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Wednesday, August 7, 2013 City Council Chambers

7:00 PM CALL TO ORDER: Approval of Minutes
• May 1, 2013 for Issaquah Plaza 221

**7:10 PM COMMUNITY CONFERENCE: Issaquah Middle School,
Application PLN COM13-00002**

An application for a Community Conference has been submitted by Mahlum Architects to convert the existing campuses of Tiger Mountain School and Clark Elementary School (both buildings to be demolished) to a new Issaquah Middle School.

The project is located at 500 Second Avenue SE, in the Olde Town subarea.

ADJOURN

Note to the Public: Copies of the Development Services Department's staff reports on the above applications are not included with this agenda but have been sent to the Development Commission. Copies of staff reports may be obtained from the Development Services Department, City Hall NW, 1775 12th Avenue NW, Issaquah, from 9:00 AM to 5:00 PM, Monday through Friday.

May 1, 2013

CITY OF ISSAQUAH
Development Commission - Minutes

City Hall South 135 E Sunset Way
Council Chambers Issaquah, WA

COMMISSION MEMBERS PRESENT

Randy HARRISON, Chair
Commissioner Michael BRENNAN
Commissioner Ray LEONG
Commissioner Mary Lou PAULY
Commissioner Mel MORGAN Jr.

STAFF PRESENT

David Favour, Deputy Director
Allison Gubata, Recording Secretary

CALL TO ORDER

HARRISON called the meeting to order at 7:07 p.m.

APPROVAL OF MINUTES

Moved by Commissioner MORGAN, Second by BRENNAN to approve the minutes of the November 14, 2012 meeting. *PAULYPAULY *requested revisions provided in writing to Recording Secretary which will be shown in minutes posted on the City of Issaquah website.* MOTION CARRIED.

PUBLIC HEARING: Issaquah Plaza 221

File Nos. PLN12-00065, 66, 67

Application for approval of land use and shoreline permits to construct two commercial buildings - One building will have approximately 2,700 square feet with a drive-through. The second building will consist of approximately 11,000 square feet. Two existing single family homes will be demolished. An existing wetland will be preserved and the stream and wetland buffers will be enhanced with native plantings. The project is located within the shoreline jurisdiction of Issaquah Creek. The project is located at the southwest corner of 56th Street SE and 221st Place SE.

Issaquah Plaza 221 - Exhibit List attached; Exhibit 1-13 and Exhibits with file 14-26

Favour - This meeting is suggested to involve some changes in procedure to better address the new Central Issaquah Development and Design Standards code. The project review will also reference differences from the community conference of 4/20/11. The Central Issaquah Plan which effects this site were adopted by Council in December 2012 and the Central Issaquah Development and Design standards effective 4/29/13. These standards will be used for the review today. Up until now we used "Green Sheets" which we will no longer be referencing.

Thanked everyone who was involved with the Central Issaquah Plan. City Council adopted the new standards/land use code which will be applied for this project. The Green sheets in the existing Land Use Code are replaced by Chapters 11 -17 of the new code and become the new green sheets. The new standards are much longer and instead of going through each one I suggest we focus on the general concepts that come up as recommended for approval. Each of you have a Central Issaquah Development and Design Standards document tonight, it's also available on the internet.

HARRISON- I believe that the process we agreed on is that at the conclusion of each of the chapters we are

going to discuss and ask for comments at the end of each chapter we will say "do we individually feel that these meet the standards as amended by the commission".

PAULY- Alternatively we would propose conditions as suggestions during the discussion of each chapter and at the end still have an overall vote to approve with any new conditions. The chapter by chapter review would be more of a discussion of potential conditions.

MORGAN- Technically make a motion to add the amendments of new conditions unless at the very beginning we have a motion to approve with conditions we cited.

BRENNAN- At the end it's hopeful for obtaining the whole content of the discussion.

The Commission generally agreed to have a general discussion at the end of each chapter with a discussion of potential new conditions. Then at the end of the overall discussion the Commission would have an overall vote and add any new conditions at that time.

Favour - The staff report is formatted to go chapter by chapter and the conditions are formatted to go in that order and we may use this as a guide this evening.

Reviewed summary statement of the project as identified above. Bldg proposal site plan showing this frontage is 221st wraps around to 56th St and Brown Bear Car Wash is across street immediately east. Across the street to the North is a storm water detention pond and Court House. At the NE corner of the property is proposed a 2700 sq ft restaurant identified as Taco Time. A second commercial building is proposed to the south with potential for additional commercial businesses. There are 3 environmental constraints around the west of the property; this project is the first one in the door to be reviewed under the Central Plan Standards. This applicant has been working with the City of Issaquah for 2 years and meets the basic elements of the new Standards generally being urban vs. suburban, also pedestrian friendly by pushing the building out to the street with a drive through moved to the rear with parking also in the rear.

We have anticipated how this project could meet the new code. For example, in the old code we had to set back the buildings 10 - 20 feet from the street, under this new plan the setback is zero feet. This plan shows a 4-5 feet setback from the sidewalk on the plans. As far as impervious surface this project is well below those requirements. Building height is well below the requirements. Parking under the new standards for this project is 3 or 4 stalls above the maximum and the SE corner of the parking lot is part of the creek buffer which extends into the 4 stalls. A proposal is to eliminate those 4 stalls to better comply with creek conditions and meet the parking requirements. A condition is proposed to eliminate the additional 4 stalls to comply with creek buffer.

The conditions of approval could be used as a guide to review the project for key issues, for example pushing the building out to the street and landscaping are important elements; generally this project is doing a good job of meeting the new design code. Specific plant types including Japanese holly and Japanese barberry shrubs should be swapped out for less invasive plants as recommended by the River and Streams Board. The proposal is to have an evergreen screening in the parking lot with non-invasive plants.

We recommend a condition to place a green element between the sidewalk and the building. This is a late addition to the central plan standards that require some plantings even if there is a zero setback still. The reason for this is we want some amount of greenery between the building and sidewalk - this does not mean a continuous strip but instead a mix of some outdoor seating and raised planters. Regarding community space and public the proposal shows several public places around the building. This code requires a minimum of 20 x 20 feet and the SE Plaza fulfills this dimensional requirement however the NE plaza north of Taco Time is slightly smaller than this dimension, however one could say the combination of several smaller plazas around the building meets this code intent. The Administration recommends a nice landscape screen element along 56th Street. When we get to the building permit stage we propose to go forward with the intent that these conditions are set up and diving into the details of the landscape plan.

Public comments received via e-mail were handed out to the Development Commission for review. Many comments address the need for a bike lane on 221st Place.

Regarding a bike lane, the City of Issaquah staff reviewed the road developments in this area, and took into consideration issues including the King County Regional trail which parallels the 221st street, close proximity of buildings and other site improvements along 221st, and Issaquah Creek at the south end, and decided that a bike lane should not be installed. Recommendation is to get some connections from 221st to the regional trail as developments occurs.

Taco Time applicant is available for clarification no formal presentation at this meeting.

Public Comment - *detailed comments were submitted via e-mail and are attached.*

Connie Marsh has a store on Gillman Blvd. and is a resident here in Issaquah. In her e-mailed comments she suffered the dismay that the Issaquah plan said that there would be a connected community and then on the very first project there is no bike lane and no sidewalks on both sides of the road. This is not consistent with the new plan. The whole road segment is totally against the vision of the Central Issaquah Plan. If you remove the bike lane from this plan this is step 1 in disconnecting the vision of connected community.

There was nothing showing all of the linkages of how the pedestrians are going to be getting connected. The information was not available about how do you get across the street. The North Fork of Issaquah Creek - it is going to be impacted, but it is not clear how. The context does not seem to be in place. Access is required by the state to the creek as it is a shoreline of statewide significance; the trail segment does not connect to anything. The Central Issaquah Plan does not have show that this is a place where a trail will be. Why would we create a trail segment that does not link to anything? Staff presentation was confusing - it would be helpful if there were a picture that showed the entire city not just segments.

David Kappler has concerns about the elimination of bike lanes on 221st. On the East Lake Sammamish trail, it is all lined by cyclone fences with no connection. He does not see that there is good access to the nearby shopping centers; however the regional trail appears to be a good commuter trail. If you go with an 8 foot wide sidewalk without a bike lane along 221st, most bikers will end up on that sidewalk which would be a jumble. David suggests going to a narrow sidewalk and narrow planting to include a bike lane on 221st.

Lisa Krieger is a bicycle commuter who moved to Issaquah with the idea that she could get to work without taking a car. Lisa recommends a variance to go to 3 feet for bike lanes and not lose the bike trail connectivity.

HARRISON- Thanked the public for their comments and requests that the Commission start their process to evaluate the project against the Central Issaquah Plan Development and Design Standards.

Favour - Suggested going by each Chapter heading and section numbers: For example CH 11 - section "O" - Site Design - this chapter was intended to be the first of the design standards.

PAULY - Has questions on development standards in Chapters 1- 10 including bike lane, street section, community space. Her questions start at Chapter 6.

Chapter 6 Circulation

HARRISON- Reviewed Chapter 6 and requested clarification on the reference of a travel lane.

Favour - Clarified that a travel lane is where cars drive.

HARRISON- Asked why the city requested the planter remain at 6'?

Favour - Stated the purpose for keeping the planter width to 6' would allow the green element to be maintained as part of the "green necklace" in balancing priorities. The standards for bike lane width are set by individual cities not King County. 5' is the City of Issaquah standard.

PAULY - All of the elements sidewalk, landscaping, parking and bike lane are all necessary, it did not cross her mind that a bike lane would be eliminated. Her concern is that if there is no bike lane now then there would not be a bike lane ever. What about consideration for giving up the parking lane instead of the bike lane? This does not look much different from a lot of the developments we have already, are all necessary; this section on the next page describes bike lane needs, all of the requirements are necessary - is the question no bike lane yet? Or does this decision mean no bike lane ever.

Favour - Yes it would be tough to make a bike lane later without the 5 feet provided now.

PAULY - Why did we not give up the parking instead of the bike lane?

Favour - If you look at just this property it's very possible to have a bike lane but further south down this street it narrows and there is little room for a bike lane. A bike lane on this street is part of a bigger project, a larger issue.

PAULY - There are a lot of strange things going on now, if we use this rationale then we are not going to get all of the elements that we wanted; she is leaning towards keeping options open.

Favour - The most recently adopted Central Issaquah Plan does not show a bike lane on 221st. The city currently is undertaking a city wide pedestrian/bike plan which is being reviewed and should be done by the end of this year. However the most recent Non-motorized plan of the Central Issaquah Plan today does not propose bike lanes on 221st.

HARRISON- With proximity at the south end towards the creek it gets very tight there for a bike lane. How critical from what you know in the pipeline, how critical is the parallel street parking down there? If a bike lane is put in, will the parallel parking continue or will it be too tight and the parking will have to be eliminated on the street?

Favour - Responded that when you get to the South end of 221st, it gets tight and you potentially have to remove a lot of the elements of a street.

HARRISON- Asked if the street section included a travel lane and a bike lane but no parallel parking, could you continue the bike lane.

