SCALE:	NA
PLOT DATE:	03-05-2015
CAD FILE:	GPIS_000C
JOB NUMBER:	14-140
CHECKED:	
DRAWN:	
STATUS:	SCHEMATIC DESIGN

COVER SHEET
CVR-1



SE 1/4 OF NE 1/4 OF SECTION 28, TOWNSHIP 24N, RANGE 6E, W.M.



LEGEND:

- PROPERTY LINE
- PROPOSED CEMENT CONCRETE TRAFFIC CURB
- STANDARD DUTY PAVEMENT
- HEAVY DUTY PAVEMENT
- CONCRETE SIDEWALK
- CONCRETE PAVING
- LANDSCAPE AREA
- NUMBER OF PARKING STALLS
- DENOTES ADA STALLS
- ADA WARNING STRIP

SITE DATA:

TOTAL PROPERTY AREA: ±82,136 SF (±1.89 AC)

IMPERVIOUS: 62,272 SF (75.8% OF 82,136 SF)
 NPGIS: 35,461 SF (43.2% OF 82,136 SF)
 PGIS: 26,811 SF (32.6% OF 82,136 SF)
 PERVIOUS: 19,864 SF (24.2% OF 82,136 SF)

ALLOWED COVERAGES:
 90% IMPERVIOUS
 10% LANDSCAPE

PROVIDED PARKING:
 STANDARD STALLS: 36 STALLS
 COMPACT STALLS: 4 STALLS
 ADA STALLS: 3 STALLS
 TOTAL STALLS: 43 STALLS

REQUIRED BUILDING SETBACKS
 0'-10' FRONT YARD (BUILD-TO-LINE FROM NW GILMAN BLVD.)
 0' SIDE YARD
 0' REAR YARD

SITE KEY:

- (A) STANDARD ASPHALT PAVEMENT
- (B) HEAVY DUTY ASPHALT PAVEMENT
- (C) HEAVY DUTY CONCRETE PAVEMENT
- (D) CONCRETE SIDEWALK
- (E) CONCRETE CURB AND GUTTER
- (F) CONCRETE VERTICAL CURB
- (G) CONCRETE CURB RAMP
- (H) ACCESSIBLE PARKING SPACE TYPICAL. SEE DETAIL FOR ACCESSIBLE PARKING SPACE SIZE, SIGN AND SYMBOL ("VAN"-INDICATES VAN ACCESSIBLE SPACE)
- (I) 12'x12' TRASH ENCLOSURE
- (J) 4" WIDE SOLID WHITE STRIPE, (TYP.)
STANDARD PARKING STALLS ARE 9.0' X 20.0'
- (K) PAINTED DIRECTIONAL ARROWS (TYP)
- (L) FIRE HYDRANT MARKER POST
- (M) T-36 PEDESTRIAN CROSSWALK STRIPPING
- (N) BIKE RACK
- (O) FIRE LANE STRIPING
- (P) ROCKERY (4' MAX. HEIGHT)
- (Q) HEIGHTENED CURB. SEE SHEET C-4.0 FOR CURB HEIGHTS.
- (R) LIGHT POLE. SEE ARCHITECTURAL PLANS FOR DETAILS.

PACLAND
 T (425) 453-9001
 F (425) 453-8208
 11405 SE 8th St.
 Suite 345
 Bellevue, WA 98004
 www.PacLand.com

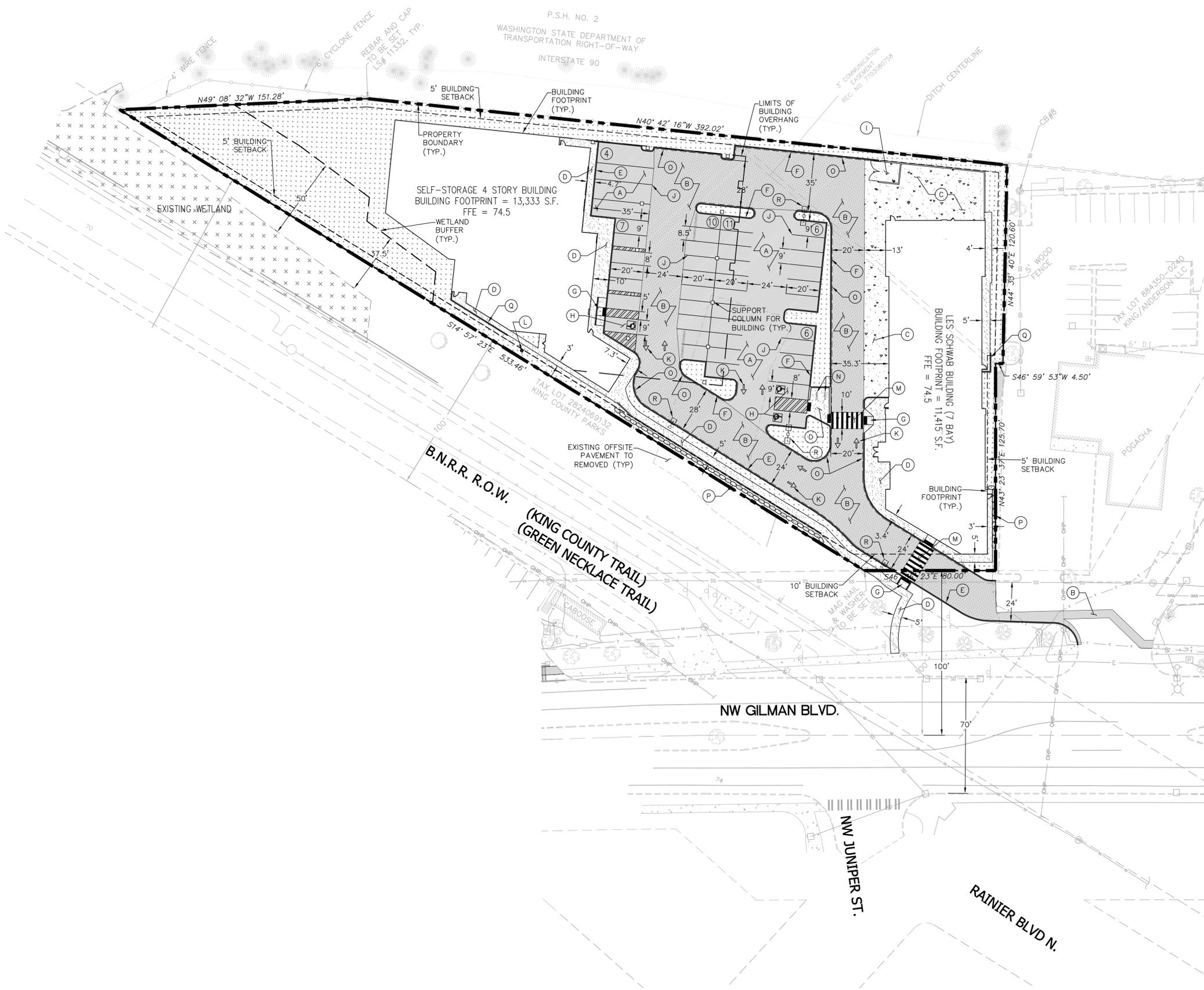
GILMAN POINT
 SELF-STORAGE and LES SCHWAB TIRE CENTER
 160 NW GILMAN BOULEVARD
 ISSAQUAH, WASHINGTON 98027

REVISIONS		
NO.	DATE	BY
1		
2		
3		
4		
5		

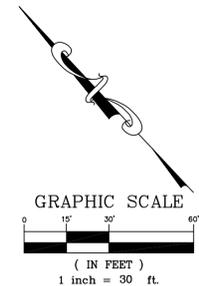
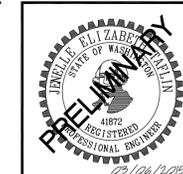
ISSUE DATES	
DESIGN APPROVAL:	
PERMIT SUBMITTAL:	
PERMIT RECEIVED:	
BID DOCS:	
CONSTR. DOCS:	

24"x36" SCALE:	1" = 30'
PLOT DATE:	03-05-2015
CAD FILE:	GPIIS_110S
JOB NUMBER:	14-140
CHECKED:	
DRAWN:	
STATUS:	SCHEMATIC DESIGN

CIVIL SITE PLAN
C-1.0



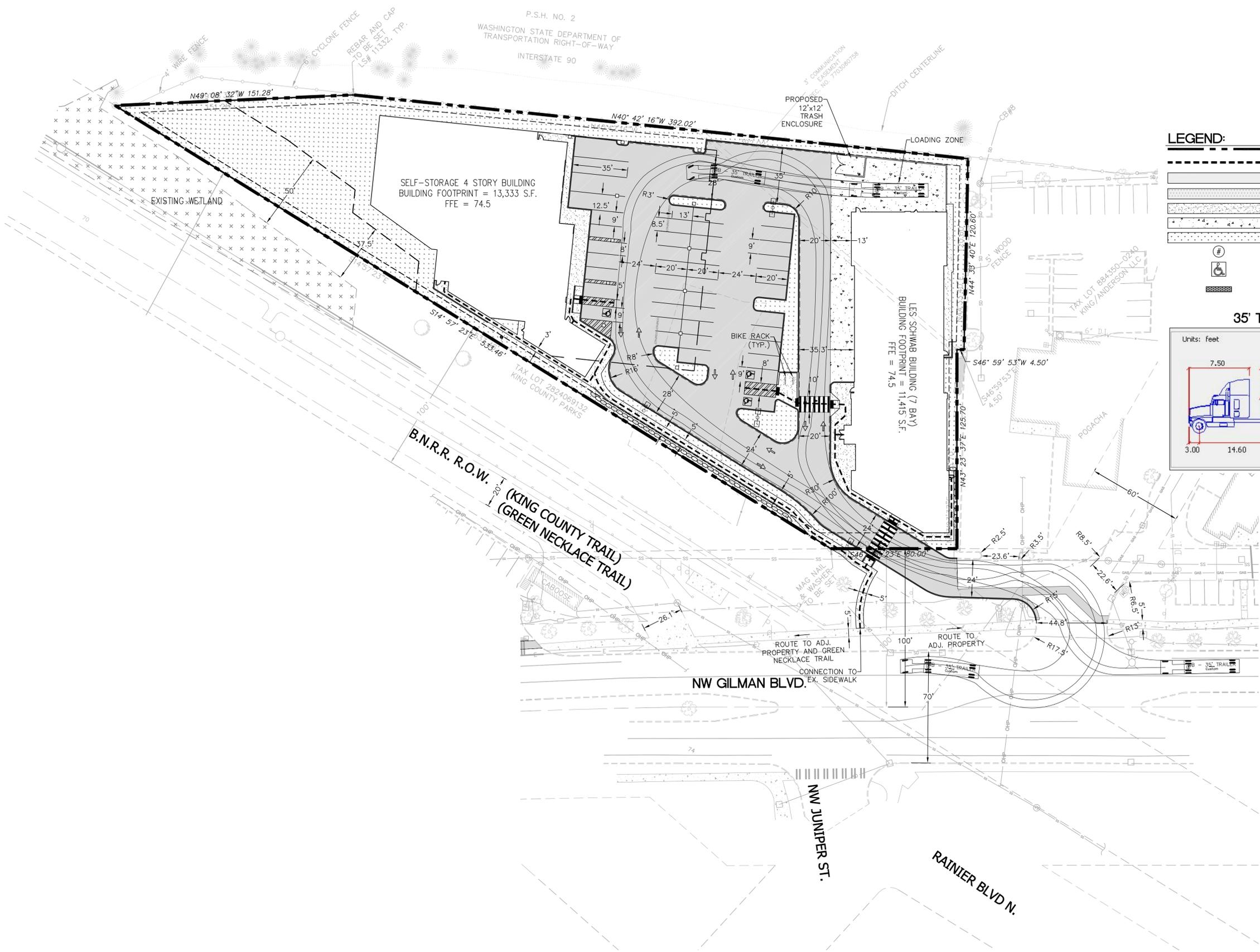
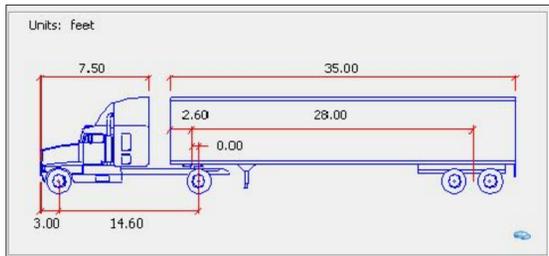
SE 1/4 OF NE 1/4 OF SECTION 28, TOWNSHIP 24N, RANGE 6E, W.M.



LEGEND:

- PROPERTY LINE
- PEDESTRIAN ACCESS ROUTE
- STANDARD DUTY PAVEMENT
- HEAVY DUTY PAVEMENT
- CONCRETE SIDEWALK
- CONCRETE PAVING
- LANDSCAPE AREA
- NUMBER OF PARKING STALLS
- DENOTES ADA STALLS
- ADA WARNING STRIP

35' TRAILER DELIVERY TRUCK



PACLAND
 T (425) 453-9001
 F (425) 453-8208
 www.PacLand.com
 11405 SE 8th St.
 Suite 345
 Bellevue, WA 98004

GILMAN POINT
 SELF-STORAGE and LES SCHWAB TIRE CENTER
 160 NW GILMAN BOULEVARD
 ISSAQUAH, WASHINGTON 98027

REVISIONS		
NO.	DATE	BY
1		
2		
3		
4		
5		

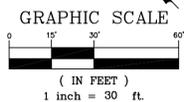
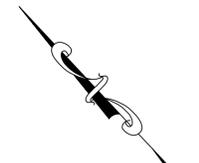
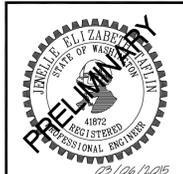
ISSUE DATES	
DESIGN APPROVAL:	
PERMIT SUBMITTAL:	
PERMIT RECEIVED:	
BID DOCS:	
CONSTR. DOCS:	

24"x36" SCALE:	1" = 30'
PLOT DATE:	03-05-2015
CAD FILE:	GPIS_110S
JOB NUMBER:	14-140
CHECKED:	
DRAWN:	
STATUS:	SCHEMATIC DESIGN



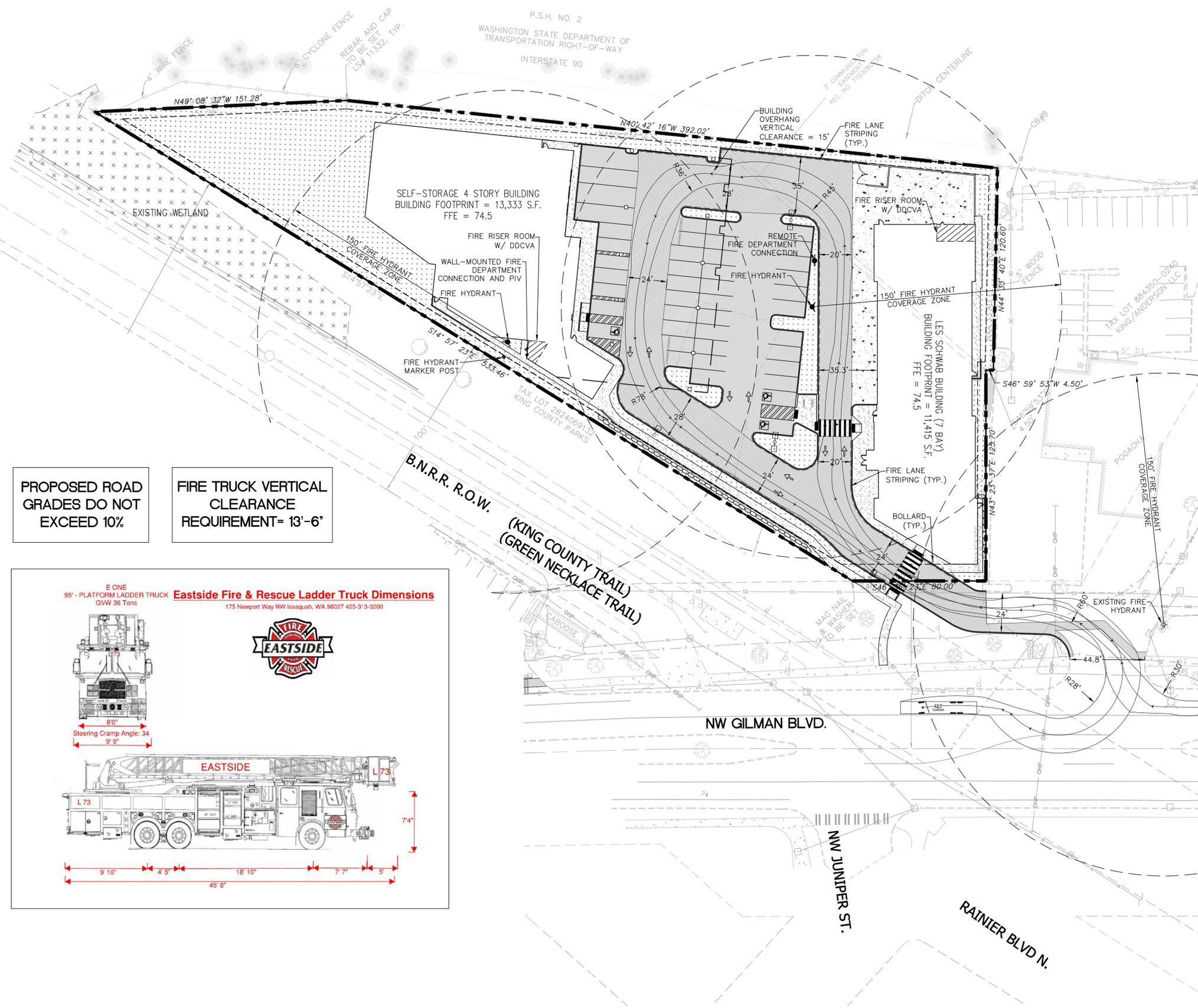
CIVIL CIRCULATION PLAN
C-2.0

SE 1/4 OF NE 1/4 OF SECTION 28, TOWNSHIP 24N, RANGE 6E, W.M.