BRENNAN - The challenge we are faced with is what sorts of connections the city wants to see. We don't want to prevent the option of creating a bike lane or shared lane on this section of the road. For example, you could go with a wider shared lane, take it out to 13 feet for bikes and cars to share then it's not a designated bike lane by taking it out of the width of the sidewalk. Or you could push into the property to create the bike lane space on these relatively busy streets. The challenge the Commission is faced with this is a CIP decision for the city - we could go with a wider shared lane instead of constrain to the 10' travel lane, go to 13' where it's easier for bikers and cars to share the lane through the corridor.

Favour - For example, you could take a little out of the sidewalk, a little out of the planter to create the bike lane space. By balancing the priorities we could find 5' for a bike lane.

HARRISON- So much money was put into the underpass under I-90 and if you are coming from Gilman Blvd. then it's really tight not and very welcoming for cyclists.

Favour - Perhaps a condition could be added that the administration shall revisit the need for bike lanes and, if

required, plans shall be revised as needed to add a bike lane.

PAULY – It sounds like it's required to have a bike lane, and is not comfortable passing it back to the Administration and having it taken out again.

MORGAN- 1st project after adoption of the 5 year Central Issaquah Plan. This is the only north-south route we have right now, one corridor that is not a regional type route, the parallel parking can create more of a danger. I would rather see the street parking come out and make it into a bike lane and not take any footage from the project itself.

PAULY – Has a question regarding the paragraph above the right of way picture on Gilman Blvd. *Favour* replied it's a copy out of the code that has nothing to do with this project.

Chapter 7 Community Space

PAULY – Has a question for applicant to discuss on community space - how do they provide all season protection? How do you use these areas year round?

Applicant Howard Kimera HD Architect - Thank you to the City of Issaquah. There is the south facing exposure to this plaza, with building overhangs primarily for the entrance. Additionally there are landscape buffers but this plaza does not have the year round weather protected requirement.

PAULY - Is the intent to get something covered?

MORGAN–This is a good question. Also if it's wet is there cover so it does not turn muddy. Does it have lighting?

Applicant Kimera – There are a lot of things we can explore and we are open to options.

HARRISON– For this southern building, tenants have not been identified. If a tenant wanted to request an outside terrace to modify more shelter, do they have the ability to provide more shelter?

Applicant Derek Doke - This is very sought after location, and the focus has been retail, with potential for a restaurant. At this point we do not know the exact usage; however expansion is capable on the building.

LEONGLEONG – Has concerns that the tenant has not been identified, and what type of use might go into this location.

Applicant Doke - Clarified allowable usage, for example a tire chain would not be allowable at this juncture. The tenant would be primarily retail, and multi-family usage is allowable.

BRENNAN – Identified that the goal is to create a welcoming community space. Which would include fixed features and flexible features such as moveable chairs and umbrellas like you might see outside of a coffee shop. Let's make sure these are appropriately addressed and we don't want to get overly rigid. Flexibility is an important piece of how this space gets used.

LEONG– That is a very good point and I echo that but I think to be fair to the applicant, as a Commission we should not ask the applicant to keep coming up with ideas. It takes time and money. I caution we are cognizant of that point.

HARRISON– Requested the applicant to briefly review what was included in the plan for this site.

Applicant Doke – This public space at the southwest corner of the southern building is open and was not planned for anything. When the tenant comes in they can come in for permit and work with staff to design the outdoor seating etc.

Favour - Identified that condition #4, states that more has to be done to this space at the north of the northern building.

MORGAN- Sounded like it was short of meeting the requirement which is 20' wide.

Applicant Kimera – There are several above ground traffic utility vaults we are trying to work around so they are not an eyesore, the landscape should help cover these.

MORGAN- Why was it not put on the south side of the building, a larger plaza area there?

Applicant Kimera – We wanted to create a corner, focus point to the two streets and try to get seating out there with a buffer, then people will be willing to sit out there. There should be some buffer because they are busy streets and the building is pretty close to the north along 56th Street. For the drive through they planned about 8 car stacking spaces and the further they move to the north, the shorter the room for stacking spaces.

MORGAN- Was Issaquah staff pushing the location of the building on the property?

Favour - You almost have room to get seating on the south side as well as the north and east sides. We requested an architecture element at the northeast corner. There will be people walking to this location too so there is a need for an entrance element at the northeast corner. However the plaza could shift to the sunnier and quieter south side.

Applicant Doke - The bus stop is on 56th right next to the drive-through circle - tried to tie in and provide the ability to walk into the plaza.

Favour – There is flexibility in this code to accept some different dimension than 20' x 20' and flexibility is determined by the Director or the Commission.

PAULY – That outdoor areas around Taco Time is designated community space, not leasable space for the restaurant? So they benefit because there are benches there, this is a community space.

Applicant Doke – This area also provides access to the trail in the west buffer.

Applicant Kimera - Ideally there would be activity on 3 sides of the building, on all 3 sides there would be some kind of active space.

PAULY - Was there a condition for getting it 20 x 20 in that specific area north of the building? Or does it include the one in the island to the west as well? The walkway between it?

Favour - It's the graded pavement area about 30 x 15 and you have the walkways to another public space, it's the activity, seating, weather all those elements that make up a space where you want to be.

PAULY – I think the 2 areas connected like that make a pretty good space.

Applicant Kimera – Regarding the plaza at the southwest corner of the southern building, we can work something out when we identify some of the uses in that Southern Building.

Chapter 8 Parking

PAULY - Is there a condition that those 4 stalls should come out?

Favour yes there is. Tied into the buffer increase, if there is not a condition, let's add one.

PAULY - Do the Commissioners agree with the staff recommendation and that the buffer had to be maintained and that those parking spaces had to go?

MORGAN- The drive lane there is a buffer reduction area.

PAULY – A wetland buffer and another is a stream buffer - not a big difference but a difference.

Favour –The stream and wetland buffers are being addressed by different code elements.

HARRISON – I am a little antsy on cutting back on a buffer, I would rather cut back on parking spots as opposed to further reductions in buffer. I would go with cutting parking over buffer reduction.

MORGAN- What if we take out the parking on the street to provide bike lanes on the street?

PAULY – No, I think they are over the recommend parking.

Favour – Yes, they are over.

BRENNAN - There is a way to accomplish this rebalance. You can be within bounds of parking allowance for these uses based on the code. We are above the maximum, having exceeded the amount of parking spaces and at the same time reducing the buffer. For me I am in the camp of achieving a balance that achieves environmental balance and still be in compliance with the parking standards, and I think that can be accomplished. It's a pretty healthy amount of parking count allowed for this project.

Favour – It does not have to be a choice of this or that, we could shift the parking around and adjust landscaping to increase the buffer to the full width.

HARRISON- The wording “The proposal falls 3 stalls outside the parking maximum” Is it 3 over?

Favour – Yes - 84 is the maximum, the proposal is 3 stalls too much.

HARRISON- Cutting out 4 for the buffer which put the parking to 83 leaving 1 parking stall short of the maximum standard.

MORGAN- It looks like this is a very small 4th parking stall, lanes could be shifted to be out of that. This is the first time we have put in the maximum required parking. I agree with this recommendation.

PAULY – I think it needs to be a little more generic, we want the stream buffer restored -how they do it is up to them. Depending on how the street layout works, with some sort of bike lane, I am not opposed to on street parking I am opposed to losing the bike lane. There are a couple of ways to get this done.

Favour – You still don't know what tenants are going in the southern building. I have encouraged the applicant to pursue housing on the second floor of the southern building for some mixed use here so if you bump up to the maximum of parking this could help supportmixed use later and keeps options open.

LEONGLEONG – I am trying to understand why there are so many dead end parking clusters, I counted 4. The site is being eaten up by the drive through area. I don't mean to have you redesign, however there seems like a better way of siting your Taco Time. This point came up before with a previous potential tenant. We talk about the number of stalls, it seems like there is a lot of driveway concrete that is eating up your space here. The circulation does not seem to be flowing well.

HARRISON- You want to have space for the people waiting in a way that does not impact the regular traffic, the non drive-through traffic, also, trying to get the maximum exposure to passersby. It does not seem like

there are a lot of options on where to put the driveway so that a sidewalk would not be hazardous.

LEONGLEONG – There is a lot of conflict driving through the driveway, meanwhile you have a stream of cars and the ingress/egress for the people coming through the drive through and those coming out of parking areas. Suggested parking in the middle, have community space right there too. Not a very easy flowing design.

Favour – We did ask for the drive through to be in the back of the building, and that was a good thing.

LEONG– Could you could put the drive through on the south side of the building?

Co-applicant for Taco Time Chris Tonkin - That is a big question, there are a lot of competing activities in there - one of the requirements is the city wanted the drive-through to be hidden from view of the public streets, but we would like to have the drive way wrap around the building - that is the more traditional drive through in our system. As far as those dead end stalls, they would be designated as employee parking so less conflict. The rest of the field of parking is not that much; we usually have 45 stalls for a 2700 square foot restaurant. This has 27 stalls, for a business model, this is under parked. We have cross access parking easements with the larger building to the south to make up for that. You have a logical idea, where the drive through would wrap around the building however with the guests parking to walk in, they would then need to walk across the drive through lane and with 60% of our business coming through the drive through lane, it is pretty busy and we don't want our walking guests having to cross that area.

HARRISON– Personally the explanation that you can use those parking spaces as employee parking, and they will be occupied by employee's cars, significantly reduces concern about parking knowing these are staff stalls.

Chapter 9 Signs

HARRISON– There is significant change in process from the past with new standards. The CIP sign chapter sign review is completed at the staff level unless the applicant provides signage as part of the Commission step. That is different.

Favour - Conceptual signs that are shown in the packet are not at the level of detail we are ready to talk about tonight.

MORGAN– I presume you have the ability to bump it up if you found it to be necessary?

Favour – Yes, yes we do.

MORGAN– To gain the wisdom and the experience of the Commission.

Chapter 10 - Landscaping

Favour – Regarding the discussion around potentially invasive evergreen shrubs, there is a condition to swap out keeping the 3 foot high landscape buffer around the parking lot, a short hedge between parking and sidewalk. This is where the Japanese holly and Japanese barberry is. We will find another substitute plant for this area.

Increasing the tree sizes are addressed through a proposed condition, we are trying to shade the surface parking lot as much as possible. The city would like to push for broad spreading canopy trees, that is addressed in condition #8. There are several landscape conditions that overlap into the landscape design chapter. The two Landscape chapters overlap and address the landscape issues.

BRENNAN – If you could comment about the interface between the building and the sidewalk. The proposed treatment is a fairly harsh condition.

Favour– This is addressed in Ch. 11 & Ch. 14

Chapter 11 Site Design

Favour- The essence is even if the building is right at the sidewalk we still want to get some green element between the public sidewalk and the building.

BRENNAN – That would consume some area of the sidewalk width? What dimension gives here? What are some example treatments?

MORGAN– I recall the plans showing a 4-5' setback between the building and the sidewalk.

Favour - If it is at zero feet there are 6"-12" pillars on buildings that go in and out, when it comes in we would propose putting a strip of greenery as well as perhaps extending out to the 8' sidewalk maybe 12" or 18".

HARRISON– Is this condition 13?

Favour - Yes the condition is based on Chapter 11.3 of the code.

MORGAN– For clarification, the planting along the building does not have to be continuous? Right?

Favour – Right.

BRENNAN – In combination of architectural features it's the idea to make the pedestrian experience interesting, it doesn't always have to be plants, it's a combination of the building, architectural features and greenery.

Favour – Yes. For example it could be a combination of tables and chairs, greenery, architectural details - it's all that combination of activity

Chapter 12 Review- Circulation

Favour– Well we pretty much covered this chapter through the bike lane discussion as well as community space, and making sure that the planter strip is highlighted. I would like to compliment the applicant for their design of the 221st planter strip - there are some nice features with boulders for sitting, steps and benches - this is off to a good start.

PAULY – This wetland trail is it under "Circulation"?

Favour – Yes. I can talk about that or do you have specific questions?

PAULY – Yes. I am wondering about the connection at the south end that goes to the adjacent parcel. Is this if we get a trail all the way along to the south?

Favour – Yes it is. And the River and Streams Board discussed why are we having this whole trail back here? You could just have those walkways that head east-west, for a total of 3, however development may occur to the south, so our recommendation is it's best to keep the options open.

PAULY – I like that it comes back into the parking area into the community space at the south end of the building. When staff reviews this area, if there is an opportunity in the open community space that maybe there is information about the trail, or provision of a bench, or covered area for someone who walks and can't continue their walk. They will then end up crossing and go into that community space and there is another feature that ties it back to the trail or something like that. The crossing of the parking lot is not painted at the south end as a designated walkway, is it missing on the plan?

Favour– There is just not a walkway shown across. Asked the applicant to put one in that goes across the southern end of the parking lot. Were you asking about way finding signage?

PAULY – Just anything should be provided, the way the community spaces read in the code you gave us tonight it's supposed to be a community space not just an empty space with a bench on it.

BRENNAN – Just an additional thought might be that while we have that buffer right there, it might be a way to connect the landscaping and the crossing to the trail, obviously with that community space the code requires that it be more than just a concrete slab with chairs on it and additional features will be included. I think it's a good opportunity to enhance that space, the connectivity back over to the stream corridor.

HARRISON– I agree with everything, I think that I would add a cautionary note, that it seems to be as obvious as the need is to designate a crossing here, I am a little leery about traffic entering the site. I am looking at line of site of someone coming in from 221st and someone driving north through the parking lot, I don't know how high the landscaping will be but if that blocks vision that could be really bad. They may not see a car coming, and a car may not see a pedestrian. Keeping a path in there is congruent with what we were talking about earlier with the bike lanes; we are trying to encourage this, trying to encourage both bike and pedestrian traffic. And having it there and having the explicit expectation for anyone who does anything with the adjacent parcel, we are looking at a path that is there and it's going to be pretty hard to explain why they are not going to continue it so long as there is space to do it. I understand the further you get along with it there is less space so it may be a moot point. With that said, I think it's a good idea.

LEONG– I agree, if I can add it's the same thing with folks parking on the south west part of the parking lot. I guess when you drive into the lot, you slow down and there are a lot of folks crossing.

HARRISON- Good point, slow is the issue there. People drive a little faster than they used to. The other thing is those parking spaces that are being taken out there, if you are going to leave now you would be backing out into the face of oncoming traffic so the spaces identified earlier for being removed these would be good spaces to remove.

LEONG– This community space we are providing on the southwest corner of the southern building, is that for 1 tenant or for 3 tenants? Do they each have to have their own community space?

Favour – We have not declared explicitly, I saw it as one community space shared by the building.

LEONG– So we don't really know, if they come in it's possible they could have 3 or 4 tenants coming in and only the one on the south end would have the community space used then.

Favour – I would see it being available for all of the tenants, a public space. Anyone on the property should be able to use this space.

LEONG– We are at a disadvantage trying to address this community space.

HARRISON– I have a question on the environmental review. I know that Mr. Rosen's comments are included in the staff report. Are you aware of any Federal Law for protection on eagles that have an impact on construction on how close to an active nest you can get? The reason I ask is there is a nesting pair close to this location. I did not see it addressed anywhere.

Favour- I know some of those nests are around the community and purposely kept quiet. It's a good point to raise, we should be aware and if it applies we'll apply it.

MORGAN– I would like to ask the applicant why the walkway was not continued from the southern building to Taco Time?

Favour – We had that discussion with staff too, I think we can go either way, it's not that hard to walk from here to there.

MORGAN– Since it's already there, if it works to connect them in?

HARRISON– Are you talking about taking it across the parking lot?

MORGAN – Yes.

Applicant Kimera – This design here from the northern to the southern building, I don't think was discussed in our meetings, I know the space in there is absolute minimum clearances for parking and aisles. If we did accommodate that we might lose at least one row on the Taco Time side. We aren't at the maximum yet; toying around with compact stall we would lose 5 stalls and 4 stalls on the southern building.

HARRISON- Are you saying that if we did what MORGAN is suggesting it would result in that?

Applicant Kimera – Yeah, I think what he was suggesting that maybe a walkway that would connect the southern building all of the way to Taco Time, that strip there is not more than 3', it's a very narrow strip. It would reduce the length of the parking stalls.

Chapter 13 –Community Space

HARRISON– Let me deviate from the norm here, when David and I met earlier this week we talked about the process for this, one of the things that David told me is the philosophical change and the way things are done, do you want to explain the level of detail that is now going to be presented and the responsibility or the area of inquiry for the Development Commission.

Favour – Well we are still working through this and it's not official or anything yet but it's an evolution from the 1990's – 2000's until now and part of this whole code update is to create and streamline the process and have the Commission look at the conceptual elements and the intent, concepts and turn the detail reviews over to the staff level.

HARRISON- Then that would be presented to the Development Commission in this meeting when it proceeds or actually in the Community Conference as well so there would be a presentation and the Commission would know for example Fieldstone is being used here. Would we even have an opportunity to be made aware of that or what is being proposed or how does that work? I think a lot of times a significant amount of discussion has been over the appearance of the buildings, the structures, the materials that are used.

Favour – The idea is to still show the materials, color, designs and then if the Commission is comfortable with the approach, you would say “yes, this looks good”, staff go ahead and finalize the detail review of it. Or like we are doing tonight you see that plaza there with a couple of benches, but we are not showing the exact final plan and we would like to step closer to this concept you see here and then have staff finalize it at the building permit construction phase.

HARRISON– Would the Commission be given as we were in the past, examples of materials that would use would that be part of the development going forward?

Favour – I think it would be. We have a week set aside, in June for the staff to look at the entire land use permit process to discuss issues such as this and consider changes following the week long process review.

BRENNAN - This is a transition period, and we are working to find the new normal. I made some notes in the margins about these are very generally written conditions and different from the way staff reports were previously done. I am o.k. with this, we are going to need to have experiences here that there is value add

from the Commission. I guess one thought, it would be nice to have some projects loop back in their finished state, to show how us how did it end up so that we develop confidence in the end product and we are achieving what we intended to achieve.

Favour - I think that is an excellent idea. I would like to add, I was pressed for time on this staff report writing and I really would have liked to have had more details in these conditions.

PAULY – Maybe we could talk more after the hearing, that was one thing I noticed, a little more detail needed in the conditions.

Chapter 14 Buildings

PAULY – On Chapter section 14.5 I don't really have a question, I just noted that I am glad you are talking about where there is protection. I guess one thing I would like to make sure is however the community spaces end up that you consider some sort of weather protection for the features maybe a canopy over a table or whatever. So that not just the building is protected but that these community spaces are covered, if it makes sense.

Favour – Agreed, the code , for example, talks about gazebos and things like that.

MORGAN– Just a general question on section 14.4 the kick plate moving it from 9” to 24” I am just curious about the design standard the thought from retail space design. Is that a big issue, it sounded reasonable to me?

Applicant Kimera – If I understood the question correctly, actually the reason we raise it is for tempered glass. It's more cost effective to have the larger kick plate, and it's a safety feature.

Chapter 15 Parking

HARRISON– Is there anything else on parking, any concerns? Suggestions, conditions?

PAULY – I think under section 15.4 we are going to propose a condition that talks about marking out that pedestrian walkway between the trail and the building.

Chapter 16 Review - Landscape

HARRISON– You talk about in 16 that the greater use of native plants should be the goal, that is part of the recommendations.

Favour – Yes, and a large part of the buffer will be enhanced with native plants and that is a SEPA determination that is attached.

PAULY – David, do you know where native plants are referred to in the conditions? I don't see references to 16. I see 16. 3 fence standards in the conditions. I don't see the conditions that reference native plants. I am wondering if we need another condition that says there will be an expansion of native plant use.

Favour – OK, we can add a condition expanding the use of native plants.

Chapter 17 Lighting

BRENNAN – I did not see a lot of detail on the lighting fixtures, so I did not spend a lot of time there

HARRISON– I would assume the objective of the new standard is to mitigate overflow of lighting into the adjacent property, certainly we do not want any light to go into the creek at night because of the salmon obviously. That's part of the code, right?

Favour – Right. It is in today's code and that would continue into the new standards.

HARRISON– After chapter review and the opportunity for input, I believe we are at a point where we will discuss staff recommendations to us.

Motions

MORGAN– I move that we approve the application for Issaquah Plaza 221 **File Nos. PLN12-00065 – 67** and the following 22 conditions.

PAULY – seconds the motion – moved and open for discussion

PAULY – I would like to propose that we add a condition #23 dealing with the addition inclusion of bike lanes. Prior to issuance of the building permit, applicant to revise plans to include appropriate right-of-way width to include a bike lane or future bike lane by reducing or eliminating the street parking, by reducing the landscape median and or reducing the sidewalk width.

MORGAN- seconds the motion

HARRISON– we have an amendment, the Commission voted unanimously in favor. Motion Carried as amended.

PAULY – I have a second condition which has to do with parking, in the South end of the site. Moves to add condition #25 that Prior to issuance of the building permit revise parking in the southwestern corner of the site to maintain the 100' stream buffer, with an exception in the drive lane where necessary to ensure safe vehicle circulation.

MORGAN– seconds the motion

HARRISON– the Commission voted unanimously in favor. Motion Carried as amended.

MORGAN– Moves to add condition #25 that would state prior to issuance of the building permit applicant shall reduce the number of parking stalls to the maximum allowed per section 8.0.

PAULY –seconds the motion

HARRISON– the Commission voted unanimously in favor. Motion Carried as amended.

MORGAN– Moves to add condition #26 which states prior to issuance of the building permit, applicant shall include markings for a pedestrian crossing from the southernmost extension of the trail to the community area on the South side of the Southern building.

BRENNAN - seconds the motion.

HARRISON– the Commission voted unanimously in favor. Motion Carried as amended

PAULY – I have a condition to add prior to issuance of the building permit, parking stalls located West and South of the drive through land shall be labeled as employee parking only.

HARRISON– the Commission voted, the motion was voted down. Not Carried as amended.

MORGAN– Moves to add condition #27 would state applicant shall provide a greater use of native plants in the transition between the native and built environment.

PAULY – seconds the motion.

HARRISON– the Commission voted unanimously in favor. Motion Carried as amended.

HARRISON– Commission voted unanimously in favor of approving the application for Issaquah Plaza 221 **File Nos. PLN12-00065 – 67** with the above amendments. Motion Carried.