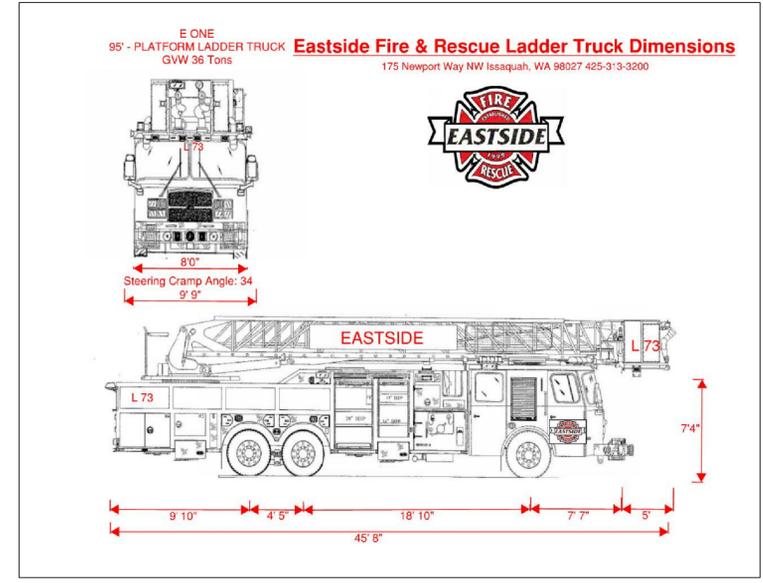
LEGEND:

	PROPERTY LINE
	PROPOSED CEMENT CONCRETE TRAFFIC CURB
	FIRE LANE STRIPING
	STANDARD DUTY PAVEMENT
	HEAVY DUTY PAVEMENT
	CONCRETE SIDEWALK
	CONCRETE PAVING
	LANDSCAPE AREA



PROPOSED ROAD GRADES DO NOT EXCEED 10%

FIRE TRUCK VERTICAL CLEARANCE REQUIREMENT= 13'-6"



PACLAND
T (425) 453-9001
F (425) 453-8208
www.PacLand.com
11400 SE 8th St.
Suite 345
Bellevue, WA 98004

GILMAN POINT
SELF-STORAGE and LES SCHWAB TIRE CENTER
160 NW GILMAN BOULEVARD
ISSAQUAH, WASHINGTON 98027

REVISIONS

NO.	DATE	BY
△		
△		
△		
△		
△		

ISSUE DATES

DESIGN APPROVAL:
PERMIT SUBMITTAL:
PERMIT RECEIVED:
BID DOCS:
CONSTR. DOCS:

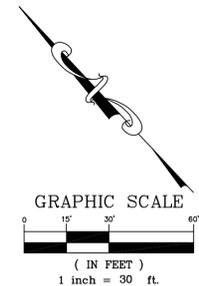
24"x36" SCALE: 1" = 30'

PLOT DATE: 03-05-2015
CAD FILE: GPIS_110S
JOB NUMBER: 14-140
CHECKED:
DRAWN:
STATUS: SCHEMATIC DESIGN

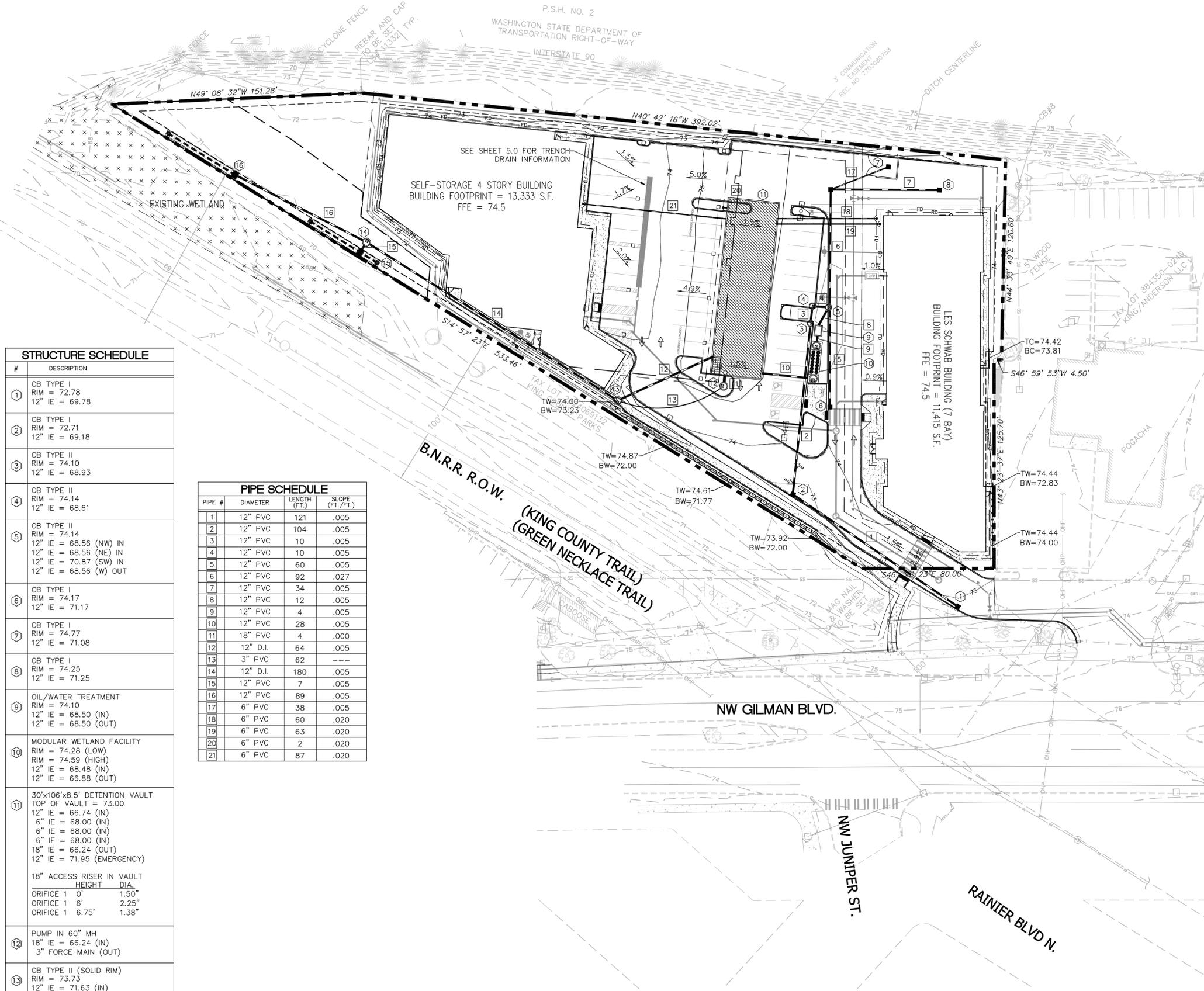


CIVIL FIRE AND RESCUE PLAN
C-3.0

SE 1/4 OF NE 1/4 OF SECTION 28, TOWNSHIP 24N, RANGE 6E, W.M.



- LEGEND:**
- SD STORM DRAIN PIPE
 - EXISTING STORM DRAIN PIPE
 - RD ROOF DRAIN LINE
 - FD FOOTING DRAIN LINE
 - STORM DRAIN CLEANOUT
 - CATCH BASIN TYPE 1/1L
 - CATCH BASIN TYPE 2
 - ① HEX (USE FOR STORM DRAINAGE STRUCTURE SCHEDULES ONLY—NOTICE THE ROTATION)
 - SQUARE (USE FOR SANITARY SEWER AND STORM DRAINAGE PIPE SCHEDULES)



STRUCTURE SCHEDULE

#	DESCRIPTION
①	CB TYPE I RIM = 72.78 12" IE = 69.78
②	CB TYPE I RIM = 72.71 12" IE = 69.18
③	CB TYPE II RIM = 74.10 12" IE = 68.93
④	CB TYPE II RIM = 74.14 12" IE = 68.61
⑤	CB TYPE II RIM = 74.14 12" IE = 68.56 (NW) IN 12" IE = 68.56 (NE) IN 12" IE = 70.87 (SW) IN 12" IE = 68.56 (W) OUT
⑥	CB TYPE I RIM = 74.17 12" IE = 71.17
⑦	CB TYPE I RIM = 74.77 12" IE = 71.08
⑧	CB TYPE I RIM = 74.25 12" IE = 71.25
⑨	OIL/WATER TREATMENT RIM = 74.10 12" IE = 68.50 (IN) 12" IE = 68.50 (OUT)
⑩	MODULAR WETLAND FACILITY RIM = 74.28 (LOW) RIM = 74.59 (HIGH) 12" IE = 68.48 (IN) 12" IE = 66.88 (OUT)
⑪	30'x106'x8.5' DETENTION VAULT TOP OF VAULT = 73.00 12" IE = 66.74 (IN) 6" IE = 68.00 (IN) 6" IE = 68.00 (IN) 6" IE = 68.00 (IN) 18" IE = 66.24 (OUT) 12" IE = 71.95 (EMERGENCY) 18" ACCESS RISER IN VAULT HEIGHT DIA. ORIFICE 1 0' 1.50" ORIFICE 1 6' 2.25" ORIFICE 1 6.75' 1.38"
⑫	PUMP IN 60" MH 18" IE = 66.24 (IN) 3" FORCE MAIN (OUT)
⑬	CB TYPE II (SOLID RIM) RIM = 73.73 12" IE = 71.63 (IN) 3" FORCE MAIN (IN) 12" IE = 71.63 (OUT)
⑭	FLOW SPLITTER RIM = 72.84 12" IE = 70.73
⑮	DISPERSION TRENCH 12" IE = 70.69
⑯	DISPERSION TRENCH 12" IE = 70.28

PIPE SCHEDULE

PIPE #	DIAMETER	LENGTH (FT.)	SLOPE (FT./FT.)
1	12" PVC	121	.005
2	12" PVC	104	.005
3	12" PVC	10	.005
4	12" PVC	10	.005
5	12" PVC	60	.005
6	12" PVC	92	.027
7	12" PVC	34	.005
8	12" PVC	12	.005
9	12" PVC	4	.005
10	12" PVC	28	.005
11	18" PVC	4	.000
12	12" D.I.	64	.005
13	3" PVC	62	---
14	12" D.I.	180	.005
15	12" PVC	7	.005
16	12" PVC	89	.005
17	6" PVC	38	.005
18	6" PVC	60	.020
19	6" PVC	63	.020
20	6" PVC	2	.020
21	6" PVC	87	.020

PACLAND
T (425) 453-9001
F (425) 453-8208
www.PacLand.com
11400 SE 8th St.
Suite 345
Bellevue, WA 98004

GILMAN POINT
SELF-STORAGE and LES SCHWAB TIRE CENTER
160 NW GILMAN BOULEVARD
ISSAQUAH, WASHINGTON 98027

REVISIONS

NO.	DATE	BY
1		
2		
3		
4		
5		
6		

ISSUE DATES

DESIGN APPROVAL: _____
 PERMIT SUBMITTAL: _____
 PERMIT RECEIVED: _____
 BID DOCS: _____
 CONSTR. DOCS: _____

24"x36" SCALE: AS NOTED

PLOT DATE: 03-05-2015

CAD FILE: GPIS_120G

JOB NUMBER: 14-140

CHECKED: _____

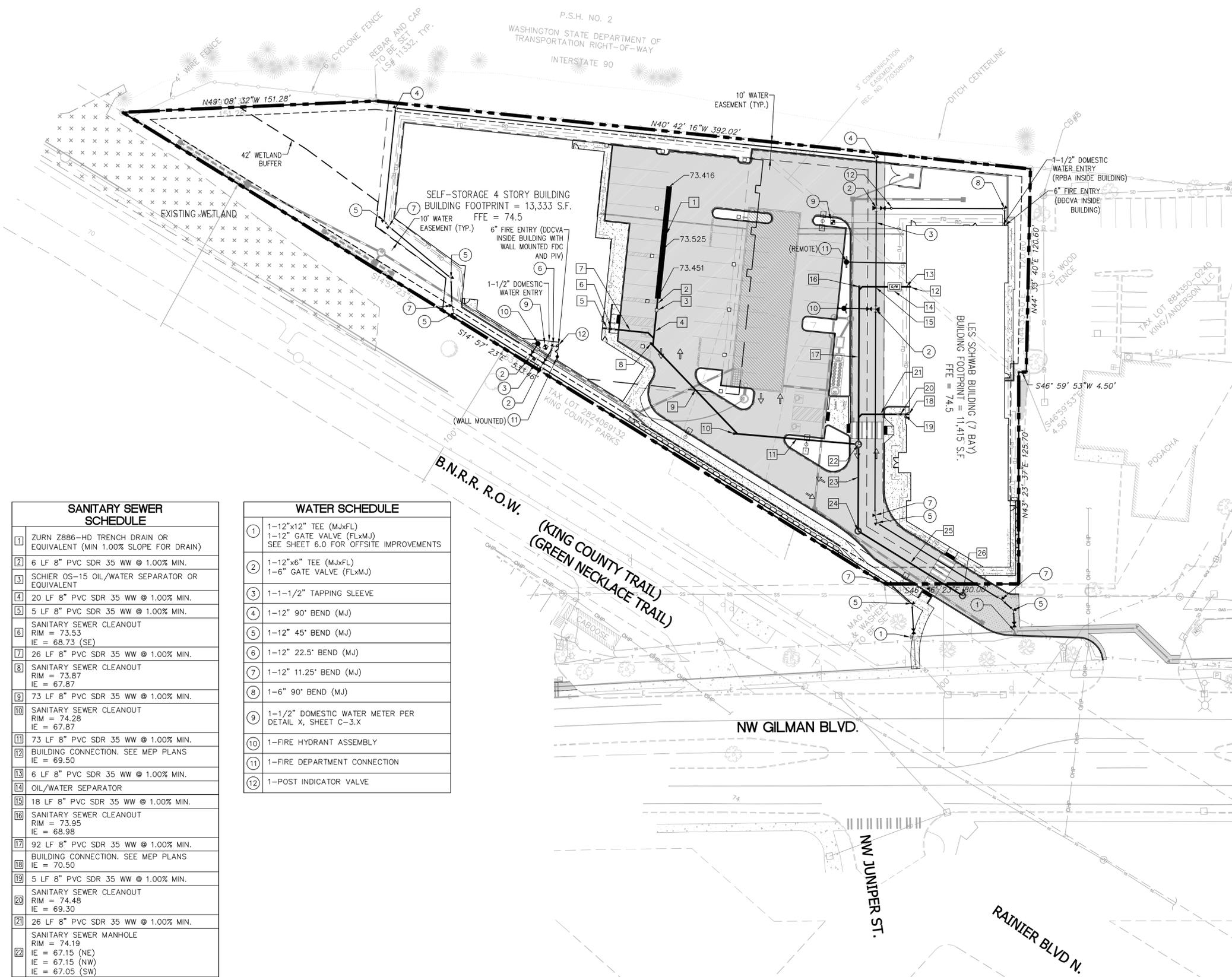
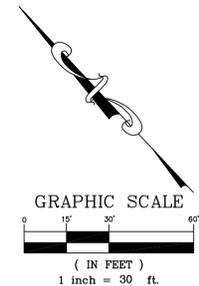
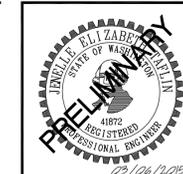
DRAWN: _____

STATUS: SCHEMATIC DESIGN



STORMWATER AND GRADING PLAN
C-4.0

SE 1/4 OF NE 1/4 OF SECTION 28, TOWNSHIP 24N, RANGE 6E, W.M.