MORGAN– Moves to direct the Development Services Department to prepare Findings of Fact for signature by the Chair which affirms the Development Commissions decision to approve the application for *Issaquah Plaza 221 Files Nos. PLN12-00065-67*

BRENNAN – seconds the motion

HARRISON– the Commission voted unanimously in favor. Motion Carried.

MORGAN– Greatly thanks the efforts the applicant has made in working with the city, and especially having to be the pioneer. Also on the building design and providing the attractive frontage on all sides of the buildings which is really a necessary part of bringing those buildings out to the street giving that pedestrian interest. I think you guys have done a great job.

HARRISON – I would like to add to that, there is a tremendous amount of time and money being put into restoration of the Issaquah Creek and the boundaries by the city and private elements too, and I personally think what you are doing especially on the creek side is really nice and sets a good precedent so thank you for that.

PAULY – Driving by your site today you have a beautiful site to build on and you have done an excellent job I think developing the hardscape portion of it, trails and the linkage are just right. You have done a really good job.

Updates

Favour – It's not official yet, I believe the Administration is leaning towards delaying the consolidation of Commissions for perhaps a year or so. Thank you to the Commissioners for participating in the survey process.

PAULY – Is the City expecting us to be more active?

Favour – We know of some projects that are in the pipeline in the valley area but there still aren't a whole lot of them coming down the line. There's a bunch going to the UVDC in large part because Port Blakely's selling some of their land so there is a real rush right now. On the valley floor, the Middle School is getting ready to build a new school and that will come to the Commission.

HARRISON– Recommended that the Commissioners have a development workshop, held at the same time we would have a development meeting.

PAULY – Would like to see a development workshop rolled into what the expectations of the Commissioners are.

HARRISON– If the city is going to continue our existence, then along with the changes that are being made along with the new plans, I still think that we have to proceed on the basis of what our responsibility is and ultimately we don't want any surprise. I personally think we need to strike a medium, between the efficiency and the responsibility.

BRENNAN – I think we also need to have a conversation about the role of staff vs. the Commission and the decision making process because the role has changed. The standards have changed, they are much more discretionary. The other piece is procedurally on how we want to go through this in an efficient way.

HARRISON– Requested that the Commission send him documentation of what they would like the workshop to cover. I will compile them and send them back out, talk to *Favour* and get back to the Commission for further discussion.

Commissioners discussed the need for process review during a workshop which will be scheduled at a later date. *Favour* agreed and will coordinate the workshop details. There was discussion of what a staff report format would look like to most efficiently allow the Commission, staff, and the public to review projects. He said he would work with staff on creating a staff report template perhaps with a column approach for the report.

Final item - election of officers

Chair Nomination: MORGAN nominated Randy Harrison as Chair, PAULY Second - all approved.

Vice Chair nomination: Commissioner Ray Leong nominated Carl Swedberg as Vice Chairman, BRENNAN Second - all approved.

The meeting adjourned at 10:10 p.m.

These minutes are a summary of the Development Commission meeting held on May 1, 2013. Recordings of the meeting are available for review at the Development Services Department, 1775 12th Avenue NW, Issaquah, during the hours of 9:00 a.m. – 5:00 p.m. Monday thru Friday.

**CITY OF ISSAQUAH
DEVELOPMENT SERVICES DEPARTMENT
DEVELOPMENT COMMISSION**

COMMUNITY CONFERENCE

STAFF REPORT

August 7, 2013

FILE NO.: Community Conference: PLN13-00002

PROJECT: Issaquah Middle School

OWNER: Issaquah School District
Representative: Steve Crawford
565 NW Holly Street
Issaquah, WA 98027

APPLICANT: Mahlum Architects
Representatives: Gregg Stewart, David Mount, Forest Payne
71 Columbia Street, Floor 400
Seattle, WA 98104

STAFF CONTACT: Jerry Lind Senior Planner
Development Services Department, (425) 837-3091

REQUEST: An application for a Community Conference has been submitted by Mahlum Architects, to convert the existing campuses of Tiger Mountain School and Clark Elementary School (both buildings to be demolished) to a new Issaquah Middle School. The new Issaquah Middle School will include one and two story sections and provide approximately 126,000 sq. ft. of academic, athletic and performing arts space on a 63 acre site. The site development will impact approximately 33.6 acres and include reconfigured parking for middle school staff and visitors; relocated high school parking; middle school bus loop; middle school softball field and track and field; relocated high school playfields; and a new driveway for the Transportation Center on the west side of 2nd Ave. SE.

Issaquah High School and the associated stadium are not part of the redevelopment and will not be modified for the project.

LOCATION: The project is located at 500 2nd Avenue SE in the Old Town subarea. See the attached Vicinity Map, Exhibit No. 2.

EXISTING LAND USE:
Subject Property: The location for the new middle school currently is occupied by one

story buildings for Clark Elementary School and Tiger Mountain Community High School and associated parking, Issaquah High School Parking and Issaquah High School softball fields. The construction limits will include approximately 33.6 acres. The complete size of the school campus including the High School is approximately 63 acres.

North: Single family residential and duplex.
South: Issaquah High School.
East: Issaquah Sportsmen's Club and undeveloped property.
West: 2nd Avenue SE, School District Transportation Facility.

EXISTING CONDITIONS: The project site is currently occupied by Clark Elementary School and Tiger Mountain High School. Both buildings will be demolished and relocated to the existing Clark Middle School that will be remodeled. The new middle school will be constructed over the existing Clark Elementary School play field and a portion of the existing High School parking area.

EXISTING ZONING: The zoning of the property is Community Facilities - Facilities (CF-F), which allows schools. In accordance with IMC 18.07.360, the Development Standards for the site (i.e. setbacks, impervious surface coverage, building height, etc.) are determined by the most restrictive contiguous zoning. The zoning to the north of the site is SF-SL (Single Family Small Lot) and SF-D (Single Family Duplex).

The development standards of those 2 zones are very similar, the difference being SF-SL has a 20 foot rear setback and the SF-D zone has a 10 foot rear setback. East of the site is King County and Community Facilities zoning and west of the bus barn area is Multifamily High Density zoning. The most restrictive zoning is the SF-SL zone which will be used as the Development Standards for the new middle school.

COMPREHENSIVE PLAN: The site is designated Community Facilities by the Issaquah Comprehensive Plan, updated April 29, 2013.

SUBAREA: Olde Town

BACKGROUND:

March 20, 2013	The City's Project Review Team held a Pre-Application Conference with representatives from the Issaquah School District and Mahlum Architects, File No. PRE13-00003.
June 12, 2013	Mahlum Architects, representing the Issaquah School District submitted the application for a Community Conference with the Development Commission, File No. COM13-00002. A letter of Complete Application was issued on June 20, 2013.

Dec. 17, 2008 Dev. Commission public hearing held for High School Master Site Plan (PLN08-00046) and Site Development Permit (PLN08-00047).

NEXT STEPS:

The process for reviewing the new middle school will be a Level 5 Master Site Plan (MSP) application and a Level 3 Site Development Permit (SDP). The MSP is required because the project is more than 15 acres. Ultimately, the MSP and SDP will be reviewed and decided by the City Council after a public hearing and recommendation from the Development Commission. Any Administrative Adjustment of Standards that will be required, such as for Building Height, Landscaping or Fences, would also be reviewed concurrently with the MSP and SDP applications. The contents of the Master Site Plan application shall be in accordance with IMC 18.07.650.

COMMUNITY CONFERENCE - PURPOSE:

Section 18.04.140(A) of the Issaquah Land Use Code states the following under Community Conference Purpose:

"The Community Conference is an informal community meeting, hosted by the Development Commission. The purpose of the meeting is to generate discussion, raise issues, and propose creative options relative to the proposed project. It is intended to provide a means by which the applicant, staff, the Development Commission and the public are able to work together in a productive and creative manner. However, options and issues raised may not be all inclusive and no guarantees on the project outcome are made at this stage."

Section 18.04.140 (B) states the following under Community Conference Expectations:

"The applicant can expect the following results from the conference:

- 1) The more information an applicant can provide for a community conference, the more complete staff's review and input will be for the proposal.*
- 2) Any information or opinions expressed by the Development Commission or the staff shall not be binding on the final decision or constitute approval or denial of the proposed project.*
- 3) Inconsistency with the Comprehensive Plans, Guiding Principles or City Vision will be discussed.*
- 4) Development Commission, staff and applicant should discuss creative approaches to address challenging site constraints or potential mitigations.*
- 5) Recommended revisions or modifications to the proposal will be discussed; and*
- 6) The applicant should be aware that additional modifications will most likely be required before the project review is final and a decision has been made."*

MASTER SITE PLAN REVIEW:

Purpose: Per IMC 18.07.590, the purpose of the Master Site Plan process is to provide flexibility to develop large parcels of land (fifteen [15] acres or greater), while assuring that:

1. Appropriate provisions are made for but not limited to: water, sanitary sewer, drainage ways, utilities, motorized and nonmotorized transportation circulation improvements, police and fire service, and any other applicable services;
2. Critical areas will be protected;
3. Usable open space will be provided;
4. Provisions are set forth for pedestrian and vehicle circulation including sidewalks and other planning features that assure safe walking conditions for students who only walk to and from school;
5. Maximum densities are established for each of the proposed land uses, as established in the Table of Permitted Land Uses (IMC18.06.130) and District Standards Table (IMC18.07.360);
6. General design elements and linkage components are established per the Master Site Plan approval criteria; and
7. Approval criteria and mitigation measures are established.

Review Process: The Issaquah Municipal Code (IMC) Section 18.07.600 states that Master Site Plans shall be reviewed through the Level 5 Review process. The flow chart in Section 18.04 for Level 5 Review requires the Community Conference for Master Site Plans. As stated above, the Master Site Plan, concurrently with the Site Development Permit, will ultimately be approved by the City Council after a Public Hearing with the Development Commission. Through the review process, the applicants must show that the project meets the approval criteria for Master Site Plans as listed in IMC 18.07.660, Exhibit No. 5. The project will also be required to meet the approval criteria for "Non-utility Community Facilities" as listed in IMC18.07.480(E), Exhibit No. 4 and Nonmotorized Facilities Standards, Exhibit No. 6.

PROJECT DESCRIPTION AND ANALYSIS:

A. Use:

The applicants are seeking a Master Site Plan (MSP) and Site Development Permit (SDP) for the construction of Issaquah Middle School. The new school will include one and two--story sections and provide 126,500 square feet of academic, athletic space on the school district's 63 acre site. The comprehensive school program includes core academic instruction, special education, performing arts, fine arts, media, physical education, a library and a commons. A covered play area building (basketball) is also proposed located west of the Middle School.

Site development will impact approximately 33.6 acres of the existing site including reconfigured parking for the middle school staff and visitors, relocated high school parking, a middle school bus loop, middle school softball field and track and field, relocated high school playfields and a new driveway for the transportation center west of 2nd Avenue. The site will accommodate four double portables for future growth.

The facility will be designed to serve an enrollment of 900 students and will have 38 teaching stations. The project is proposed to be phased over two years (Spring 2014 through 2016). Construction phasing will be controlled by the requirement to fully occupy

the Tiger Mountain Community High School, Clark Elementary School, and the new Issaquah Middle School on one single site while a modernization of the existing Middle School (by the Community Center) takes place in preparation for the Tiger Mountain and Clark Elementary students to move in. The detailed narrative description is attached as Exhibit No. 3.