- LEGEND:**
- XX" WA D.I. WATER MAIN PIPE
 - X" DOM WA DOMESTIC WATER PIPE
 - Water Meter Symbol WATER METER
 - Water Tee Symbol WATER TEE
 - Gate Valve Symbol GATE VALVE
 - Reducer Symbol REDUCER
 - Fire Hydrant Symbol FIRE HYDRANT
 - Fire Department Connection Symbol FIRE DEPARTMENT CONNECTION
 - Post Indicator Valve Symbol POST INDICATOR VALVE
 - 90° Bend Symbol 90° BEND
 - 45° Bend Symbol 45° BEND
 - 22.5° Bend Symbol 22.5° BEND
 - 11.25° Bend Symbol 11.25° BEND
 - DDCVA Symbol DDCVA
 - RBPA Symbol RBPA
 - Circle with Number Symbol CIRCLE (USE NUMBERS FOR WATER PLAN AND LETTERING FOR SITE PLAN)
 - Sanitary Sewer Pipe Symbol SANITARY SEWER PIPE
 - Sanitary Sewer Cleanout Symbol SANITARY SEWER CLEANOUT
 - Sanitary Sewer Manhole Symbol SANITARY SEWER MANHOLE
 - Oil Water Separator Symbol OIL WATER SEPARATOR
 - Square with Number Symbol SQUARE (USE FOR SANITARY SEWER AND STORM DRAINAGE PIPE SCHEDULES)

ALL WATER MAIN PIPE SHALL BE RESTRAINED JOINT (RJ)

SANITARY SEWER SCHEDULE	
1	ZURN Z886-HD TRENCH DRAIN OR EQUIVALENT (MIN 1.00% SLOPE FOR DRAIN)
2	6 LF 8" PVC SDR 35 WW @ 1.00% MIN.
3	SCHIER OS-15 OIL/WATER SEPARATOR OR EQUIVALENT
4	20 LF 8" PVC SDR 35 WW @ 1.00% MIN.
5	5 LF 8" PVC SDR 35 WW @ 1.00% MIN.
6	SANITARY SEWER CLEANOUT RIM = 73.53 IE = 68.73 (SE)
7	26 LF 8" PVC SDR 35 WW @ 1.00% MIN.
8	SANITARY SEWER CLEANOUT RIM = 73.87 IE = 67.87
9	73 LF 8" PVC SDR 35 WW @ 1.00% MIN.
10	SANITARY SEWER CLEANOUT RIM = 74.28 IE = 67.87
11	73 LF 8" PVC SDR 35 WW @ 1.00% MIN.
12	BUILDING CONNECTION. SEE MEP PLANS IE = 69.50
13	6 LF 8" PVC SDR 35 WW @ 1.00% MIN.
14	OIL/WATER SEPARATOR
15	18 LF 8" PVC SDR 35 WW @ 1.00% MIN.
16	SANITARY SEWER CLEANOUT RIM = 73.95 IE = 68.98
17	92 LF 8" PVC SDR 35 WW @ 1.00% MIN.
18	BUILDING CONNECTION. SEE MEP PLANS IE = 70.50
19	5 LF 8" PVC SDR 35 WW @ 1.00% MIN.
20	SANITARY SEWER CLEANOUT RIM = 74.48 IE = 69.30
21	26 LF 8" PVC SDR 35 WW @ 1.00% MIN.
22	SANITARY SEWER MANHOLE RIM = 74.19 IE = 67.15 (NE) IE = 67.15 (NW) IE = 67.05 (SW)
23	51 LF 8" PVC SDR 35 WW @ 1.00% MIN.
24	SANITARY SEWER MANHOLE RIM = 67.81 IE = 66.53 (NE) IE = 66.43 (SE)
25	73 LF 8" PVC SDR 35 WW @ 1.00% MIN.
26	SANITARY SEWER MANHOLE RIM = 73.36 IE = ±65.70 (NW) IE = ±65.70 (SE) IE = ±65.70 (SW) CONNECT TO EXISTING SANITARY SEWER MAIN LOCATION PRIOR TO CONSTRUCTION OF ANY PORTION OF PROPOSED SEWER MAIN EXTENSION AND NOTIFY ENGINEER OF ANY DISCREPANCIES.

WATER SCHEDULE	
1	1-12"x12" TEE (MJxFL) 1-12" GATE VALVE (FLxMJ) SEE SHEET 6.0 FOR OFFSITE IMPROVEMENTS
2	1-12"x6" TEE (MJxFL) 1-6" GATE VALVE (FLxMJ)
3	1-1-1/2" TAPPING SLEEVE
4	1-12" 90° BEND (MJ)
5	1-12" 45° BEND (MJ)
6	1-12" 22.5° BEND (MJ)
7	1-12" 11.25° BEND (MJ)
8	1-6" 90° BEND (MJ)
9	1-1/2" DOMESTIC WATER METER PER DETAIL X, SHEET C-3.X
10	1-FIRE HYDRANT ASSEMBLY
11	1-FIRE DEPARTMENT CONNECTION
12	1-POST INDICATOR VALVE

PACLAND
 T (425) 453-9001
 F (425) 453-8208
 www.PacLand.com
 11400 SE 8th St.
 Suite 345
 Bellevue, WA 98004

GILMAN POINT
 SELF-STORAGE and LES SCHWAB TIRE CENTER
 160 NW GILMAN BOULEVARD
 ISSAQUAH, WASHINGTON 98027

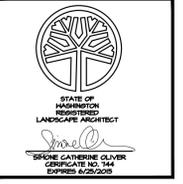
REVISIONS		
NO.	DATE	BY
1		
2		
3		
4		
5		
6		

ISSUE DATES	
DESIGN APPROVAL:	
PERMIT SUBMITTAL:	
PERMIT RECEIVED:	
BID DOCS:	
CONSTR. DOCS:	

24"x36" SCALE:	1" = 30'
PLOT DATE:	03-05-2015
CAD FILE:	GPIS_130U
JOB NUMBER:	14-140
CHECKED:	
DRAWN:	
STATUS:	SCHEMATIC DESIGN



WATER, SEWER, AND OTHER UTILITY PLAN
C-5.0



© Copyright - Altmann Oliver Associates, LLC.

AOA
Environmental
Planning &
Landscape
Architecture

**Altmann
Oliver
Associates,
LLC**

PO Box 578
Carnation, WA 98014
Office (425) 333-4555
Fax (425) 333-4509

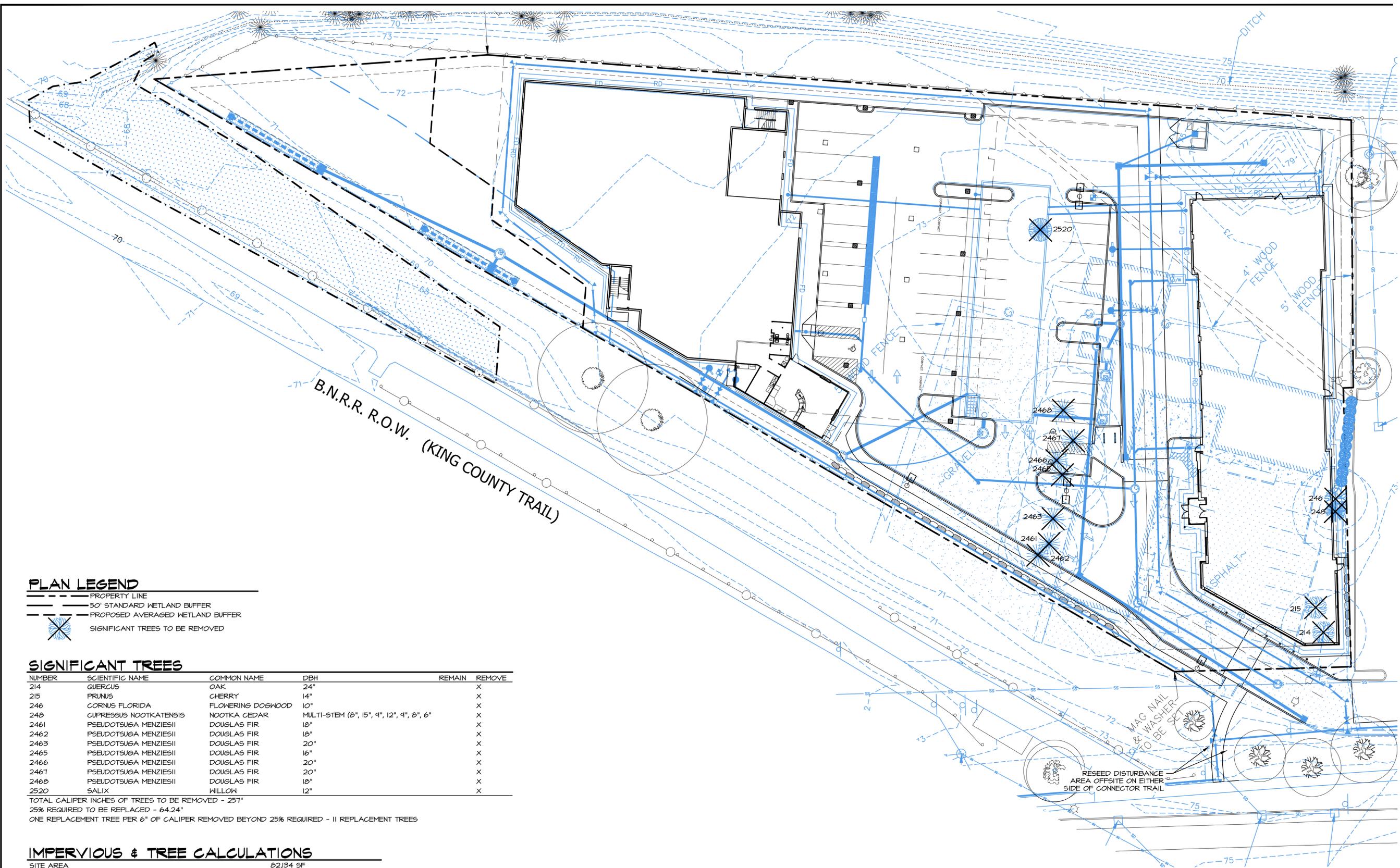
GILMAN POINT

SELF-STORAGE and LES SCHWAB TIRE CENTER
160 NW GILMAN BOULEVARD
ISSAQUAH, WASHINGTON 98027

REVISIONS		
NO.	DATE	BY
1		
2		
3		
4		
5		
6		

24"x36" SCALE:	AS NOTED
PLOT DATE:	03-09-2015
CAD FILE:	4558-LA-03-09-15
JOB NUMBER:	14-140
CHECKED:	
DRAWN:	
STATUS:	SCHEMATIC DESIGN

PRELIMINARY
TREE PLAN
L1.1



PLAN LEGEND

- PROPERTY LINE
- 50' STANDARD WETLAND BUFFER
- PROPOSED AVERAGED WETLAND BUFFER
- X SIGNIFICANT TREES TO BE REMOVED

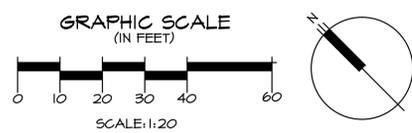
SIGNIFICANT TREES

NUMBER	SCIENTIFIC NAME	COMMON NAME	DBH	REMAIN	REMOVE
214	QUERCUS	OAK	24"		X
215	PRUNUS	CHERRY	14"		X
246	CORNUS FLORIDA	FLOWERING DOGWOOD	10"		X
248	CUPRESSUS NOOTKATENSIS	NOOTKA CEDAR	MULTI-STEM (Ø", 15", 9", 12", 9", Ø", 6"		X
2461	PSEUDOTSUGA MENZIESII	DOUGLAS FIR	18"		X
2462	PSEUDOTSUGA MENZIESII	DOUGLAS FIR	18"		X
2463	PSEUDOTSUGA MENZIESII	DOUGLAS FIR	20"		X
2465	PSEUDOTSUGA MENZIESII	DOUGLAS FIR	16"		X
2466	PSEUDOTSUGA MENZIESII	DOUGLAS FIR	20"		X
2467	PSEUDOTSUGA MENZIESII	DOUGLAS FIR	20"		X
2468	PSEUDOTSUGA MENZIESII	DOUGLAS FIR	18"		X
2520	SALIX	WILLOW	12"		X

TOTAL CALIPER INCHES OF TREES TO BE REMOVED - 257"
 25% REQUIRED TO BE REPLACED - 64.24"
 ONE REPLACEMENT TREE PER 6" OF CALIPER REMOVED BEYOND 25% REQUIRED - 11 REPLACEMENT TREES

IMPERVIOUS & TREE CALCULATIONS

SITE AREA	82,134 SF
TOTAL PERVIOUS	20,276 SF
TOTAL IMPERVIOUS AREA	61,858 SF (75.31%)
FOUR TREES PER 5,000 SF OF SITE AREA OUTSIDE BUFFER	66 TREES REQUIRED

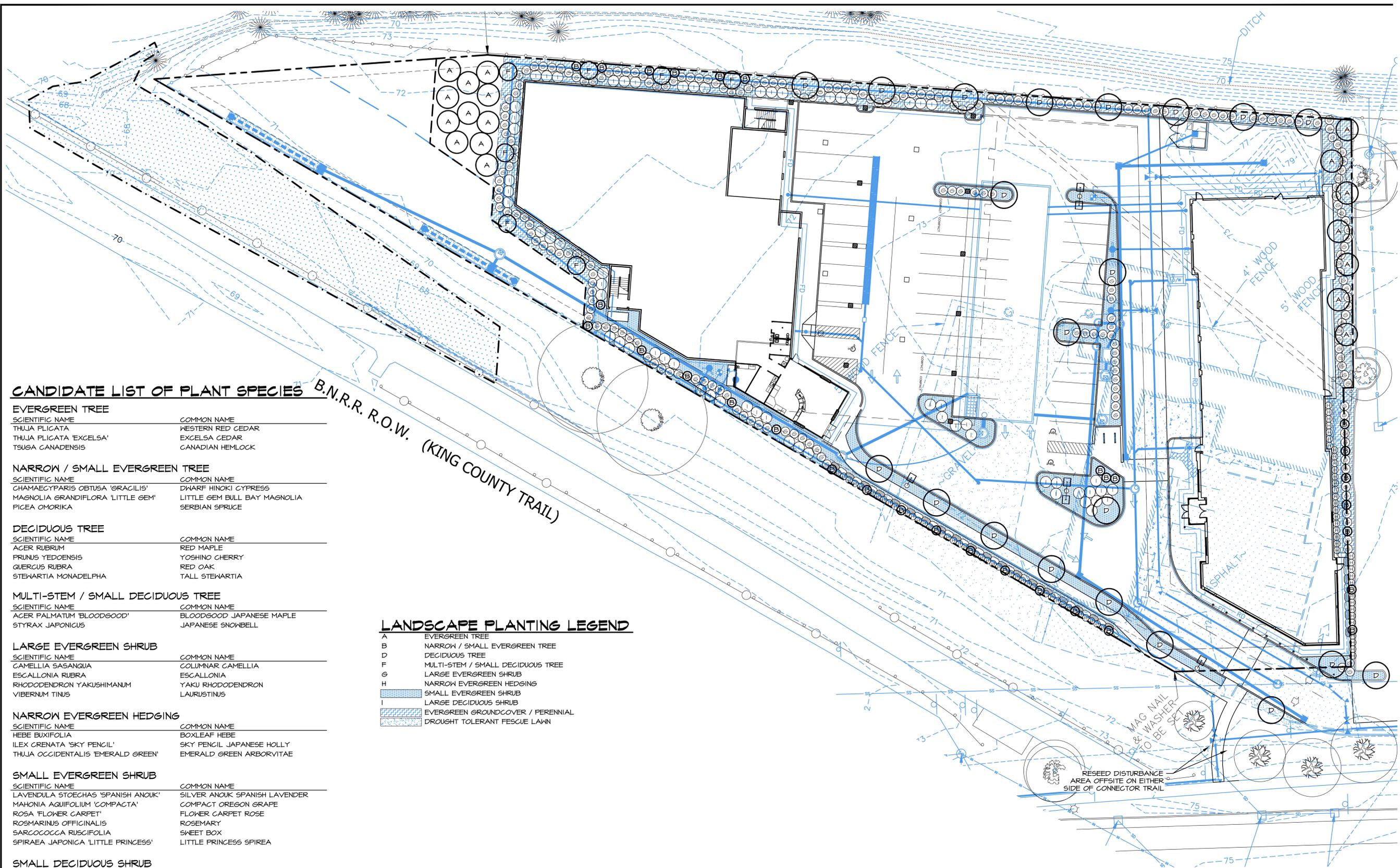


SHEET INDEX

SHEET NUMBER	SHEET TITLE
L1.1	PRELIMINARY TREE PLAN
L1.2	PRELIMINARY LANDSCAPE PLAN

GENERAL NOTES

1. BASE INFORMATION PROVIDED BY MAGELLAN ARCHITECTS, 8383 158TH AVE. NE, SUITE 280, REDMOND, WA 98052, (425) 855-4300.



CANDIDATE LIST OF PLANT SPECIES

EVERGREEN TREE

SCIENTIFIC NAME	COMMON NAME
THUJA PLICATA	WESTERN RED CEDAR
THUJA PLICATA 'EXCELSA'	EXCELSA CEDAR
TSUGA CANADENSIS	CANADIAN HEMLOCK

NARROW / SMALL EVERGREEN TREE

SCIENTIFIC NAME	COMMON NAME
CHAMAECYPARIS OBTUSA 'GRACILIS'	DWARF HINOKI CYPRESS
MAGNOLIA GRANDIFLORA 'LITTLE GEM'	LITTLE GEM BULL BAY MAGNOLIA
PICEA OMORIKA	SERBIAN SPRUCE