This meeting scheduled before the Development Commission is for the informal "Community Conference" meeting required as part of the development approval process. A Master Site Plan will be required along with a Site Development Permit. The Master Site Plan is required to be approved by the City Council. The Development Commission will recommend a decision to the City Council regarding the applications.

Zoning of the property is Community Facilities- Facilities (CF-F), which allows schools. In accordance with IMC18.07.360, the Development Standards for the site (i.e. setbacks, impervious surface coverage, building height, etc.) are determined by the most restrictive contiguous zoning. In this case, the most restrictive contiguous zoning is Single Family-Small Lot, SF-SL.

B. Development Standards:

1. Findings of Fact

The following are the development standards for developments in the "SF-SL" zone:

ITEM	PROPOSED	CITY CRITERIA
Maximum Density:	N/A	4.5
Minimum Lot Size:	N/A	6,000 s.f.
Max. impervious surface:	39%	50%
Min. pervious surface:	61%	50 %
Max. Base Building Height:	44'-4" *	30'

SETBACKS	PROPOSED	CITY CRITERIA
Front Yard (west: 2 nd Avenue)	480 feet	10 feet
Rear Yard (east)	460 feet	20 feet
Side Yard (north: Evans St.)	30 feet **	6 feet
Side Yard (south)	N/A (High School)	6 feet

* The maximum height allowed in the SF-SL zone is 30 feet. To exceed the 30 foot height will require an Administrative Adjustment of Standards to be processed with the Site Development Permit. A permit of this type was processed for the High School which has a height of approximately 48 feet.

** The current setback shown is approximately 60 feet. After street dedication given for Evans Street, the building setback will be 30 feet.

C. Access/Street Improvements/Traffic:

Access:

Access to the middle school will be from a shared driveway that was constructed for the high school off of 2nd Avenue SE which is a signalized intersection. This same intersection will also be the new bus entry to the Transportation Center on the west side

of 2nd Avenue which will have one drive lane down and 2 drive lanes up. The new roadway grade leading to the Transportation Center is not identified and retaining walls within that steep sloped area will be up to 20 feet high maximum. The existing road to that transportation center is proposed to remain for vehicle access. That campus entrance to the High School has 2 lanes in and 2 lanes out. An existing driveway that currently serves Clark Elementary School will be removed. The existing driveway location serving Clark Elementary and Tiger Mountain High along SE Evans Street will also be removed. Access along Evans Street is proposed for the service yard area north of the school and at the end of Evans is an entrance to bus parking and a parking lot to serve staff. A 24-foot wide roadway is also shown on the south side of the building near the main entrance as "student drop off". That is a looped roadway around the south parking lot.

The gravel roadway that serves as access to the Sportsmen's Club will be will be reconfigured to the north to accommodate the new middle school softball field. The reconfigured road is proposed to remain as gravel.

The project will need to meet the requirements for Nonmotorized facilities as described in IMC 18.07.080, Exhibit 6), including walkway connections throughout the site and to the adjacent roadway sidewalks, bicycle parking, etc.

Fire access: Fire access will be provided around the school. A reinforced grass pavement Pedestrian promenade will double as a fire access drive along the west side of the school and will connect to the fire lane at the middle school parking lot for access to the south side of the building. Fire lane access to the east of the school will be provided via the bus loop. The fire lane access to the north of the school will be provided via the service entrance. The configuration and development of access to the courtyard will be reviewed with the Fire Marshall as part of the permit process.

The narrative states that interim fire protection , including temporary fire hydrants and temporary dry fire sprinkler system if need, as well as a 20 foot clear fire access land will be maintained throughout the construction process.

Street Improvements: All sidewalks along the frontages shall be replaced as needed with curb, gutter, sidewalk and landscape planters to meet current City standards. Approximately 30 feet of street dedication will be needed along Evans Street north of the school which is currently a half street.

The current access drive that serves the Issaquah Sportsman Club is a gravel road that is proposed to be relocated about 80 feet further north. The rerouted drive is proposed to remain as gravel.

Transportation Concurrency: In accordance with IMC 18.15.240(A)(2),the following is exempt from Transportation Concurrency: *"Public elementary, middle or junior high schools; including new facilities and any renovation, expansion, modernization or reconstruction of existing facilities and/or the addition of portable facilities"*.

Rainier Trail: The current Rainier Trail runs between the High School parking lot and the existing softball fields that will be replaced. The proposal is for the trail to be re-located to the east of the new track and field within the City's 50 foot wide unpaved roadway and utility easement and off the school property. The new trail would be about 14 feet above the track. Landscaping on the hillside between the track and trail should

not be fully hidden by densely planted trees to allow some visual surveillance for trail users. The design of how this will fully work and connect to the existing trail system is not yet fully worked out or approved. The Parks & Recreation Department will want to approve the trail relocation and construction.

The applicant will need to provide schematic level sections showing that the a road can meet the City's standards to include a 12' shared use trail, 5' of landscape strip, 5' bike lane, and 2 travel lanes within the 50' easement. The shared trail should be a minimum of 5' east of the property line.

D. Project Phasing:

Aerial photos with the project development imposed on those aerials illustrate the phasing of the Middle School project (6 sheets show phasing with Exhibit 8). The entire project will take a few years to complete with different areas of the large site being affected at different times with regards to demolition, staging and construction. The narrative states that the school project will occur in 3 phases as follows:

- Phase 1 (Spring 2014 to Fall 2015) will include constructing the new middle school, demolishing the high school parking lot, constructing the new middle school softball field, track and field and parking area and bus loop, relocating the Sportsmen's Club access road, construct the new entrance road to the Transportation Center, and construct all flatwork within construction zone boundary around the Middle School.
- Phase 2 (Summer 2015 to August 2016). At the *existing* Middle School, this will include demolishing portions of the existing school buildings, removal of flatwork and grub landscaping, construct approximately 50,000 sq. ft. of addition for Clark Elementary, tenant improvements to a portion of the existing facility for Tiger Mountain School and Clark Elementary School of approximately 53,750 sq. ft.
- Phase 3 (Summer 2016 to December 2016) will include demolition of the existing Clark Elementary and Tiger Mountain school buildings, demolition of existing parking lots and associated flatwork and landscaping associated with Clark Elementary and Tiger Mountain schools, removal of temporary classroom portables and platforms, construction of flatwork within construction zone boundary around the new building, completion of middle school parking lot and pick-up/drop off loop on the south side of the school, construction of high school parking lot on the west side of the project site, and construction of high school play fields on the west side of the campus.

IMC 18.07.660(F) lists the elements of the Master Site Plan application that must be deemed acceptable to the City Council prior to approval. Item No. 7 addresses "Phasing" in the following manner:

"Each phase of the proposed development, as it is planned to be completed, provides for the required parking spaces, streets, sidewalks, recreation facilities and park land, landscape and open spaces, critical area designations and buffers and utility service areas, and rights-of-way necessary for creating and sustaining a desirable and stable environment."

With submittal of the Master Site Plan application, the School District will need to submit a detailed plan for the phasing and staging of the entire project. Of special interest will be how the school proposes to address parking for the adjacent high school students

and staff during periods when there may be less on-site parking. This issue of phasing and its impact to the surrounding neighborhood should also be addressed in the SEPA review and determination by the District.

E. Storm Drainage/Soils

Storm drainage will be required to follow the applicable King County Surface Water Design Manual. All runoff from the site will be infiltrated onsite. Roof runoff will be collected in downspouts and conveyed to underground infiltration trenches, mimicking the current site system. It is anticipated that the soils will have sufficient infiltration capacity to infiltrate field runoff as well. Water quality requirements will be met through the use of rain garden filtration for all runoff from all driving surfaces, including parking lots and roads. The project is proposed to be designed to minimize underground storage through the use of low impact bio-retention (rain gardens) to take advantage of infiltrative soils on the site. The rain gardens are proposed to be scattered throughout the parking and landscape areas and integrated into the site design.

F. Site Grading and excavation and Geotechnical analysis:

The site slopes from east to west. The campus will be terraced to function. Multiple retaining walls will be required to drop in elevation from the high elevation of the east side to the lower elevation near 2nd Avenue. Efforts will be made to reduce offsite and export of materials. The elevations of the site features were selected in order to balance cut and fill volumes and work with the construction phasing. Any excess soil could possibly be used as berming to the east to help buffer noise impacts from the adjacent Sportsman Club facility.

A foundation soils report by a licensed Washington State Geotechnical Engineer will be required at the time of Building Permit submittal.

G. Utility Improvements (Water and Sewer):

A 12" diameter water main is maintained by the City in SE Evans Street and 2nd Ave. SE. A new 8" water loop will be installed around the Middle School building to provide service for fire hydrants, domestic service, irrigation and fire service. The water loop construction will need to be phased with a majority of the loop and fire hydrants installed during Phase 1 as needed for fire safety and the loop completion occurring in Phase 3 after Clark Elementary School is demolished. Fire service for Clark Elementary will be removed during the Middle School construction and will need to be temporarily relocated until Clark Elementary demolition.

Sewer service currently exists in SE Evans Street and 2nd Avenue SE. The service will be extended south from an existing 8-inch sewer main located in the Campus Access Drive. The grease interceptor serving the kitchen area will be located on the east side of the building. One additional sewer serve will feed the gym locker room from the south which will connect into the sewer main located in the north High School drive which currently serves the High School.

There are also power poles and lines on the site that will need to be relocated

H. Design and Layout:

Building Design: The building will have a similar contemporary design as that of the adjacent high school to the south. The building is proposed to be cited into the middle of the site into space that is currently occupied with a earth playfield just east of the elementary school. The new middle school is designed to closely fit into that space as the elementary school will not be demolished until after renovation is completed at the new site (at the existing middle school). The entry and entry plaza area is located on the southern side of the school near the main parking area. The building is U-shaped, with the open end facing the north. It's unclear how the landscaped/courtyard area in the middle functions.

The school is designed as a two story building and is fairly modulated. Some further modulation may be advisable on the eastern elevation identified as "aux gym lockers" as that 79' long standing seam metal wall is fairly plan. Elevations indicate that building materials include:

- Ground face CUU
- Composite wood veneer panels
- Aluminum curtainwalls
- Aluminum storefront
- Aluminum sunshades
- Fiber cement panels
- Standing seam metal panels
- Rooftop mechanical screenwalls
- Glass windows - unknown

Building Height: One of the most significant design and compatibility issues is the building height. As shown in the building elevations, the gymnasium portion of the building and classrooms will all be approximately 44 feet in height. That height is taller than the houses in the surrounding residential neighborhoods, and is taller than the maximum height of 30 feet that is allowed in the Single Family- Small Lot (SF-SL) and Single Family Duplex (SF-D) zones to the north along SE Evans Street. However, given the size of the school property, the relative isolation of the building (although closest to Evans Street), and the use, the height may not be a problem. In addition, the new High School to the south has a height of approximately 48 feet. An Administrative Adjustment of Standards will need to be requested and approved to exceed the building height of 30 feet.

Site Layout: The new middle school will be constructed over the existing Clark Elementary play field and a portion of the existing high school parking area in order to maintain schooling at the existing schools on the site that will be used until they are relocated to the site of the existing middle school near the Issaquah Community Center. When those 2 buildings are torn down, it will be replaced with parking, indoor and outdoor play fields, landscaping, lawn and a future area for portables as needed. Because of the location of the new middle school in the center of the site, it will be more visible from Evans Street and less visible from 2nd Avenue. East of the new school will be athletic fields for track & sport field and a softball field.