DECIDUOUS TREE

SCIENTIFIC NAME	COMMON NAME
ACER RUBRUM	RED MAPLE
PRUNUS YEDOENSIS	YOSHINO CHERRY
QUERCUS RUBRA	RED OAK
STEWARTIA MONADELPHA	TALL STEWARTIA

MULTI-STEM / SMALL DECIDUOUS TREE

SCIENTIFIC NAME	COMMON NAME
ACER PALMATUM 'BLOODGOOD'	BLOODGOOD JAPANESE MAPLE
STYRAX JAPONICUS	JAPANESE SNOWBELL

LARGE EVERGREEN SHRUB

SCIENTIFIC NAME	COMMON NAME
CAMELLIA SASANQUA	COLUMNAR CAMELLIA
ESCALLONIA RUBRA	ESCALLONIA
RHODODENDRON YAKUSHIMANUM	YAKU RHODODENDRON
VIBERNUM TINUS	LAURUSTINUS

NARROW EVERGREEN HEDGING

SCIENTIFIC NAME	COMMON NAME
HEBE BUXTIFOLIA	BOXLEAF HEBE
ILEX CRENATA 'SKY PENCIL'	SKY PENCIL JAPANESE HOLLY
THUJA OCCIDENTALIS 'EMERALD GREEN'	EMERALD GREEN ARBORVITAE

SMALL EVERGREEN SHRUB

SCIENTIFIC NAME	COMMON NAME
LAVENDULA STOECHAS 'SPANISH ANOUK'	SILVER ANOUK SPANISH LAVENDER
MAHONIA AQUIFOLIUM 'COMPACTA'	COMPACT OREGON GRAPE
ROSA 'FLOWER CARPET'	FLOWER CARPET ROSE
ROSMARINUS OFFICINALIS	ROSEMARY
SARCOCOCCA RUSCIFOLIA	SHEET BOX
SPIRAEA JAPONICA 'LITTLE PRINCESS'	LITTLE PRINCESS SPIREA

SMALL DECIDUOUS SHRUB

SCIENTIFIC NAME	COMMON NAME
PANICUM 'HEAVY METAL'	HEAVY METAL SWITCHGRASS

EVERGREEN GROUNDCOVER / PERENNIALS

SCIENTIFIC NAME	COMMON NAME
ARCHTOSTAPHYLOS U. 'MASSACHUSETTS'	MASSACHUSETTS KINNICKINNIK
ASTILBE ARENDsii HYBRIDS	ASTILBE
BERBERIS THUNBERGII 'CONCORDE'	PURPLE LEAF JAPANESE BARBERRY
BERGENIA CORDIFOLIA 'WINTERGLUT'	HEARTLEAF BERGENIA
DAPHNE X TRANS. 'ETERNAL FRAGRANCE'	ETERNAL FRAGRANCE DAPHNE
ECHINACEA 'WHITE SWAN'	WHITE SWAN CONEFLOWER
HELLEBORUS ARGUTIFOLIUS 'PAC. FROST'	PACIFIC FROST HELLEBORE
HOSTA 'FRANCEE, PATRIOT OR SAGAE'	FRANCEE, PATRIOT OR SAGAE HOSTA
IBERIS SEMPERVIRENS 'SNOWFLAKE'	SNOWFLAKE CANDY TUFT
PENNISETUM ALOPECUROIDES 'PIGLET'	PIGLET FOUNTAIN GRASS
RUDBECKIA HIRTA	GLORIOSA DAISY

LANDSCAPE PLANTING LEGEND

- A EVERGREEN TREE
- B NARROW / SMALL EVERGREEN TREE
- D DECIDUOUS TREE
- F MULTI-STEM / SMALL DECIDUOUS TREE
- G LARGE EVERGREEN SHRUB
- H NARROW EVERGREEN HEDGING
- I SMALL EVERGREEN SHRUB
- J LARGE DECIDUOUS SHRUB
- K EVERGREEN GROUNDCOVER / PERENNIAL
- L DROUGHT TOLERANT FESCUE LAWN

B.N.R.R. R.O.W. (KING COUNTY TRAIL)



© Copyright - Altmann Oliver Associates, LLC.

AOA
Environmental
Planning &
Landscape
Architecture

**Altmann
Oliver
Associates,
LLC**

PO Box 578
Carnation, WA 98014
Office (425) 333-4555
Fax (425) 333-4549

GILMAN POINT

SELF-STORAGE and LES SCHWAB TIRE CENTER
160 NW GILMAN BOULEVARD
ISSAQUAH, WASHINGTON 98027

REVISIONS

NO.	DATE	BY
1		
2		
3		
4		
5		
6		

ISSUE DATES

DESIGN APPROVAL:

PERMIT SUBMITTAL:

PERMIT RECEIVED:

BID DOCS:

CONSTR. DOCS:

24"x36" SCALE: AS NOTED

PLOT DATE: 03-09-2015

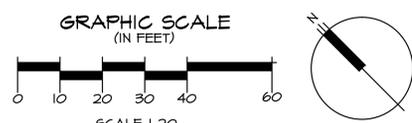
CAD FILE: 4558-LA-03-09-15

JOB NUMBER: 14-140

CHECKED:

DRAWN:

STATUS: SCHEMATIC DESIGN



GENERAL NOTES

1. BASE INFORMATION PROVIDED BY MAGELLAN ARCHITECTS, 8383 158TH AVE. NE, SUITE 280, REDMOND, WA 98052, (425) 855-4300.

PRELIMINARY
LANDSCAPE PLAN
L1.2



© Copyright - Altmann Oliver Associates, LLC.

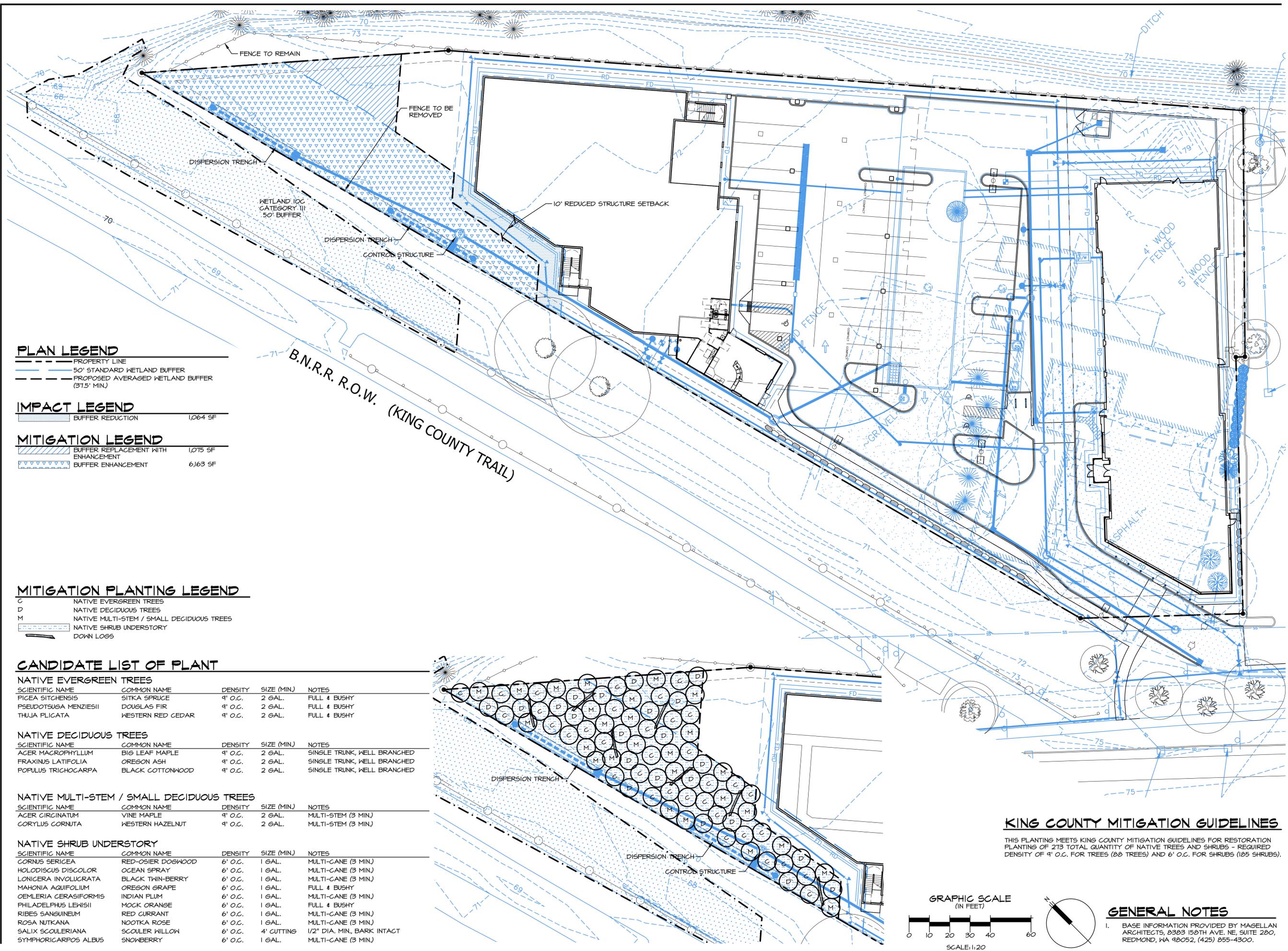
AOA
Environmental
Planning &
Landscape
Architecture

**Altmann
Oliver
Associates,
LLC**

PO Box 578
Carnation, WA 98014
Office (425) 333-4555
Fax (425) 333-4509

GILMAN POINT

SELF-STORAGE and LES SCHWAB TIRE CENTER
160 NW GILMAN BOULEVARD, ISSAQUAH, WASHINGTON 98027



PLAN LEGEND

- PROPERTY LINE
- 50' STANDARD WETLAND BUFFER
- PROPOSED AVERAGED WETLAND BUFFER (37.5' MIN.)

IMPACT LEGEND

- [Pattern] BUFFER REDUCTION 1,064 SF

MITIGATION LEGEND

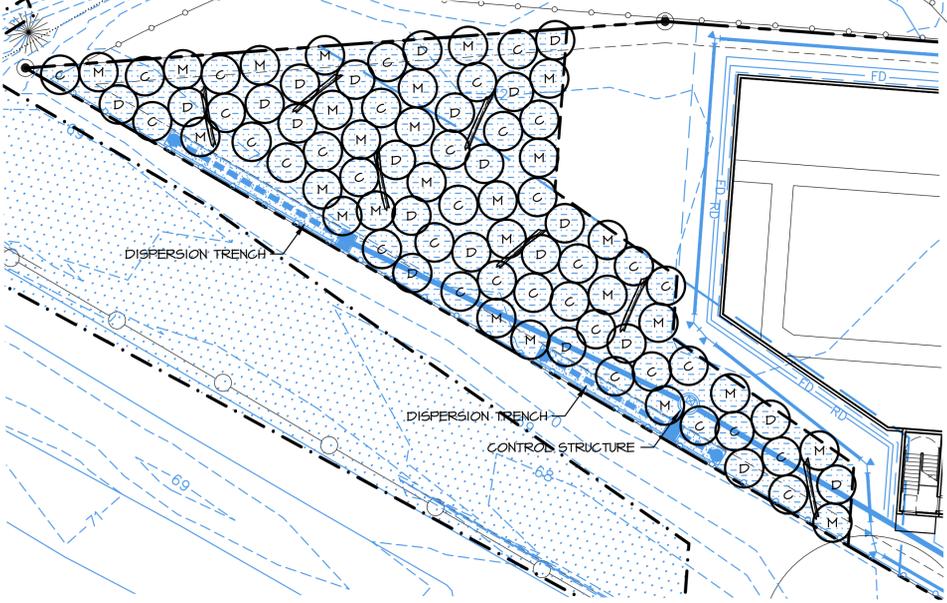
- [Pattern] BUFFER REPLACEMENT WITH ENHANCEMENT 1,075 SF
- [Pattern] BUFFER ENHANCEMENT 6,163 SF

MITIGATION PLANTING LEGEND

- C NATIVE EVERGREEN TREES
- D NATIVE DECIDUOUS TREES
- M NATIVE MULTI-STEM / SMALL DECIDUOUS TREES
- [Pattern] NATIVE SHRUB UNDERSTORY
- DOWN LOGS

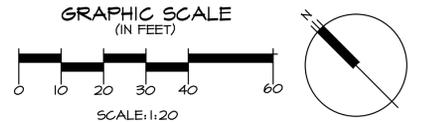
CANDIDATE LIST OF PLANT

NATIVE EVERGREEN TREES				
SCIENTIFIC NAME	COMMON NAME	DENSITY	SIZE (MIN.)	NOTES
PICEA SITCHENSIS	SITKA SPRUCE	9' O.C.	2 GAL.	FULL & BUSHY
PSEUDOTSUGA MENZIESII	DOUGLAS FIR	9' O.C.	2 GAL.	FULL & BUSHY
THUJA PLICATA	WESTERN RED CEDAR	9' O.C.	2 GAL.	FULL & BUSHY
NATIVE DECIDUOUS TREES				
SCIENTIFIC NAME	COMMON NAME	DENSITY	SIZE (MIN.)	NOTES
ACER MACROPHYLLUM	BIG LEAF MAPLE	9' O.C.	2 GAL.	SINGLE TRUNK, WELL BRANCHED
FRAXINUS LATIFOLIA	OREGON ASH	9' O.C.	2 GAL.	SINGLE TRUNK, WELL BRANCHED
POPULUS TRICHOCARPA	BLACK COTTONWOOD	9' O.C.	2 GAL.	SINGLE TRUNK, WELL BRANCHED
NATIVE MULTI-STEM / SMALL DECIDUOUS TREES				
SCIENTIFIC NAME	COMMON NAME	DENSITY	SIZE (MIN.)	NOTES
ACER CIRCINATUM	VINE MAPLE	9' O.C.	2 GAL.	MULTI-STEM (3 MIN.)
CORYLUS CORNUTA	WESTERN HAZELNUT	9' O.C.	2 GAL.	MULTI-STEM (3 MIN.)
NATIVE SHRUB UNDERSTORY				
SCIENTIFIC NAME	COMMON NAME	DENSITY	SIZE (MIN.)	NOTES
CORNUS SERICEA	RED-OSIER DOGWOOD	6' O.C.	1 GAL.	MULTI-CANE (3 MIN.)
HOLODISCUS DISCOLOR	OCEAN SPRAY	6' O.C.	1 GAL.	MULTI-CANE (3 MIN.)
LONICERA INVOLUCRATA	BLACK TWIN-BERRY	6' O.C.	1 GAL.	MULTI-CANE (3 MIN.)
MAHONIA AQUIFOLIUM	OREGON GRAPE	6' O.C.	1 GAL.	FULL & BUSHY
OEMLERIA CERASIFORMIS	INDIAN PLUM	6' O.C.	1 GAL.	MULTI-CANE (3 MIN.)
PHILADELPHUS LEWISII	MOCK ORANGE	6' O.C.	1 GAL.	FULL & BUSHY
RIBES SANGUINEUM	RED CURRANT	6' O.C.	1 GAL.	MULTI-CANE (3 MIN.)
ROSA NUTKANA	NOOTKA ROSE	6' O.C.	1 GAL.	MULTI-CANE (3 MIN.)
SALIX SCOULERIANA	SCOULER WILLOW	6' O.C.	4" CUTTING	1/2" DIA. MIN., BARK INTACT
SYMPHORICARPOS ALBUS	SNOWBERRY	6' O.C.	1 GAL.	MULTI-CANE (3 MIN.)



KING COUNTY MITIGATION GUIDELINES

THIS PLANTING MEETS KING COUNTY MITIGATION GUIDELINES FOR RESTORATION PLANTING OF 273 TOTAL QUANTITY OF NATIVE TREES AND SHRUBS - REQUIRED DENSITY OF 9' O.C. FOR TREES (80 TREES) AND 6' O.C. FOR SHRUBS (185 SHRUBS).



GENERAL NOTES

- BASE INFORMATION PROVIDED BY MAGELLAN ARCHITECTS, 8383 158TH AVE. NE, SUITE 280, REDMOND, WA 98052, (425) 855-4300.

REVISIONS		
NO.	DATE	BY
1		
2		
3		
4		
5		

ISSUE DATES	
DESIGN APPROVAL:	
PERMIT SUBMITTAL:	
PERMIT RECEIVED:	
BID DOCS:	
CONSTR. DOCS:	

24"x36" SCALE: AS NOTED
PLOT DATE: 03-09-2015
CAD FILE: 4568-MIT-03-09-15
JOB NUMBER: 14-140
CHECKED:
DRAWN:
STATUS: SCHEMATIC DESIGN

PRELIMINARY WETLAND BUFFER MITIGATION PLAN
W1.1



Copyright © 2015, Magellan Architects PLLC
 Architect for this project are instruments of the
 State of Washington. This project is not to be
 used for any other project without the
 written consent of Magellan Architects PLLC.
 This project is not to be used for any other
 project without the written consent of
 Magellan Architects PLLC. The architect
 does not warrant the accuracy of the
 information provided in this set of plans
 and is not responsible for any errors or
 omissions. The architect is not
 responsible for any construction delays
 or cost overruns. The architect is not
 responsible for any construction defects
 or other construction issues. The
 architect is not responsible for any
 construction accidents or injuries.
 The architect is not responsible for any
 construction claims or lawsuits.

Magellan
 ARCHITECTS
 8383 158th Avenue Northeast, Suite 280
 Redmond, Washington 98052
 Tel (425) 885-4300 Fax (425) 885-4303
 www.magellanarchitects.com
 Copyright © 2015 Magellan Architects PLLC

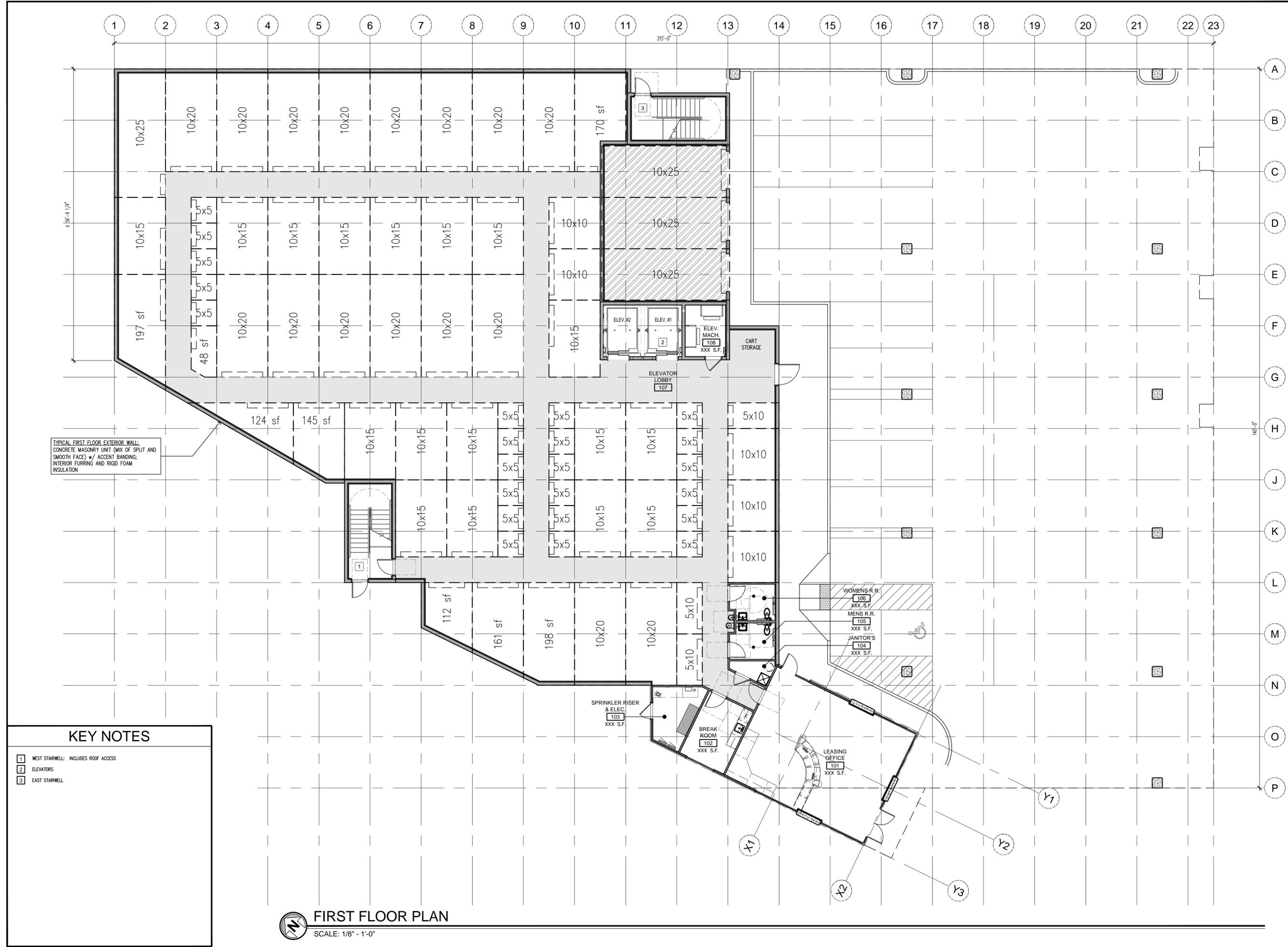
GILMAN POINT
 SELF-STORAGE and LES SCHWAB TIRE CENTER
 160 NW GILMAN BOULEVARD
 ISSAQUAH, WASHINGTON 98027

REVISIONS		
NO.	DATE	BY
1		
2		
3		
4		
5		
6		

ISSUE DATES
 DESIGN APPROVAL:
 PERMIT SUBMITTAL:
 PERMIT RECEIVED:
 BID DOCS:
 CONSTR. DOCS:

24"x36" SCALE:	AS NOTED
PLOT DATE:	03-10-2015
CAD FILE:	14-140_A21_ASDP
JOB NUMBER:	14-140
CHECKED:	
DRAWN:	DYM
STATUS:	ISSUE FOR ASDP

STORAGE BUILDING
 FIRST FLOOR PLAN
AA2.1



KEY NOTES

- 1 WEST STAIRWELL: INCLUDES ROOF ACCESS
- 2 ELEVATORS
- 3 EAST STAIRWELL

FIRST FLOOR PLAN
 SCALE: 1/8" = 1'-0"



Copyright © 2015, Magellan Architects PLLC
 Architect for this project are instruments of the State of Washington.
 This project is for use solely with respect to the project identified in the title block. No part of this project may be reproduced, stored in a retrieval system, or transmitted in any form or by any means, electronic or mechanical, including photocopying, recording, or by any information storage and retrieval system, without the express written approval of Magellan Architects PLLC. Any use of this project for any other purpose may be made without prior written permission. The purchaser is to use this set of plans for the construction of the project identified in the title block. Magellan Architects PLLC does not warrant the additional set(s) of plans shall not entitle the purchaser to construct more than one building.

Magellan
 ARCHITECTS
 8383 158th Avenue Northeast, Suite 280
 Redmond, Washington 98052
 Tel (425) 885-4300 Fax (425) 885-4303
 www.magellanarchitects.com
 Copyright © 2015 Magellan Architects PLLC

GILMAN POINT
 SELF-STORAGE and LES SCHWAB TIRE CENTER
 160 NW GILMAN BOULEVARD
 ISSAQUAH, WASHINGTON 98027

REVISIONS		
NO.	DATE	BY
1		
2		
3		
4		
5		

ISSUE DATES
 DESIGN APPROVAL:
 PERMIT SUBMITTAL:
 PERMIT RECEIVED:
 BID DOCS:
 CONSTR. DOCS:

24"x36" SCALE:	AS NOTED
PLOT DATE:	03-10-2015
CAD FILE:	14-140_A21_ASDP
JOB NUMBER:	14-140
CHECKED:	
DRAWN:	DYM
STATUS:	ISSUE FOR ASDP

STORAGE BUILDING
 SECOND FLOOR PLAN
AA2.2



TYPICAL EXTERIOR WALL FLOORS 2-4:
 3.625" STEEL STUDS w/ 5/8" GYPSUM SHEATHING, 2" R-10 RIGID FOAM CONTINUOUS INSULATION AT THE EXTERIOR BTW Z-FURRING @ 24" O.C., CONTINUOUS W.R.B., ARCHITECTURAL CLADDING PER ELEVATIONS

KEY NOTES

1 WEST STAIRWELL: INCLUDES ROOF ACCESS
 2 ELEVATORS
 3 EAST STAIRWELL

SECOND FLOOR PLAN
 SCALE: 1/8" = 1'-0"



Copyright © 2015, Magellan Architects PLLC
 Architect for this project are instruments of the State of Washington.
 This project is for use solely with respect to the project and may not be reproduced, stored in a retrieval system, or transmitted in any form or by any means, electronic or mechanical, including photocopying, recording, or by any information storage and retrieval system, without the express written approval of Magellan Architects PLLC. Any use of this project for other purposes may be made without prior written permission. The purchaser is to use this set of plans for the construction of the project and is not to be used for any other project. Plans shall not be used to construct more than one building.

Magellan
 ARCHITECTS
 8383 158th Avenue Northeast, Suite 280
 Redmond, Washington 98052
 Tel (425) 885-4300 Fax (425) 885-4303
 www.magellanarchitects.com
 Copyright © 2015 Magellan Architects PLLC

GILMAN POINT
 SELF-STORAGE and LES SCHWAB TIRE CENTER
 160 NW GILMAN BOULEVARD
 ISSAQUAH, WASHINGTON 98027

REVISIONS		
NO.	DATE	BY
1		
2		
3		
4		
5		
6		

ISSUE DATES
 DESIGN APPROVAL:
 PERMIT SUBMITTAL:
 PERMIT RECEIVED:
 BID DOCS:
 CONSTR. DOCS:

24"x36" SCALE:	AS NOTED
PLOT DATE:	03-10-2015
CAD FILE:	14-140_A21_ASDP
JOB NUMBER:	14-140
CHECKED:	
DRAWN:	DYM
STATUS:	ISSUE FOR ASDP

STORAGE BUILDING
 THIRD FLOOR PLAN
AA2.3



KEY NOTES

1 WEST STAIRWELL: INCLUDES ROOF ACCESS
 2 ELEVATORS
 3 EAST STAIRWELL

THIRD FLOOR PLAN
 SCALE: 1/8" = 1'-0"



Copyright © 2015, Magellan Architects PLLC
 Architect for this project are instruments of the
 State of Washington. This project is for use only with respect to
 the project and shall not be reproduced, stored in a retrieval
 system, or transmitted in any form or by any means,
 electronic or mechanical, including photocopying,
 recording, or by any information storage and retrieval
 system, without the express written approval of Magellan
 Architects PLLC. Any use of this project for any other
 purpose, in whole or in part, without the express written
 permission of Magellan Architects PLLC, is prohibited.
 The purchaser, to use this set of plans, for the
 construction of a building, shall be responsible for the
 additional set(s) of plans that are not included in the
 purchase to construct more than one building.

Magellan
 ARCHITECTS
 8383 158th Avenue Northeast, Suite 280
 Redmond, Washington 98052
 Tel (425) 885-4300 Fax (425) 885-4303
 www.magellanarchitects.com
 Copyright © 2015 Magellan Architects PLLC

GILMAN POINT
 SELF-STORAGE and LES SCHWAB TIRE CENTER
 160 NW GILMAN BOULEVARD
 ISSAQUAH, WASHINGTON 98027

REVISIONS		
NO.	DATE	BY
1		
2		
3		
4		
5		
6		

ISSUE DATES

DESIGN APPROVAL: _____
 PERMIT SUBMITTAL: _____
 PERMIT RECEIVED: _____
 BID DOCS: _____
 CONSTR. DOCS: _____

24"x36" SCALE:	AS NOTED
PLOT DATE:	03-10-2015
CAD FILE:	14-140_A21_ASDP
JOB NUMBER:	14-140
CHECKED:	
DRAWN:	DYM
STATUS:	ISSUE FOR ASDP

STORAGE BUILDING
 FOURTH FLOOR PLAN
AA2.4



KEY NOTES

1 WEST STAIRWELL: INCLUDES ROOF ACCESS
 2 ELEVATORS
 3 EAST STAIRWELL

FOURTH FLOOR PLAN
 SCALE: 1/8" = 1'-0"



Copyright © 2015 Magellan Associates PLLC
 Architect for this project are instruments of the
 State of Washington. This project is for use only with respect to
 the project and shall not be reproduced, stored in a retrieval
 system, transmitted in any form or by any means, electronic
 or mechanical (including photocopying, recording, or
 by any information storage and retrieval system), without the
 express written approval of Magellan Associates PLLC. The
 purchaser to use this set of plans for the construction of
 additional set(s) of plans shall not entitle the
 purchaser to construct more than one building.

Magellan
 ARCHITECTS
 8383 158th Avenue Northeast, Suite 250
 Redmond, Washington 98052
 Tel (425) 885-4300 Fax (425) 885-4303
 www.magellanarchitects.com
 Copyright © 2015 Magellan Associates PLLC

GILMAN POINT
 SELF-STORAGE and LES SCHWAB TIRE CENTER
 160 NW GILMAN BOULEVARD
 ISSAQUAH, WASHINGTON 98027

REVISIONS		
NO.	DATE	BY
1		
2		
3		
4		
5		
6		

ISSUE DATES

DESIGN APPROVAL: _____

PERMIT SUBMITTAL: _____

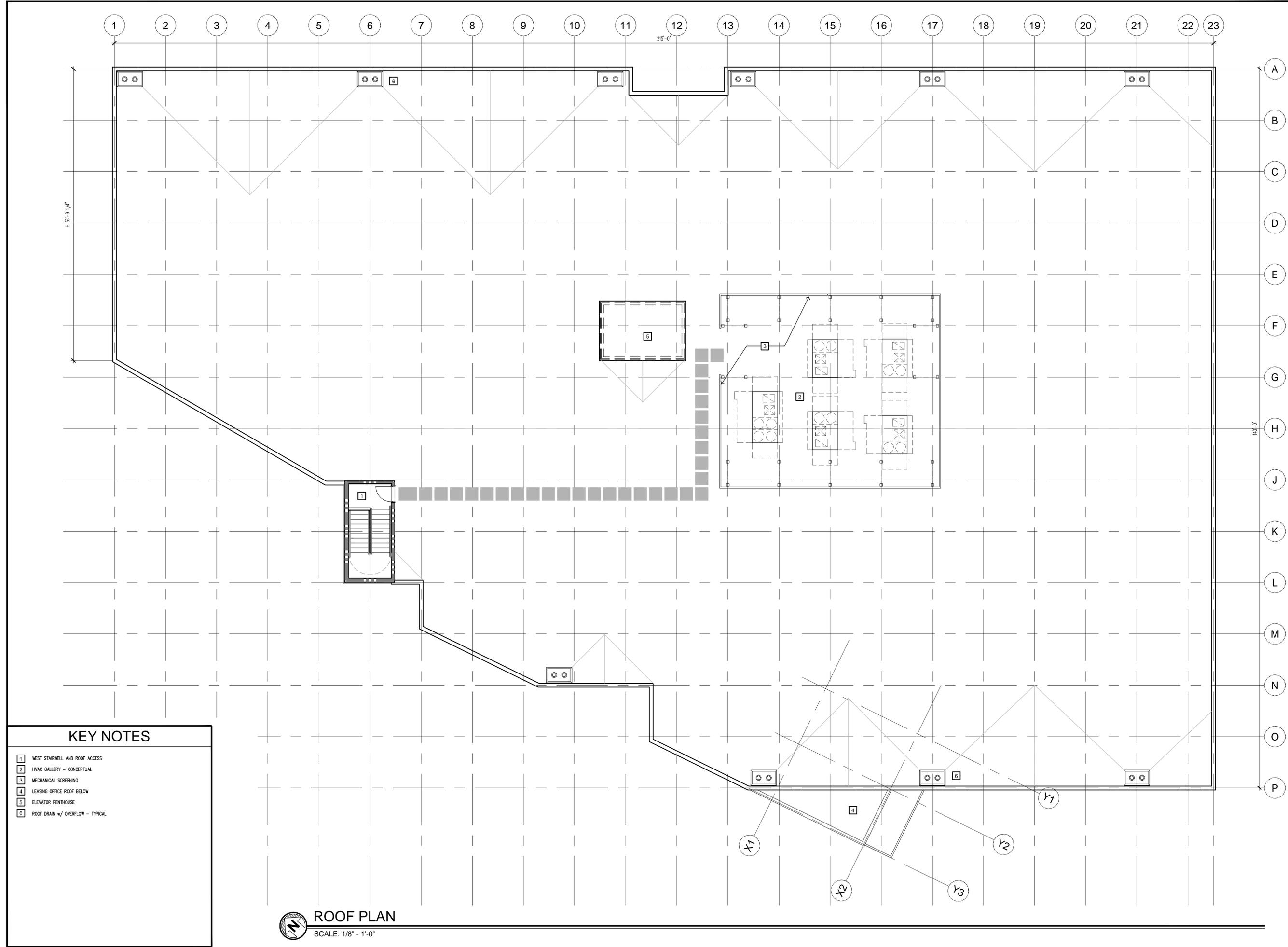
PERMIT RECEIVED: _____

BID DOCS: _____

CONSTR. DOCS: _____

24"x36" SCALE:	AS NOTED
PLOT DATE:	03-10-2015
CAD FILE:	14-140_A21_ASDP
JOB NUMBER:	14-140
CHECKED:	
DRAWN:	DYM
STATUS:	ISSUE FOR ASDP

STORAGE BUILDING
 ROOF PLAN
AA2.5



KEY NOTES

- 1 WEST STAIRWELL AND ROOF ACCESS
- 2 HVAC GALLERY - CONCEPTUAL
- 3 MECHANICAL SCREENING
- 4 LEASING OFFICE ROOF BELOW
- 5 ELEVATOR PENTHOUSE
- 6 ROOF DRAIN w/ OVERFLOW - TYPICAL

ROOF PLAN
 SCALE: 1/8" = 1'-0"



METAL PANEL

HR-36 Roof & Wall

12" No Reveal - Flat Pan

12" Coverage

COOL ZINC GREY
SRI: 39 • 24ga & 22ga

COOL PARCHMENT
SRI: 57 • 24ga & 22ga

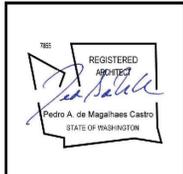
COOL COLONIAL RED
SRI: 33 • 24ga & 22ga

SPLIT FACE CMU

METAL CHANNELS AND BRACKETS

STOREFRONT
Light Gray

CONCRETE



Copyright © 2015 Magellan Associates PLLC. All rights reserved. This drawing is the property of Magellan Associates PLLC and is not to be used, copied, reproduced, or transmitted in any form or by any means, electronic or mechanical, including photocopying, recording, or by any information storage and retrieval system, without the express written approval of Magellan Associates PLLC. The design shown here is preliminary and may be made without prior written permission. The purchaser is responsible for obtaining all necessary permits and approvals for the construction of the building and for the construction of the building. The purchaser is responsible for obtaining all necessary permits and approvals for the construction of the building.