Off-Street Parking:

The Table of Off-Street Parking Standards, IMC 18.09.050, requires "3 spaces per classroom or 1 space per 3 seats in auditorium, whichever is greater". The school proposes to have 38 teaching stations; therefore, a minimum of 114 parking spaces is required for the new school. The application states that 142 spaces will be provided.

Parking structures have been encouraged at the Pre-Application to better use the site and grading, but parking structures are not being provided by the School District to meet the parking needs.

Parking for both the high school and the middle school are overlapping on the school district property. The applicants will need to show how the proposal meets that criterion.

If the number of required stalls cannot be met, an Administrative Adjustment of Parking Standards will need to be submitted and reviewed with the Master Site Plan and Site Development Permit. The School District will need to justify the number of proposed stalls and show that parking is adequate and will not negatively affect the surrounding neighborhood. The criterion for approval of an Administrative Adjustment of Parking Standards is listed in IMC 18.09.060(E).

The parking stall dimensions look acceptable. Standard stalls are 9' x 20' and compact stalls are 9' x 15', both with a 24' backup maneuvering aisle. A combination of both size stalls is proposed. An 18" overhang is also allowed, but if overhanging sidewalks then they need to be widened. The breakdown of standard, compact and handicap stalls will need to be specified. IMC 18.09.090-H states that up to 60% of the required parking may be sized for compact stalls provided they are not located along a fire lane.

Barrier Free parking (accessible stalls) will need to comply with the parking table in the International Building Code (2012 edition), including van spaces. 14 accessible stalls are shown; located in the west parking area, the south parking area and the bus loop drop off parking lot. Accessible routes of travel will need to be ensured to the school facilities.

The circulation for the parking looks fine being off the main entrance shared with the high school. The parking is rather massive and islands and peninsulas will need to be added to the parking lot to further break up the appearance of paving in accordance with the landscaping code. The fact that the parking is broken up into sections is, aesthetically, a nice feature as opposed to having a really large, single parking lot.

During construction of Phase 1(Spring 2014 to Fall 2015), there will be a reduction of on-site parking. The narrative states that there will be no less than 413 parking spaces for the High School, 46 spaces for Tiger Mountain High, and 67 spaces for Clark Elementary. The High School was approved for 595 parking spaces (SDP application PLN08-00047). Large events will need to be coordinated between different days to meet the shared parking needs of the schools. High School students will be requested to carpool or take the bus due to the lack of available parking.

Loading Spaces:

IMC 18.09.110 requires that schools provide one Type A loading space per food service operation. Type A spaces are required to be at least 24 feet in depth and 10 feet in width. A loading dock area is proposed along the north side of the school off of Evans Street. Further details of that area are needed and how it best serves the food service area of the school.

Bicycle Parking:

Bicycle parking is required at the ratio of 5% of the required automobile parking spaces for the first 3200 required auto stalls and 1% of the auto stalls in excess of 300. IMC 18.09.040(1)(3)(c) can require additional spaces for high volume of bicycle activity include uses for schools. That would be expected of the middle school. A minimum of 114 parking spaces is required to serve the middle school; therefore, a minimum of 6 bicycle spaces is required, but that would seem too few for a Middle School. The number of bicycle stalls being provided is unknown at this time.

Bicycle parking is proposed in the following locations: on the south, east and west sides of the new school building. Covered areas for bicycle parking would be appropriate.

Lighting:

The project will need to meet the requirements of the Outdoor Lighting Standards, IMC 18.07.107 with regard to exterior lighting of the parking lot, the plazas and the building. The requirements for Low Density Residential/Multifamily Residential would apply. A lighting plan, meeting the submittal requirements of IMC 18.07.107(D) will need to be submitted. Locations of light standards are shown on the site plan, but no details are yet available.

Landscaping:

A preliminary landscaping plan is shown on sheet L-500. The plan is currently very preliminary, in showing areas of shrub beds, lawn areas, reinforced grass pavement, bioretention areas, restoration planting, green roof planting, and locations for deciduous and coniferous tree plantings. Most of the site will be re-landscaped. Existing trees that will be preserved will need to be identified with the SDP plans. Tree protection measures during the construction periods will need to be clearly shown on the plans. The exact location, type and planting specifications will need to be submitted with the Site Development Permit. The landscaping should compliment that previously done for the High School.

A substantial amount of grading will be done for the new roadway to the Transportation Center and that area should be well planted with native trees and shrubs and minimize hydroseeding/wild flower mixes that generally become weed plantings. This is true for other larger expanse areas as well.

There are 3 parking lots proposed for the school, 2 south and southwest of the building and 1 east of the building. For parking lots equal or greater than 25 stalls which all 3 have, the interior layout requires 25 square feet of landscaping to be provided for every parking space. Interior parking also requires shade trees at the ratio of 1 tree for every 6 parking spaces, a minimum of 10 feet high and 2" caliper when planted. Planting islands with trees are required at the ends of each row and midway between the ends. A span of no more than 10 parking spaces is allowed between planting islands (6 if spaces are double backed).

The landscape plan will need to meet the requirements of the City's Landscape Code, IMC 18.12. Some of these requirements include an evaluation of the worthiness to preserve existing trees and other landscaping on site; a soil analysis and evaluation of the practical use of the existing soil; evaluation of Low Impact Development techniques as already discussed; a water budget analysis and irrigation system design.

Tree preservation:

Section 18.12.1385 (A) of the Landscaping and Tree Preservation code requires with a single family zoning designation, that 30% of the total caliper of all significant trees in developable site area be retained. Section 18.12.1385(B) allows a reduction of up to 50% of the tree retention requirements through a Level 2 Review process (Administrative Adjustment of Standards) with approval meeting specific criteria as shown below.

1. *The modification is consistent with the purpose and intent of this chapter, Chapter 18.12 IMC, Landscaping and Tree Preservation; and*
2. *The modification incorporates the retention of a grouping(s) of smaller trees that make up the equivalent diameter inches and retains other natural vegetation occurring in association with the smaller tree grouping(s); and*
3. *The modification is necessary because the size, shape, topography, or location of the subject property may jeopardize the reasonable use of the property and reasonable alternatives do not exist; and*
4. *The modification is necessary because the required ingress/egress, existing and proposed utility locations, trails, storm drainage improvements or similar constraints may jeopardize the reasonable use of the property and reasonable alternatives do not exist; and*
5. *The modification is necessary to provide solar access to a building that incorporates active solar devices. Windows are solar devices only when they are south-facing and include special storage elements to distribute heat energy.*

The northeast corner of the site is the heaviest forested part of the site and most trees in that area will be removed to accommodate the new softball field and the relocated road to the Sportsmen Club. The area south of Tiger Mountain High is also dense in tree cover that will convert to parking. Trees kept in clusters for retention are recommended over single scattered trees.

A site inventory of significant trees was conducted for the site and was shared with the Pre-Application submittal. The site inventory was organized into 5 zones which include the existing Clark Elementary, Tiger Mt. High and Transportation Center. The 5 zones noted the total caliper of significant trees as follows:

ZONES	TOTAL TREES	TOTAL CALIPER INCHES	CALIPER INCHES REMOVED	CALIPER INCHES RETAINED
Zone 1	429	6,036"	6,036"	0
Zone 2	686	686"	686"	0
Zone 3	140	1,548"	1,188"	360"
Zone 4	375	4,760"	3,846"	914"
Zone 5	399	6,173"	1,787"	4,386"
TOTAL:	2,029	19,203"	13,543"	5,660"

In accordance with Landscaping and Tree Preservation regulations, 5,761" of tree caliper inches are required to be retained (30%) and 5,660" of tree caliper inches are proposed to be retained (29.47%). The project is slightly short by 101 tree caliper inches. Most of the trees to be saved are located within Zone 5 which is located at the Transportation Center area on the western side of 2nd Avenue. The majority of the trees being removed for the new school are due to parking lots and for the various sports fields proposed.

The site plan will either need to be revised to provide further tree retention or an Administrative Adjustment of Standards will need to be applied for and granted to compensate for the slight shortage. The removal of trees will be mitigated with new trees as indicated on the preliminary landscaping plan.

Waste Facilities & Recycling:

The waste and recycling facilities is proposed to be located along the north side of the building off of SE Evans Street. No information for the size of the dumpsters and recycling containers as been provided with the Community Conference application. That area should be designed as well as possible and landscaped well as it has the closest visibility to the residential across the street to the north. The facility will need to be properly screened and approval will be required from CleanScapes.

Signage:

The middle school proposes to modify the existing monument/digital reader board sign to reflect both the High School and Issaquah Middle School. The sign is located at the main entrance off of 2nd Avenue. No other signage is identified at this time.

Fences:

Fences of varied heights will be used around the school site, both for protection along walkways, and around the sports fields. Fence lines are indicated on the site plan, sheet A-001. An Administrative Adjustment of Standards may be needed if taller fences are proposed within the building setbacks. For instance, the maximum height of fences in front yards (the first 10 feet) is limited to 4 feet in height. The heights of fences are currently unknown.

I. Environmental Review:

The Issaquah School District is the SEPA lead agency for the project (as they were for the High School project) and will complete SEPA review of the new middle school. The Development Services Department is requesting to review the environmental checklist, associated reports and the SEPA Determination prior to issuance of a SEPA determination by the School District.

A traffic and parking study should be part of the SEPA review to address and minimize impacts of off-site traffic and parking impacts during construction phasing. Additionally, there may be hazardous materials around the site due to the proximity of the adjacent Issaquah Sportsmen's Club.

See comments below under City's SEPA review.

J. Impact and Mitigation Fees:

The School District will be required to pay Impact and Mitigation Fees for the increase in square footage for the Middle School. Credit is given for school facilities that will be removed. Impact fees are required at the Issuance of Building Permits and by the mitigation/impact fee schedule in effect at that time.

K. City Departments Review Comments:

Fire Department

1. A monitored fire alarm system shall be installed per NFPA, Washington State Accessibility Code and Eastside Fire & Rescue requirements. Submit for a Fire permit.
2. Verify fire department access loop around school. Submit an access route plan to fire department for review and approval.
3. Install 5" Storz Fitting(s) for all new and existing fire hydrants within 300' of structure
4. Fire hydrants to be reviewed and approved by Eastside Fire.
5. Fire flow shall be determined based on type of construction and actual square footage of building. We do not know the construction type at this time. Provide this information to Eastside for review and approval.
6. Structure shall have a fire sprinkler system installed per NFPA and Eastside Fire & Rescue requirements. Submit for Fire permit.
7. Structure may need a Post Indicator Valve (PIV) if fire sprinkler room does not have direct access from outside.
8. Fire Department Connection (FDC) is required. Please verify location to Eastside Fire.