Magellan
ARCHITECTS
8383 158th Avenue Northeast, Suite 280
Redmond, Washington 98052
Tel: (425) 885-4300 Fax: (425) 885-4303
www.magellanarchitects.com

SOUTH ELEVATION
SCALE: 1/8" = 1'-0"



WEST ELEVATION
SCALE: 1/8" = 1'-0"

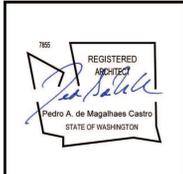
GILMAN POINT
SELF-STORAGE and LES SCHWAB TIRE CENTER
160 NW GILMAN BOULEVARD
ISSAQUAH, WASHINGTON 98027

REVISIONS		
NO.	DATE	BY
1		
2		
3		
4		
5		

ISSUE DATES	
DESIGN APPROVAL:	
PERMIT SUBMITTAL:	
PERMIT RECEIVED:	
BID DOCS:	
CONSTR. DOCS:	

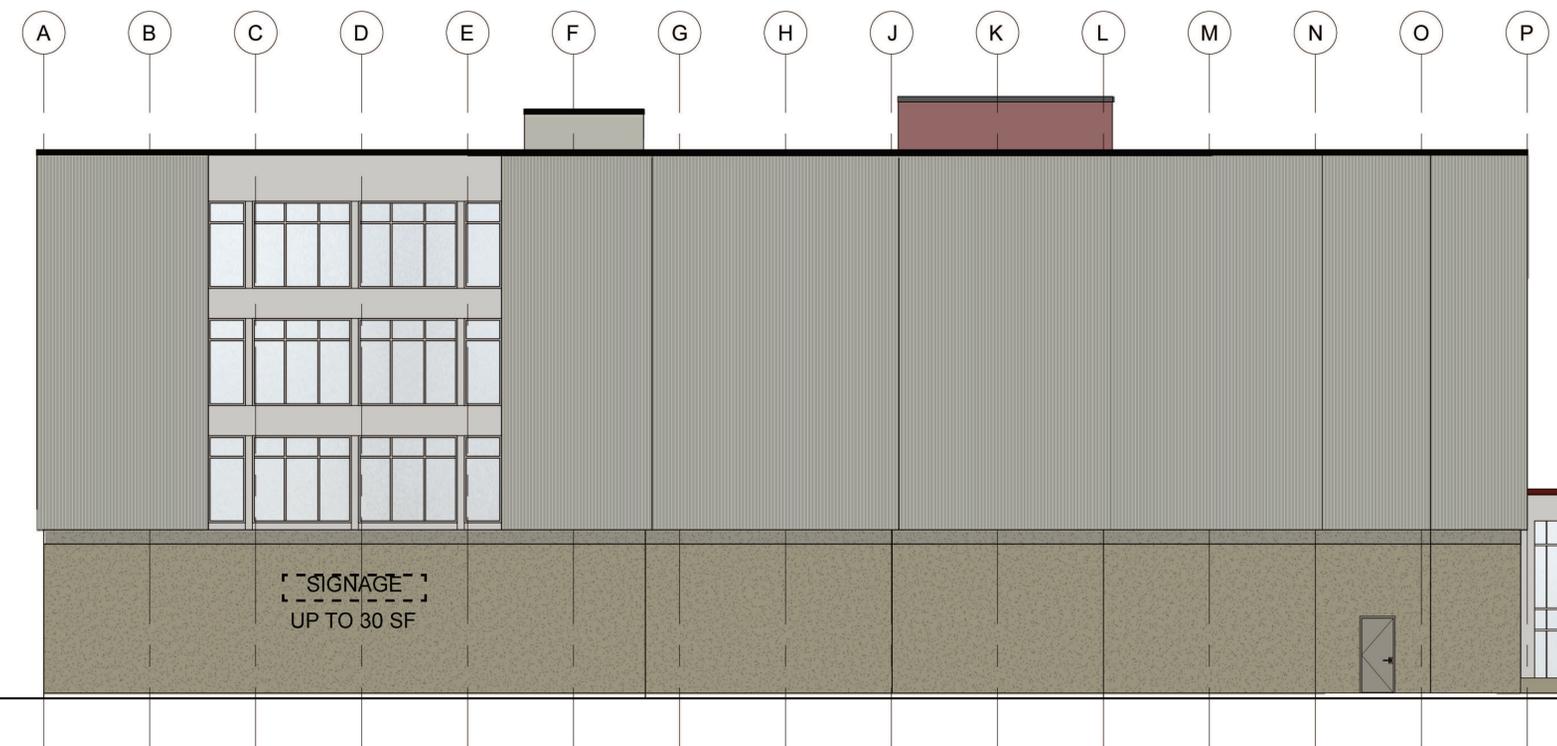
24"x36" SCALE:	AS NOTED
PLOT DATE:	03-10-2015
CAD FILE:	14-140_A31_ASDP
JOB NUMBER:	14-140
CHECKED:	
DRAWN:	
STATUS:	ISSUE FOR ASDP

STORAGE BUILDING
EXTERIOR ELEVATIONS
AA3.1



Copyright © 2015 Magellan Associates PLLC. All rights reserved. This drawing is the property of Magellan Associates PLLC and is not to be used for any other project without the written approval of Magellan Associates PLLC. The purchaser of this drawing agrees to indemnify and hold Magellan Associates PLLC harmless from any and all claims, damages, costs, and expenses, including reasonable attorney's fees, that may be incurred by Magellan Associates PLLC as a result of the purchaser's use of this drawing for any purpose other than that intended by Magellan Associates PLLC. The purchaser shall be responsible for obtaining all necessary permits and approvals for the construction of the building. The purchaser shall be responsible for obtaining all necessary approvals for the construction of the building. The purchaser shall be responsible for obtaining all necessary approvals for the construction of the building.

Magellan
ARCHITECTS
8383 158th Avenue Northeast, Suite 200
Redmond, Washington 98052
Tel (425) 885-4300 Fax (425) 885-4303
www.magellanarchitects.com
Copyright © 2015 Magellan Associates PLLC



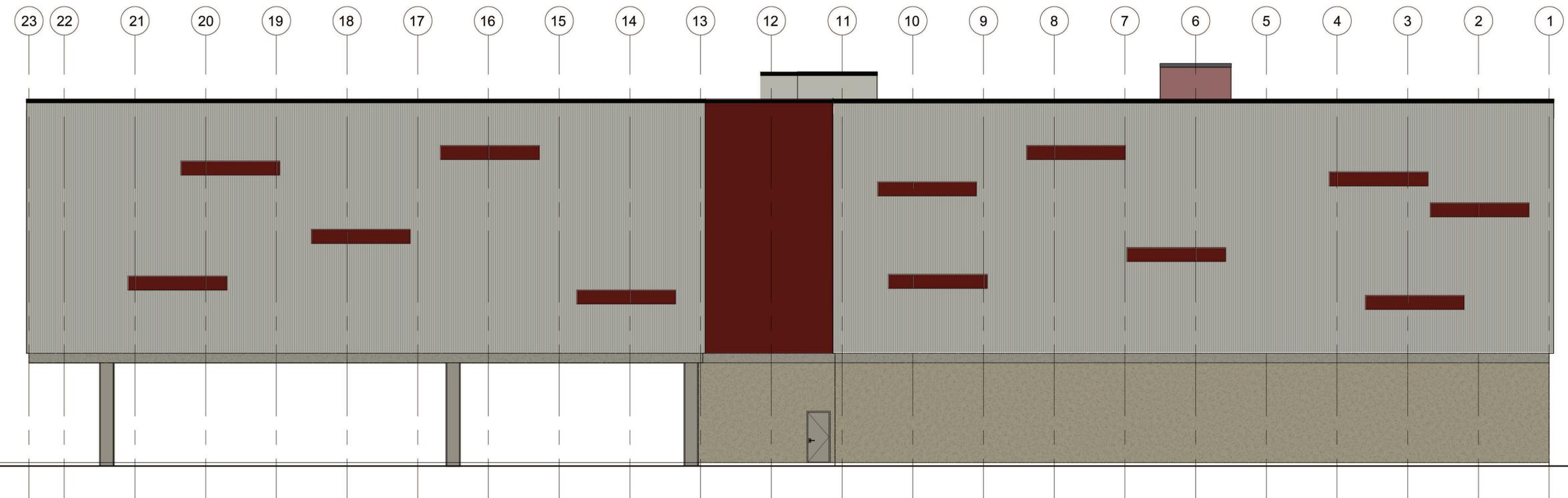
NORTH ELEVATION
SCALE: 1/8" = 1'-0"

GILMAN POINT
SELF-STORAGE and LES SCHWAB TIRE CENTER
160 NW GILMAN BOULEVARD
ISSAQUAH, WASHINGTON 98027

REVISIONS		
NO.	DATE	BY
1		
2		
3		
4		
5		
6		

24"x36" SCALE:	AS NOTED
PLOT DATE:	03-10-2015
CAD FILE:	14-140_A31_ASDP
JOB NUMBER:	14-140
CHECKED:	
DRAWN:	
STATUS:	ISSUE FOR ASDP

STORAGE BUILDING
EXTERIOR ELEVATIONS
AA3.2



EAST ELEVATION
SCALE: 1/8" = 1'-0"



WEST VIEW



NORTH EAST VIEW



SOUTH VIEW



Copyright © 2015, Magellan Associates PLLC. All rights reserved. This project is the property of the Architect. For this project, the Architect's services for use solely with respect to this project are limited to those specifically stated in the contract documents. No part of this project may be reproduced, stored in a retrieval system, or transmitted in any form or by any means, electronic or mechanical, including photocopying, recording, or by any information storage and retrieval system, without the express written approval of Magellan Associates PLLC. Any use of this project for any other purpose may be made without prior written permission. The purchaser is responsible for obtaining all necessary permits and approvals for the construction of the project. The Architect does not warrant the accuracy of the information provided herein and does not warrant the purchaser to construct more than one building.

Magellan
ARCHITECTS
8383 158th Avenue Northeast, Suite 200
Redmond, Washington 98052
Tel (425) 885-4300 Fax (425) 885-4303
www.magellanarchitects.com
Copyright © 2015, Magellan Associates PLLC

GILMAN POINT
SELF-STORAGE and LES SCHWAB TIRE CENTER
160 NW GILMAN BOULEVARD
ISSAQUAH, WASHINGTON 98027

REVISIONS		
NO.	DATE	BY
1		
2		
3		
4		
5		
6		

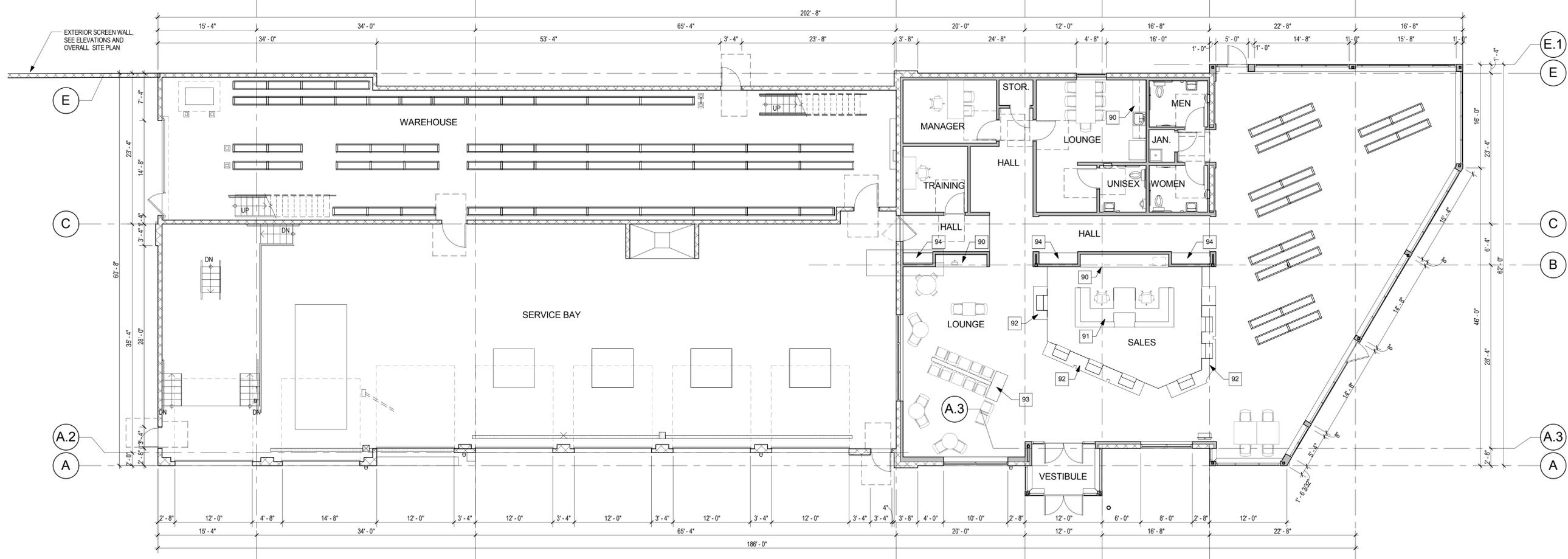
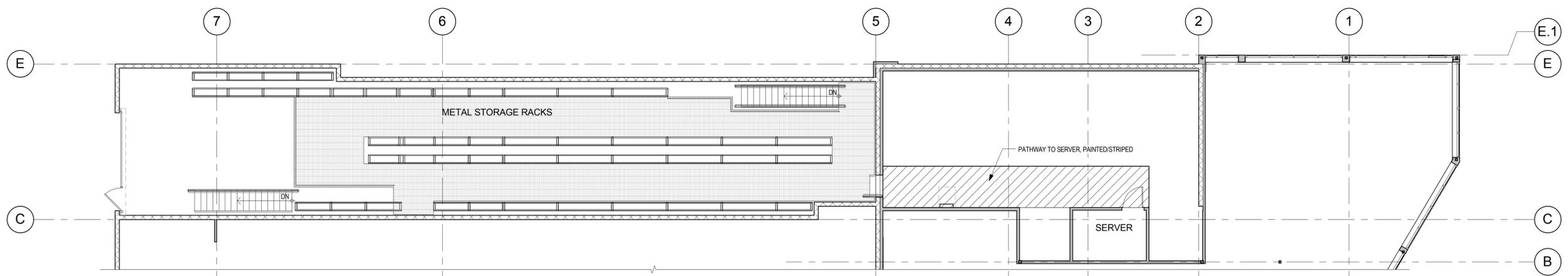
ISSUE DATES:
DESIGN APPROVAL:
PERMIT SUBMITTAL:
PERMIT RECEIVED:
BID DOCS:
CONSTR. DOCS:

24"x36" SCALE:	AS NOTED
PLOT DATE:	03-10-2015
CAD FILE:	14-140_A31_ASDP
JOB NUMBER:	14-140
CHECKED:	
DRAWN:	
STATUS:	ISSUE FOR ASDP

STORAGE BUILDING
COLORED PERSPECTIVES
AA3.3

LES SCHWAB PROJECT DATA AND CODE ANALYSIS

APPLICABLE CODES:	2012 IBC, IPC, IEBC, IMC, IFC, IECC, 2011 NEC,	CONSTRUCTION TYPE:	V-B, FULLY SPRINKLERED
BUILDING ANALYSIS		HEIGHT AND AREA INCREASES:	YES
BUILDING HEIGHT:	25'-4"	BASE ALLOWABLE SF/STORIES PER IBC TABLE 503	9000 SF / 1 STORY
BUILDING STORIES:	1	SPRINKLED IN ACCORDANCE WITH IBC 903.3.1.1	YES
BUILDING OCCUPANCIES:	M / S-1	SQUARE FOOTAGE INCREASE PER IBC 506.3	36,000 SF PER STORY = (9,000 x (9,000x2))
OCCUPANCY SEPARATION REQUIRED PER IBC 508.4:	NONE	OCCUPANCY CALCULATIONS	
GROSS SQUARE FOOTAGE PER OCCUPANCY:		OCCUPANCY LOAD CALCULATED USING IBC TABLE 1004.1.2	
GROUND FLOOR M:	4,584 SF	M OCCUPANCY - 4,584 SF	30 GROSS SF/OCCUPANT
GROUND FLOOR S-1:	6,474 SF	M OCCUPANCY - 4,584 SF	30 GROSS SF/OCCUPANT
MEZZANINE LEVEL M:	0 SF		
MEZZANINE LEVEL S-1:	1,498 SF		
TOTAL SQUARE FOOTAGE:	12,556 SF	EXITS/EXIT WIDTH PER 1005.3.2	2 REQUIRED WITH 29' TOTAL WIDTH REQUIRED
		PROVIDED EXITS AND WIDTH	2 PROVIDED, 1 AT 36' AND 1 AT 72'
		S-1 OCCUPANCY - 7,972 SF	300 GROSS SF/OCCUPANT
		EXITS/EXIT WIDTH PER 1005.3.2	1 REQUIRED WITH 6' TOTAL WIDTH REQUIRED
		PROVIDED EXITS AND WIDTH	4 PROVIDED AT 36' EACH
		TOTAL OCCUPANT LOAD:	179



Drawn By: Checked By: Date

Revision

No.