Building Division of Development Services Department:

1. Waste diversion - large projects: A waste diversion plan shall be provided prior to construction or demolition identifying the approximate amount, by weight, of construction and demolition material to be removed from a project site; the hauler; and the receiving facility or location for each commodity. Projects shall divert to recycling a minimum of 50 percent of construction waste and demolition materials removed from the site. Projects shall have at least two C&D bins on site at all times. One bin shall be for commingled recyclable materials and one bin shall be for landfill-bound waste. Materials from the commingled bin shall be sent to a third party certified facility.
2. An asbestos survey must be conducted by an AHERA - certified building inspector and filed with the Puget Sound Clean Air Agency (206-515-2800). 2. A Notice of Intent MUST be filed with the Puget Sound Air Pollution Control Agency prior to any demolition work being done. A copy of the Notice of Intent must be kept on site and be available for review by inspection personnel. 3. All asbestos must be removed prior to demolition.
3. A foundation soils report by a licensed WA state geotechnical engineer will be required at the time of building permit submittal. A peer review of the submitted soils report by a second engineer may be required on some sites.
4. Construction railings are required in accordance with section 3306.4 of the IBC, and must be sufficient to direct pedestrians around construction areas. Additional

pedestrian protection must be provided as specified in section 3306 and table3306.1.
All work subject to field inspection.

Engineering

1. How will the phases of the project be handled to incorporate and/or replace the existing Stormwater Pond and Bioswale?
2. How will the phases of the project incorporate and/or replace the water main loop?
3. It appears the project involves two separate parcels, Parcel # 3434069030 is main large parcel, and parcel to the east is Parcel #3424069120.
4. Existing Well located south of SE Evans St at the east end.
5. Update Handicap Ramps.
6. Fire Hydrants must meet the standard and need a 5" storz adapter.

Parks Department:

1. Parks will want impacts to parking identified that might affect the Community Center and Pool. Parks will want to approve the trail relocation and construction. Parks will want to coordinate with the school district where City property abuts ISD property.

Police:

1. Parking is going to be an issue with the upper parking lot being removed. What is the plan and count of existing parking vs. available parking during construction?
2. Security of schools is a heightened consideration. All aspects of building security and features should be considered to include physical security, building access and egress, as well as surveillance and "panic type" alarm or systems.
3. The public safety radio system is due for replacement in the near future. This will be an entirely new system to what is currently in use (800 Mhz vs. digital). Inquiry should be made to the regional radio system planners for what will be required for public safety radios to work within the new facilities.

SEPA

1. SEPA review is required for construction of schools over 4,000 SF, per WAC 197-11-800(1)(b)(iii).
2. Lead Agency - The City and School District need to determine Lead Agency status, who is responsible for SEPA compliance. In the past, the School District has been Lead Agency (ex: Issaquah High School), because the School District initiated the proposal. If the School District wants to assume Lead Agency status for this proposal, the City will want an agreement that the SEPA Determination is not issued until the City has had a chance to review and approve the SEPA analysis of impacts and mitigation measures before issuance.
3. This approach is preferable to the City having to formally comment on the SEPA Determination after it's issued.

4. The School District should involve the City in scoping the SEPA evaluation, particularly the transportation analysis, so we're in concurrence on assumptions, methodologies, etc. before the analysis is completed.
5. Critical Areas - Middle School construction to the north of the existing High School fields would be proximate to steep slopes of lower Tiger Mountain according to the City's GIS mapping. There are also mapped landslide hazard areas. Critical area steep slopes are slopes greater than 40% with a minimum 20-foot elevation change. We will need more site-specific topography to determine how close proposed construction is to steep slope areas. A 50-foot buffer and 15-foot building setback is required from 40% or greater steep slopes. The buffer may be reduced to 10-feet with a geotech report.
6. The Transportation Center on the west side of 2nd Ave is adjacent to wetland areas. Will there be any expansion of the Center; expanding buildings, parking or the existing impervious surface? Expansion could encroach into wetlands or wetland buffer areas.

L. Public Notification:

As part of the Community Conference process, public notice is required to be provided to all property owners within 300 feet of the exterior boundaries of the proposal site at least 10 days prior to the meeting. Notice of the project and the Community Conference meeting was mailed out to 238 residents on July 23, 2013.

Notice was mailed to the applicant team members on August 22, 2013.

Notice of the Community Conference Meeting was published in the Issaquah Press on July 24, 2013.

Notice of the Community Conference Meeting was posted on the City's web site calendar.

M. Exhibit List:

1. Application, COM13-00002, received 6-12-2013
2. Vicinity Map.
3. Project Narrative, received 6-12-2013
4. Approval Criteria for Non-utility Community Facilities, IMC18.07.480-E
5. Approval Criteria for Master Site Plans, IMC 18.07.660 (4 pages)
6. Nonmotorized Facilities Standards, IMC 18.07.080.
7. Aerial photo of school property site (year 2012)
8. Aerial photos of site with phased development imposed over aerial (6 sheets)
9. Cover Sheet (with colored drawing of front entrance)
10. Site Plan/Overall Zoning Summary, sheet G-003
11. Site Plan/Accessibility Plan, sheet G-006
12. Paving & Horizontal Control Plan, sheet C-400
13. Grading & Drainage Plan, sheet C-500
14. Site Sections, sheet C-520
15. Planting Plan, sheet L-500
16. Colored elevations of Middle School & Covered play building
17. Topographic Survey, sheet 1 of 10

18. Topographic Survey, sheet 2 of 10
19. Topographic Survey, sheet 3 of 10
20. Topographic Survey, sheet 4 of 10
21. Topographic Survey, sheet 5 of 10
22. Topographic Survey, sheet 6 of 10
23. Topographic Survey, sheet 7 of 10
24. Topographic Survey, sheet 8 of 10
25. Topographic Survey, sheet 9 of 10
26. Topographic Survey, sheet 10 of 10

JL/jl

COM13-00002, Issaquah Middle School Comm Conf staff report 8-7-13

LAND USE PERMIT APPLICATION



CITY OF
ISSAQUAH
DEVELOPMENT SERVICES

1775 - 12th Ave. NW | P.O. Box 1307
Issaquah, WA 98027
425-837-3100
issaquahwa.gov

This Section For Staff Use Only	
Permit Number: <u>COM13-00002</u>	Date Received: <u>RECEIVED</u>
Staff Contact: <u>JERRY LIND</u>	<u>JUN 12 2013</u>

City of Issaquah

Type of Application: Community Conference

PROJECT INFORMATION

Name of Project (if applicable): Issaquah Middle School

Project Site Address: 500 Second Avenue ~~NE~~ SE

Parcel Number: Parcel A (3424069030), Parcel B (3424069120) & Parcel C (3424069185)

OWNER

Name: Issaquah School District, Steve Crawford

Address: 565 NW Holly Street, Issaquah, Washington 98027

Phone: 425.837.7040 Email: crawfords@issaquah.wednet.edu

APPLICANT

Name: Mahlum Architects - Gregg Stewart

Address: 71 Columbia Street, Floor 4, Seattle, Washington 98104

Phone: 206.441.4151 Email: gstewart@mahlum.com

CONTACT

Name: Gregg Stewart + DAVID MOUNT

Address: 71 Columbia Street, Floor 4, Seattle, Washington 98104

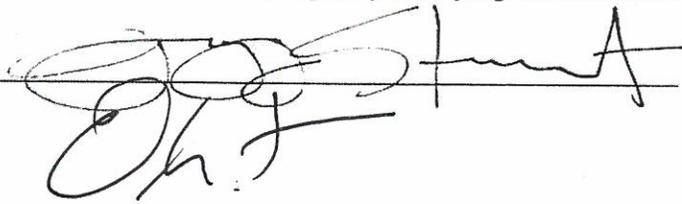
Phone: 206.441.4151 Email: gstewart@mahlum.com

PROPOSED PROJECT DESCRIPTION

Please provide a brief description of the project. (Use an additional sheet of paper, if necessary.)

See attached.

I certify (or declare) under penalty of perjury under the laws of the State of Washington that all application information, including plans and reports, are true and complete to the best of my knowledge. I understand the lead agency is relying on them to make its decision.

Signature: 

Date: 6-4-13

Exhibit 1

PROJECT SITE INFORMATION

Legal Description: (Use an additional sheet of paper, if necessary.)
See attached.

Zoning Designation: CF-F Community Facilities-Facilities

Land Use Designation: Community Facilities

Subarea Designation: Olde Town

Shoreline Designation, if applicable: _____

Existing Land Use: CF-F

Adjacent Land Uses North: SF-SL, SF-D

South: SF-S

East: CF-OS

West: MF-H

Acreage in square feet: 2,660,073 SF

Does the site contain any of the following environmentally critical areas? Check all that apply.

- Flood Hazard Area
- Streams
- Steep Slope Hazard Area
- Landslide Hazard Area
- Wetlands
- Coal Mine Hazard Area

PROPOSED DEVELOPMENT STATISTICS

Proposed Land Use: CF-F

Density (multifamily only): N.A.

Impervious Surface Ratio: 39%

Pervious/Landscaping/Open Space Provided (in square feet): 1,622,000

Maximum Proposed Building or Structure Height: 43'-9"

Total Proposed Building Square Footage (Gross Area): 126,000

Proposed Setbacks Front: 20 FT (per SF-S)

20 FT (per SF-

Rear: SL)

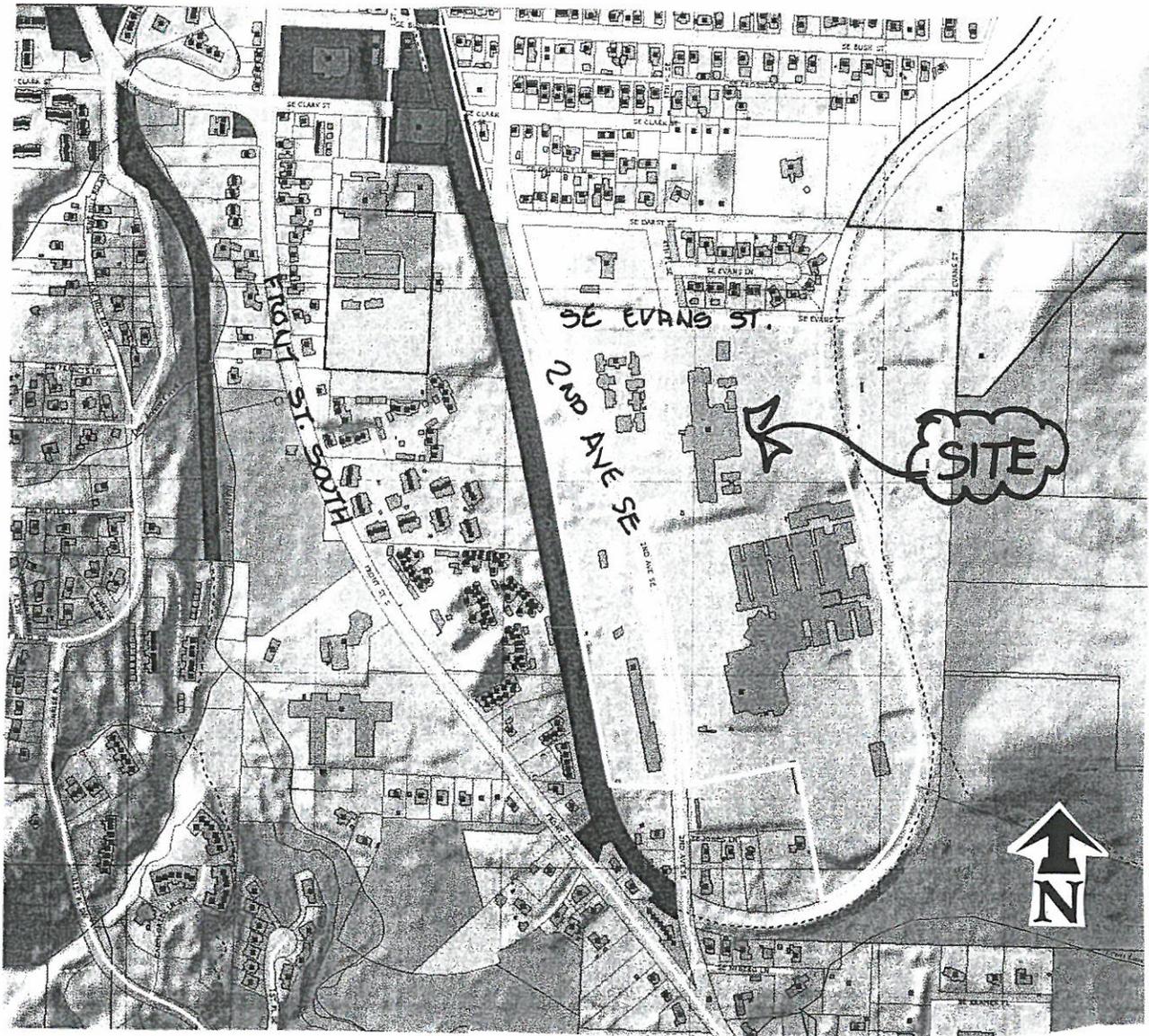
Side: 8 FT (per SF-S)