Galloway
Planning, Architecture, Engineering
6165 S. Willow Dr., Suite 320
Greenwood Village, CO 80111
303.770.8884
303.770.3636
www.gallowayus.com

Tires LES SCHWAB

NOT FOR CONSTRUCTION

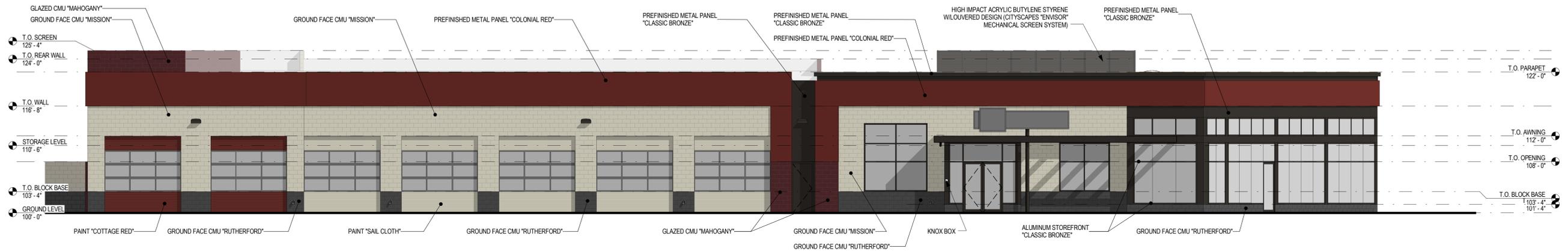
LES SCHWAB TIRE CENTER
ISSAQUAH, WA

160 NW Gilman,
Issaquah, WA 98027

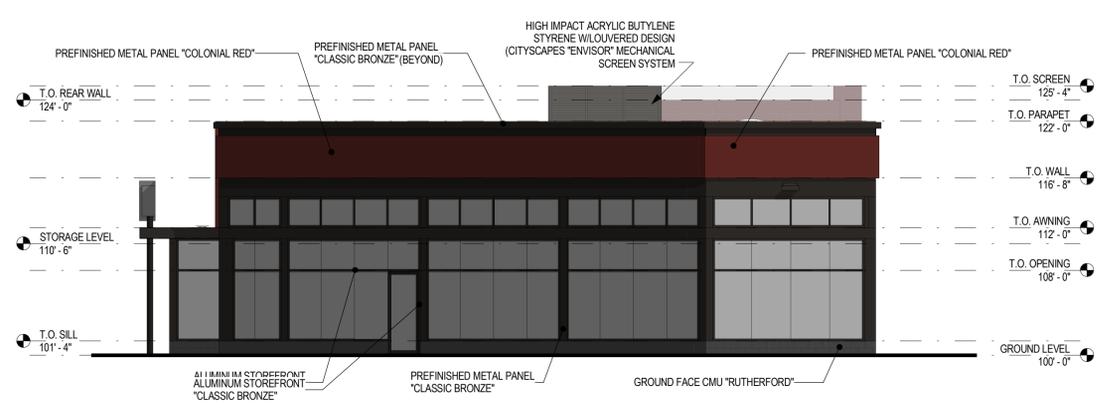
Project No: LST700007
Sheet Scale: As indicated
Designed By: KCN
Drawn By: KCN
Date: 5/8/2015

LES SCHWAB
FLOOR PLANS AND
CODE SUMMARY

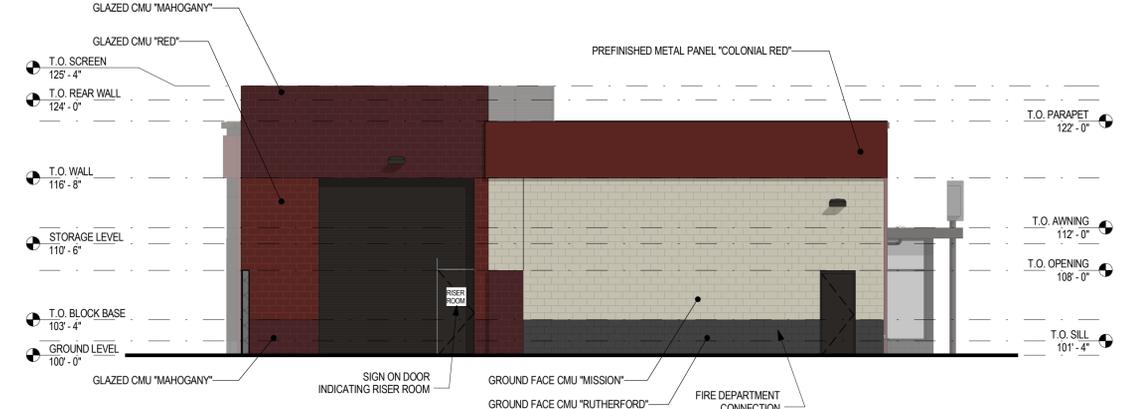
AB2.1



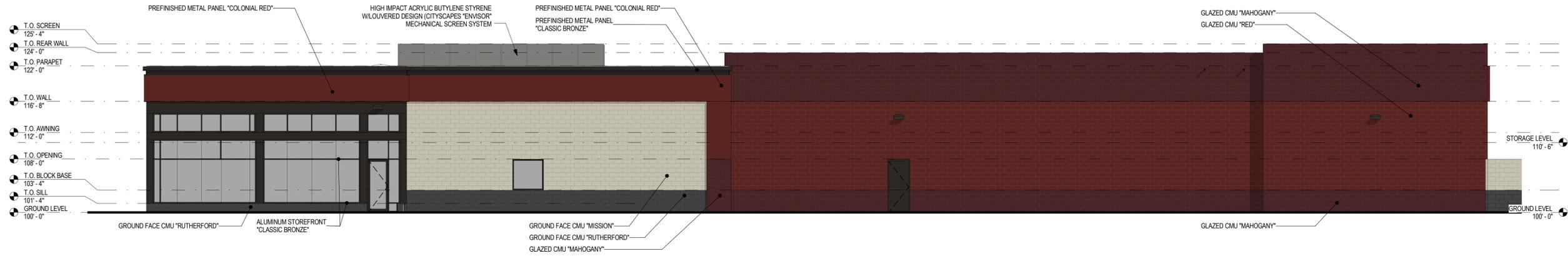
1 FRONT ELEVATION/SOUTHEAST
SCALE: 1/8" = 1'-0"



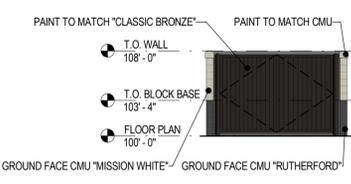
2 SHOWROOM ELEVATION/SOUTHWEST
SCALE: 1/8" = 1'-0"



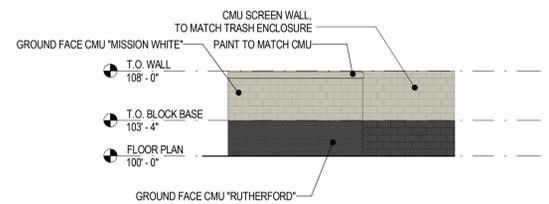
3 LOADING ELEVATION/NORTHEAST
SCALE: 1/8" = 1'-0"



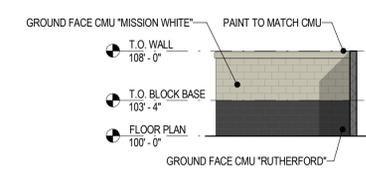
4 REAR ELEVATION/NORTHWEST
SCALE: 1/8" = 1'-0"



5 FRONT ELEVATION
SCALE: 1/8" = 1'-0"



6 SIDE ELEVATION
SCALE: 1/8" = 1'-0"



7 BACK ELEVATION
SCALE: 1/8" = 1'-0"

NO.	Revision	Drawn By	Checked By	Date

Galloway
Planning, Architecture, Engineering
6185 S. Willow Dr., Suite 320
Greenwood Village, CO 80111
303.770.8884
www.gallowayus.com



NOT FOR CONSTRUCTION

**LES SCHWAB TIRE CENTER
ISSAQUAH, WA**

160 NW Gilman,
Issaquah, WA 98027

Project No.	LST70007
Sheet Scale	1/8" = 1'-0"
Designed By	KCN
Drawn By	KB
Date	5/8/2015

EXTERIOR
ELEVATIONS AND
FINISH SCHEDULE

AB3.1



1 COLOR RENDERING
SCALE:

No.	Revision	Drawn By	Checked By	Date

Galloway
 Planning, Architecture, Engineering
 6185 S. Willow Dr., Suite 320
 Greenwood Village, CO 80111
 303.770.8884 O F
 303.770.3838
 www.gallowayus.com
 ©2015, Galloway Company, Inc. All Rights Reserved



NOT FOR CONSTRUCTION

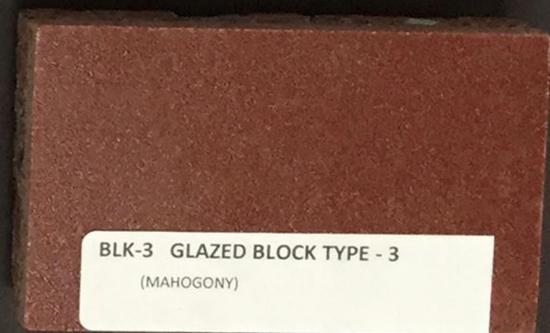
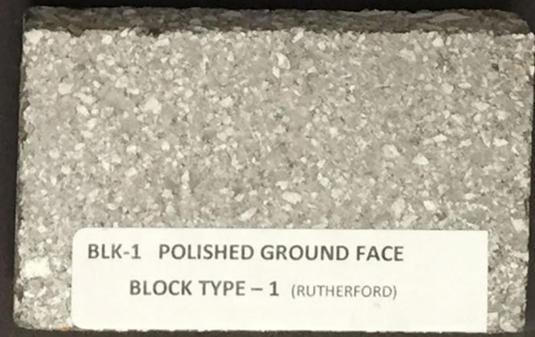
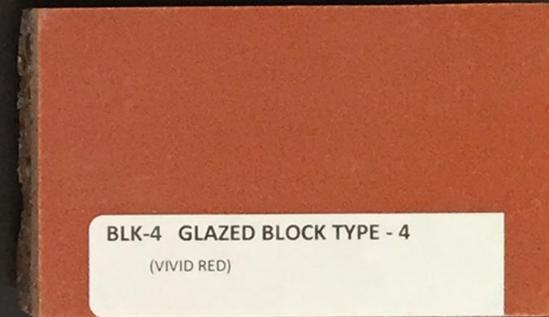
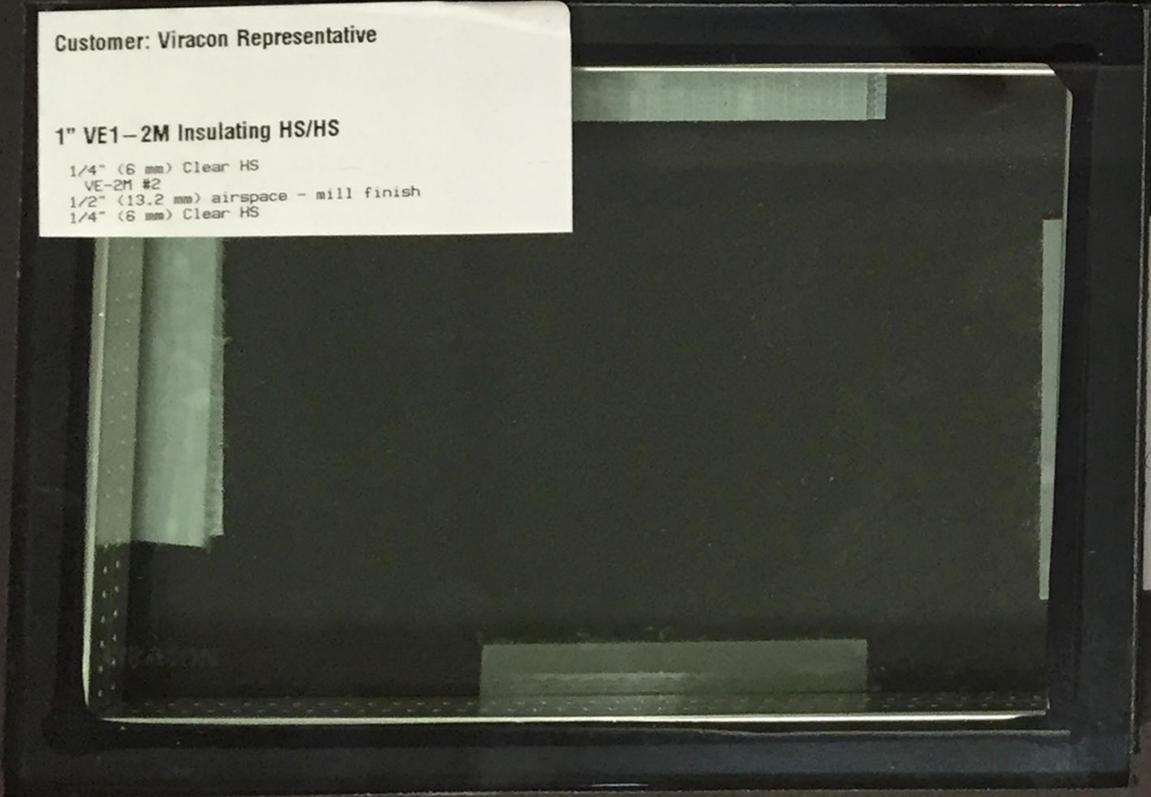
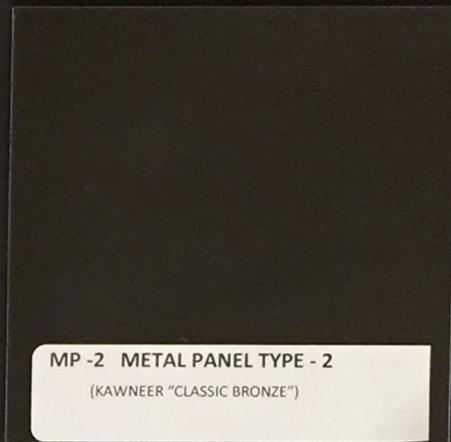
LES SCHWAB TIRE CENTER
 ISSAQUAH, WA

160 NW Gilman,
 Issaquah, WA 98027

Project No:	LST700007
Sheet Scale:	
Designed By:	KCN
Drawn By:	KB
Date:	3/6/2015

COLOR RENDERING

AB3.2



No.	Revision	Drawn By	Checked By	Date
NOT FOR CONSTRUCTION				
LES SCHWAB TIRE CENTER ISSAQUAH, WA <small>160 NW Climan Issaquah, WA 98027</small>				
<small>Project No: LST700007 Sheet Scale: Designed By: Designer Drawn By: Author Date: 9/8/2015</small>				
LES SCHWAB COLOR & MATERIALS BOARD				
AB3.3				



Copyright © 2015 Magellan Associates PLLC
 Architect for this project are instruments of the
 this project for use solely with respect to
 to be transmitted in any form or by any means,
 electronic or mechanical (including photocopying,
 without the express written approval of Magellan
 may be made without prior written permission. The
 purchaser to use this set of plans for the
 conditions set forth in the contract documents and the
 purchaser to construct more than one building.

Magellan
 ARCHITECTS
 8383 158th Avenue Northeast, Suite 280
 Redmond, Washington 98052
 Tel (425) 885-4300 Fax (425) 885-4303
 www.magellanarchitects.com
 Copyright © 2015 Magellan Associates PLLC

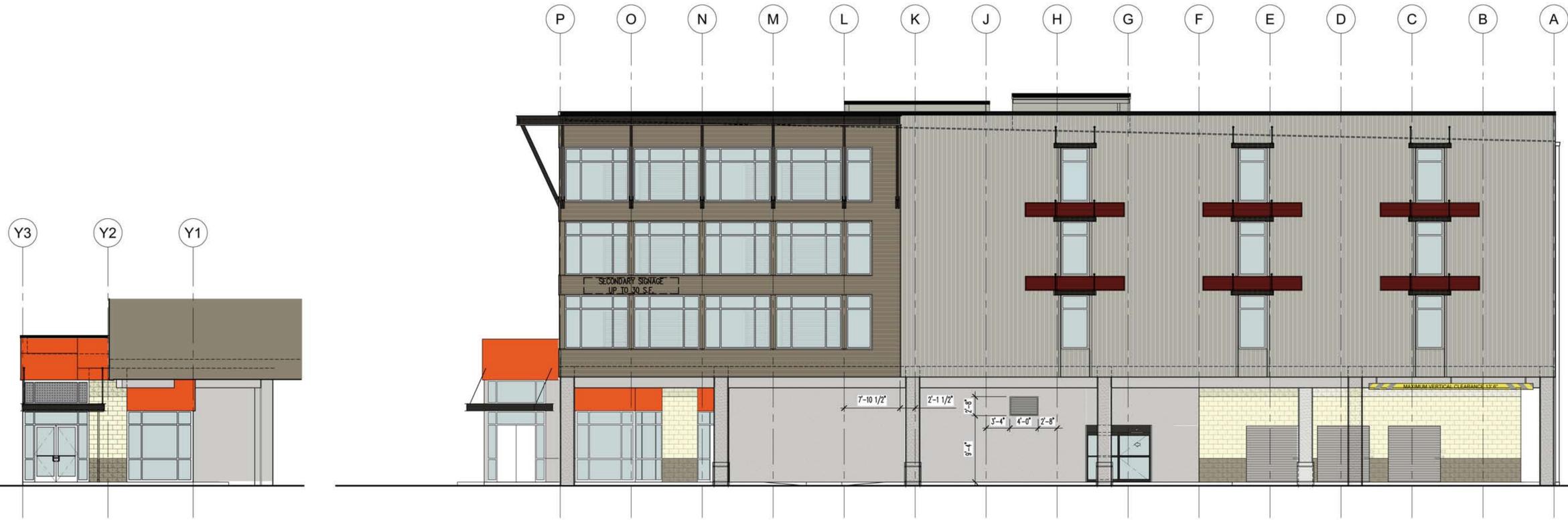
GILMAN POINT SELF-STORAGE
 NEW 4-STORY SELF-STORAGE FACILITY
 160 NW GILMAN BOULEVARD
 ISSAQUAH, WASHINGTON 98027

REVISIONS		
NO.	DATE	BY
1		
2		
3		
4		
5		
6		

ISSUE DATES
 DESIGN APPROVAL:
 PERMIT SUBMITTAL: 08-19-2015
 PERMIT RECEIVED:
 BID DOCS:
 CONSTR. DOCS:

24"x36" SCALE: 1/8" = 1'-0"
 PLOT DATE: 08-17-2015
 CAD FILE: 14-140_A31
 JOB NUMBER: 14-140
 CHECKED: GS / PC
 DRAWN: DYM/RYY/JJ
 STATUS: ISSUE FOR PERMIT

EXTERIOR ELEVATIONS
A3.1



2 SOUTH ELEV. - OFFICE FACADE
 SCALE: 1/8" = 1'-0"

2 SOUTH ELEVATION
 SCALE: 1/8" = 1'-0"



3 WEST ELEVATION
 SCALE: 1/8" = 1'-0"



Copyright © 2015 Magellan Associates PLLC
 The Architect for this project are instruments of the
 this project services for use solely with respect to
 to be transmitted in any form or by any means,
 electronic or mechanical (including photocopying,
 without the express written approval of Magellan
 may be made without prior written permission. The
 purchaser to use this set of plans for the
 additional set(s) of plans shall not entitle the
 purchaser to construct more than one building.

Magellan
 ARCHITECTS
 8383 158th Avenue Northeast, Suite 200
 Redmond, Washington 98052
 Tel (425) 885-4300 Fax (425) 885-4303
 www.magellanarchitects.com
 Copyright © 2015 Magellan Associates PLLC

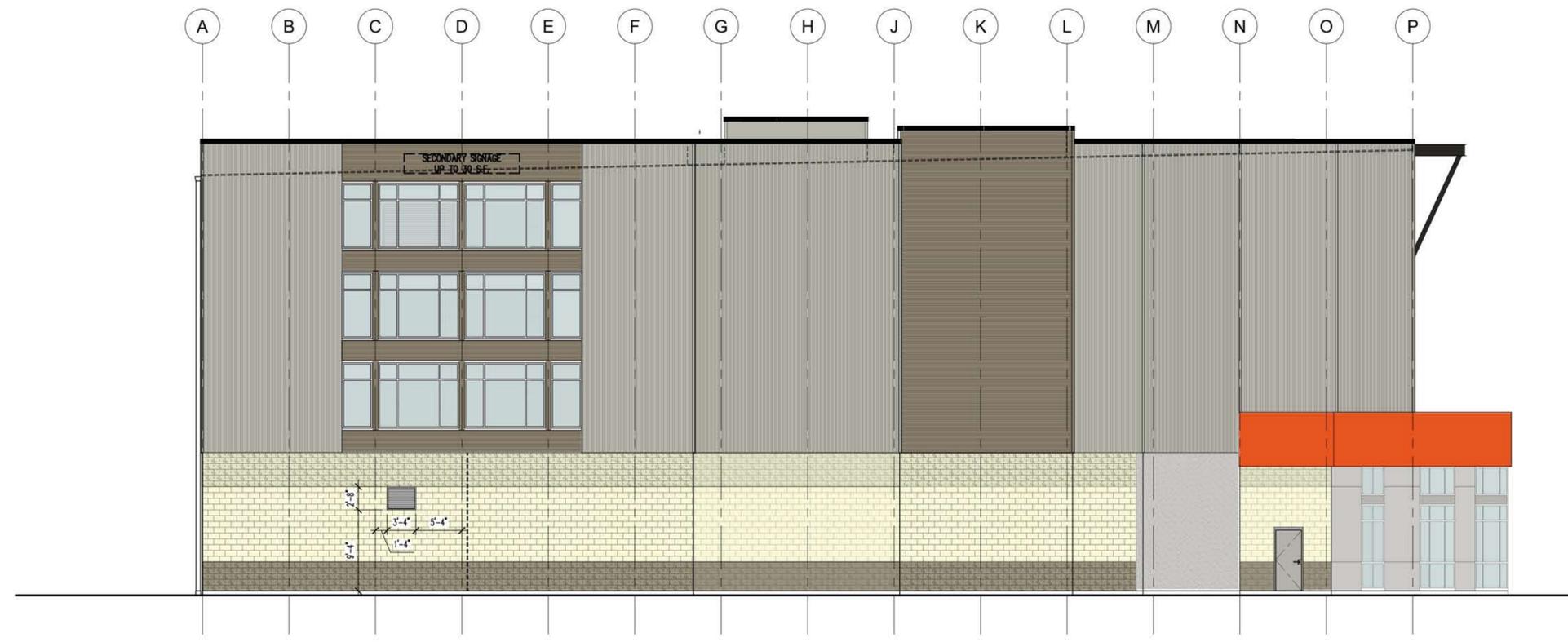
GILMAN POINT SELF-STORAGE
 NEW 4-STORY SELF-STORAGE FACILITY
 160 NW GILMAN BOULEVARD
 ISSAQUAH, WASHINGTON 98027

REVISIONS		
NO.	DATE	BY
1		
2		
3		
4		
5		
6		

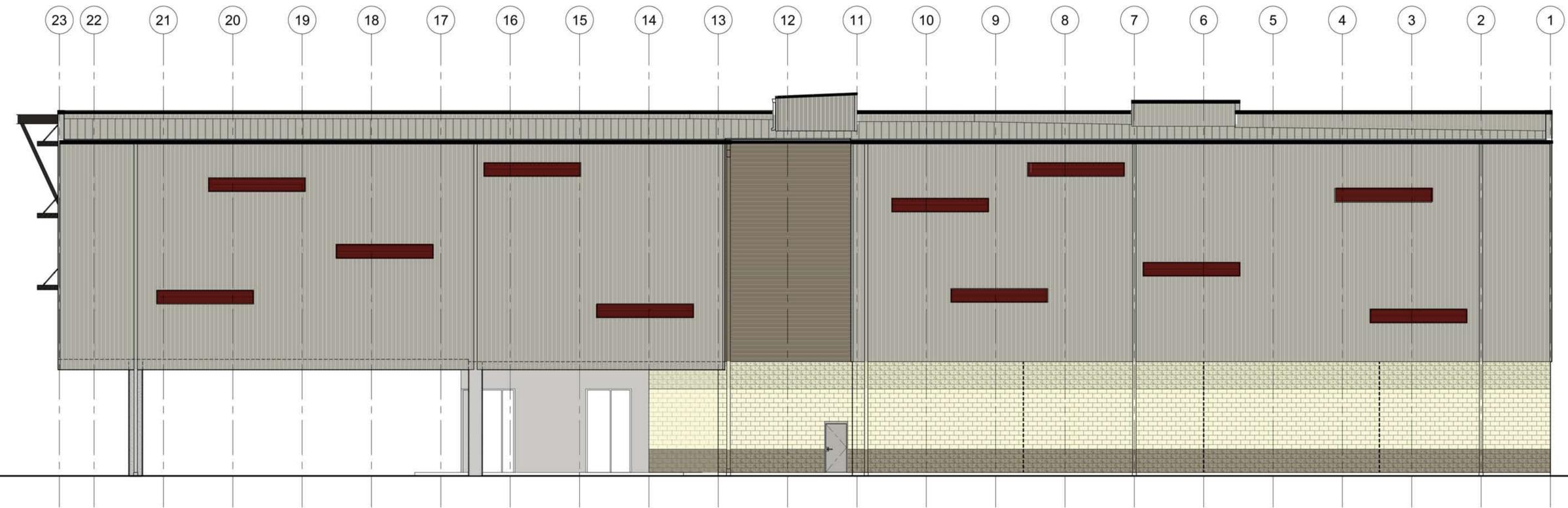
ISSUE DATES
 DESIGN APPROVAL:
 PERMIT SUBMITTAL: 08-19-2015
 PERMIT RECEIVED:
 BID DOCS:
 CONSTR. DOCS:

24"x36" SCALE: 1/8" - 1'-0"
 PLOT DATE: 08-17-2015
 CAD FILE: 14-140_A31
 JOB NUMBER: 14-140
 CHECKED: GS / PC
 DRAWN: DYM/RYY/JJ
 STATUS: ISSUE FOR PERMIT

EXTERIOR ELEVATIONS
A3.2



1 NORTH ELEVATION
 SCALE: 1/8" - 1'-0"



2 EAST ELEVATION
 SCALE: 1/8" - 1'-0"

ATTACHMENT 8 – ASDP15-00002

August 27, 2015

ASDP15-00002 – Gilman Point

Administrative Adjustment of Standards: Section 6.3 – Circulation Facilities

Administrative Adjustment of Standards (AAS) for Circulation Facilities:

Request: Removal of Pedestrian Circulation facilities (sidewalk) along the east side (parking lot side) of the new internal street.

New Internal Street

The following AAS approval criteria for Circulation Facilities are applicable per Section 6.3 of the Central Issaquah Development and Design Standards:

- 1. Vision.** The proposal is equal to or superior in achieving the intent of the Central Issaquah Plan, Development and Design Standards and this Chapter;

Complies. Per the explanations provided in Sections 6.2.B-C and 12.3.A of ASDP15-00002, Attachment 1 (CIDDS review spreadsheet) the reduction of pedestrian circulation facilities from the east side of the internal street achieves the intent of the applicable standards and intent of the Central Issaquah Plan, Development and Design Standards and Chapter 6 – Circulation Facilities.

The purpose of the modification is to concentrate pedestrians away from the parking lot; and, to allow enough space for a ladder truck and emergency services to be able to access the site. Other street elements including the planter strip and site lighting will be provided along the east side of the internal street within the parking lot landscape islands and will provide a formalized edge along the east side of the street.

- 2. Access.** The proposal will not create significant adverse impacts to the abutting properties or right-of-ways, dedicated tracts, or easements.

Complies. The proposal does not create any significant adverse impacts to the abutting properties, right-of-ways, tracts or easements. Pedestrian access is maintained via a sidewalk provided on the west side of the internal street.

- 3. Compatibility.** The proposal is compatible with, and would not significantly or adversely affect the scale, character, and design of the surrounding neighborhood or District.

Complies. The proposal is compatible with the existing neighborhood and does not significantly or adversely affect the scale, character or design of surrounding area.

- 4. Sufficient Reason.** Sufficient reason is shown for the adjustment in order to address exceptional or extraordinary circumstances and conditions applicable to the facility such as existing physical constraints that are not contemplated or provided by this chapter.

Complies. Sufficient reason is provided to grant the adjustment due to site limitations caused as a result of an atypical property configuration and the location of new and existing roads, building and utilities. The functionality of the site and pedestrian and vehicular access will continue to function in the manner intended by the Central Issaquah Plan and the Development and Design Standards.

- 5.0 Safety.** The proposal does not negatively impact public safety and operation, nor create any hazardous features.

Complies. The proposal does not negatively impact public safety or create any hazardous features.

- 6.0 Services and Maintenance.** The proposal will not create negative impacts to public services, including fire and emergency services nor adversely affect how well the surrounding public facilities can be maintained.

Complies. The proposal will not negatively impact public services or adversely the maintenance of surrounding public facilities.

- 7.0 Priorities.** The criteria listed in Circulation, Section 6.2.C Priorities are applied.

Complies. The modification is consistent with the priorities listed in Section 6.2.C. to the greatest extent feasible given the constraints of the site. Vehicular travel lanes have been reduced to 10 feet in width; and, direct pedestrian access through the site is maintained between the storage facility and the existing and proposed circulation facilities.

Conditions of Approval

The following condition has been incorporated into the Notice of Decision – File No. ASDP15-00002.

ASDP15-00002 – Condition 5: “All pedestrian circulation facilities shall be a minimum of 5 feet in width, clear of intrusions (bollards, handrails, railings, etc.). For the west sidewalk of the internal street, the applicant shall explore options that would allow the west sidewalk to be widened.



Development Services Department

1775 – 12th Ave. NW | P.O. Box 1307

Issaquah, WA 98027

PH: 425-837-3100

issaquahwa.gov

ATTACHMENT 9 – ASDP15-00002

August 27, 2015

ASDP15-00002 – Gilman Point

Administrative Adjustment of Standards: Section 11.3.F – Streetwall

Administrative Adjustment of Standards (AAS) for CIDDS Section 11.3.F – Streetwall

Request: Allow the new internal street to serve as the Build-to-Line for the purpose of creating a Streetwall.

Specific criteria for this AAS are not provided in Section 11 – Site Design. Per Section 1.1.E.4 – Purpose and Applicability, the following AAS approval criteria for are applicable:

A. Vision. The proposed alternative is equal or superior to the Central Issaquah Plan vision, goals, and polices.

Complies. Per the explanation provided below in Section D; and in Section 11.3.F of Attachment 1, the proposed modification is consistent with the vision, goals and policies of the Central Issaquah Plan.

B. Access. The proposal will not create significant adverse impacts to the abutting properties or right-of-ways, dedicated tracts, or easements.

Complies. The proposal does not create any significant adverse impacts to the abutting properties, right-of-ways, tracts or easements. Pedestrian access is maintained via a sidewalk provided on the west side of the internal street.

C. Compatibility. The proposal is compatible with the character of the surrounding properties and their potential development under the Central Issaquah Plan.

Complies. The proposal is compatible with the existing neighborhood and does not significantly or adversely affect the scale, character or design of surrounding area.

D. Intent. The adjustment will be equal to, or superior in, fulfilling the intent and purpose of the original requirements.

Complies. The intent of CIDDS 11.3.F (Establish Streetwall (Build-to Lines)) is to ensure that buildings are located with a specified and close proximity to Circulation Facilities and Community Spaces, and to create a strong street wall presence in order to frame the Public Realm and create a pleasant pedestrian experience. The characteristics of the site virtually prohibit provision of the required amount of streetwall facing NW Gilman Blvd, given the need for the site to function, including vehicular access via a road into the site:

- 1) The property is considerably set back from NW Gilman Blvd (approx. 70 ft)*
- 2) Relatively short property frontage (approx. 80 ft)*
- 3) The unusual parcel configuration*

For example, the required street wall length is 48 ft (60% of the 80 ft frontage). The width of the street, including all its required elements is a minimum of 42 ft. The required minimum streetwall plus the street width is 13% greater than the available frontage.

The proposal places the front building within the Build-to line to the greatest extent feasible. In addition, the front building is also place within the Build-to line of the new street/Circulation Facility. While this is not the same as placing the building at the front property line, it will help compensate for the lack of building while recognizing the site's constraints.

E. Safety. The proposal does not negatively impact any safety features of the project, nor create any hazardous features.

Complies. The proposal does not negatively impact public safety or create any hazardous features.

F. Services. The proposal will not create negative impacts to public services, including fire and emergency services.

Complies. The proposal does not negatively impact public safety or create any hazardous features.