Parking Spaces Provided: 142

VICINITY MAP

Issaquah Middle School

500 2nd Avenue SE, Issaquah, WA



mahlum

MEMORANDUM

RECEIVED

JUN 12 2013

12 June 2013

City of Issaquah

To: Christopher Wright, *City of Issaquah*
 From: Gregg Stewart AIA, *Mahlum*

Subject: **Combined Master Site Plan | Site Development Permit
 Community Conference Project Narrative**

Owner: **Issaquah School District**
 Project: **Issaquah Middle School**

BUILDING DEVELOPMENT

General Description

The project will convert the existing campuses of Tiger Mountain and Clark Elementary into the Issaquah Middle School; while modernizing the existing Issaquah Middle School in order to relocate Tiger Mountain and Clark Elementary to the middle school site. The new Issaquah Middle School will include one and two story sections and provide approximately 126,000 square feet of academic, athletic and performing arts space on a 63 acre site. Site development will impact approximately 33.6 acres and include reconfigured parking for middle school staff and visitors; relocated high school parking; a middle school bus loop; middle school softball field and track and field, relocated high school playfields and a new driveway for the Transportation Center. The site development will accommodate (4) double portables for future growth, drainage design will accommodate the additional impervious area. The existing high school grandstands, softball field and track will not be altered. The project will be executed in a phased construction process in order to keep all students on site throughout the construction of the new facility.

The comprehensive school program includes core academic instruction, special education, performing arts, fine arts, media, physical education, a library and a commons. The facility will be designed to serve an enrollment of 900 students and will have 38 teaching stations.

SITE DEVELOPMENT

General Description

The Issaquah Middle School project site is currently occupied by Clark Elementary School and Tiger Mountain Community High School and associated parking, Issaquah High School (IHS) parking and IHS softball fields. The property is surrounded by a residential neighborhood to the north, IHS to the south, the Issaquah Sportsmen's Club and undeveloped wooded property to the east and the School District transportation facility to the west. The project construction limits will include approximately 33.6 acres. The complete site (Parcels A, B and C) inclusive of Issaquah High School, Clark Elementary school and Tiger Mountain Community High School is approximately 63 acres in size. The new middle school will be

constructed over the existing Clark Elementary play field and a portion of the existing IHS parking area, therefore, a new high school parking area will be provided to replace lost parking stalls. Play fields at Clark Elementary school will be limited during construction of the new middle school. The project also includes a softball sport field and a synthetic track and field for the middle school. In addition, a new sport field for the High School program will be included with the improvements. The building will be designated as Construction Type IIB and will be sprinkled in accordance with NFPA 13 where required. A material/color palette complementary to the high school will be proposed for the middle school.

IHS and the associated stadium are not part of the redevelopment and will not be modified for this project. The access to one of the high school parking lots (to the south of the main access drive) will be modified slightly to allow the main access drive to function more efficiently with the new middle school, otherwise the high school parking to the south of the main access drive will not be modified for this project. An accessible route will be provided between the new IMS and IHS, as well as an accessible route between all site features and amenities on the IMS site. All ramps will be accessible.

The existing driveway location serving Clark Elementary off of Second Avenue SE will be removed. The existing driveway locations serving Clark Elementary and Tiger Mountain Community High School along SE Evans Street will also be removed. There will be a new curb cut along SE Evans Street for maintenance access to the west fields. Four two-classroom portables for the middle school will be located near SE Evans St directly west of the new building. The majority of vehicular access to the project site will be via Second Avenue SE at the intersection controlled by a traffic signal installed when IHS was replaced, including the new high school parking lot (to replace the one being demolished), the middle school staff/visitor lot and middle school pick-up/drop-off. Buses will enter and exit via SE Evans Street and some staff parking will be accessed via SE Evans Street. Power poles for overhead electrical lines within the new middle school bus loop will be relocated as A-001 indicates. The corresponding easement with PSE will be adjusted accordingly. Overhead power lines along 2nd Avenue SE will be relocated underground, as was completed in front of the high school. The existing gravel access road and easement, as well as the electrical service lines, for the Issaquah Sportsmen's Club will be re-configured to the north to accommodate the middle school softball field. The re-configured road will remain gravel.

Fire access will be provided around the proposed school. A grass pave (reinforced grass pavement) pedestrian promenade will double as a fire access drive along the west side of the school and will connect to the fire lane at the middle school parking lot for access to the south side of the building. Fire lane access to the east of the school will be provided via the bus loop. And fire lane access to the north of the school will be provided via the service entrance. The configuration and development of access to the courtyard will be reviewed with the Fire Marshall as part of the Master Site Plan process. Sewer service will be extended south from the existing 8-inch sewer main located in the Campus Access Drive (on Parcel A). A new 8-inch water main will be looped around the school to serve fire hydrants, service to the fire sprinkler system, a domestic service connection and an irrigation connection.

Facility waste and recycling enclosures of approximate sizes will be provided on the north side of the building and will be screened from the road. This space will also have a loading area meeting space and maneuvering requirements outlined in IMC 18.09.110.

Mechanical screening elements at the building and the Gymnasium exceed the 30' site height limit. Maximum heights are indicated on the A-200 series building elevation sheets.

Transportation Center Improvements:

A new driveway to the Issaquah School District Transportation Center will be installed at the signalized intersection. This will form the new entry and exit for buses. The new access will allow the south-bound left turn lane onto the High School to be lengthened, increasing the signalized queuing area.

Signage

Modify existing monument/digital reader board sign to reflect both Issaquah High School and Issaquah Middle School.

Fencing

All general fencing will be coated black vinyl chain link and be of various heights as indicated below:

Fencing at property boundaries	6'-0"
Fencing at softball fields perimeter	8'-0"
Backstops at softball fields	30'-0"
Fencing at track and field	8'-0"

Existing fencing installed at Issaquah High School to remain unless noted otherwise.

There will also be a limited amount of ornamental fencing at courtyard.

During construction a 42" high railing will be provided to direct pedestrians around construction areas.

Site Demolition

The project construction limits will include approximately 33.6 acres of the District property north of the high school. Demolition within the construction limits will reshape the entire property. The project will include the removal of all existing school buildings in phased demolition throughout the duration of the project. Existing utilities within limits of the building excavation will be re-routed or removed and capped at the edge of excavation. Safety barriers including an 8'-0" tall plywood fence will be provided between Clark Elementary School and the new middle school during demolition and construction.

Temporary Erosion and Sedimentation Control

All temporary erosion and sedimentation control requirements will be in compliance with the City of Issaquah and King County Best Management Practices (BMPs). The Temporary Erosion and Sedimentation Control plan for the proposed site has been designed to protect off-site properties as well as minimize the quantity of sediment-laden water from entering the public storm system.

Best Management Practices (BMP's) will include, but are not limited to, temporary catch basin filters on existing catch basins and silt fence to prevent sediment laden runoff from exiting the construction site. The contractor will also be required to lay polyethylene sheeting over disturbed surfaces that will remain un-worked. The erosion control measures will be in accordance with Washington State Department of Ecology standards to prevent erosion or sedimentation problems downstream during construction. The project will also require that NPDES regulations be met, including turbidity monitoring and possibly pH monitoring.

Site Grading and Excavation

Due to the topography of the existing site, grading limitations within easements, and the full program requirements for the new middle school extensive grading will be required to terrace the site into a cohesive, functioning campus. Multiple retaining walls will be required to drop in elevation from the high elevation on the east side of the site to the lower level area near Second Avenue SE.

To minimize associated earthwork costs, efforts have been made to reduce offsite import and export. The elevations of site features were selected in order to balance cut and fill volumes and work with the construction phasing.

To accommodate this, all significant grading activities should occur during dry weather (summer months) to reduce unsuitable soil caused by rainwater saturation.

Based on past experience with the High School development the native soil contains large amounts of cobble which should be sorted out prior to use as structural fill material under buildings or in top layers of landscape areas (planter beds).

Geotechnical soil testing and exploration has revealed the play area where the Middle School Building will be located has soil generally not suitable to support structural footing loads to varying depths up to 18-foot deep. In lieu of over-excavating unsuitable material the soil will be amended with a soil aggregate pier system in order to save cost and minimize disturbance associated with heavy earth moving activities.

Stormwater Design:

All runoff from the site will be infiltrated onsite.

Roof runoff will be collected in downspouts and conveyed to underground infiltration trenches, mimicking the current site system. It is anticipated that the soils will have sufficient infiltration capacity to infiltrate field runoff as well.

Water quality requirements will be met through the use of rain garden filtration for all runoff from all driving surfaces (parking and roads).

Water Distribution

The Issaquah Public Works Department maintains water service, a 12" diameter water main, within SE Evans Street and 2nd Avenue SE. A new 8" diameter water loop will be installed around the Middle School building to provide service for fire hydrants, domestic service, irrigation, and fire service. The water loop construction will need to be phased with a majority of the loop and fire hydrants installed during Phase 1 as needed for fire safety and the loop completion occurring in Phase 3 after Clark Elementary School is demolished. Fire service (FDC, 6" Fire Line, and 6" backflow device in vault) for the Clark Elementary School will be removed during the Middle School construction and will need to be temporarily relocated until Clark Elementary demolition.

Sanitary Sewer

The Issaquah Public Works Department maintains sewer service within SE Evans Street and 2nd Avenue SE. Sewer service will be extended south from the existing 8-inch sewer main located in the Campus Access Drive (on Parcel A). The grease interceptor serving the kitchen area will be located on the east side of the building. One additional sewer service will feed the gym locker room from the south which will connect into the sewer main located in the north High School drive which currently serves the High School building.

CONSTRUCTION PHASING

General Description

Construction is anticipated to occur in three phases, over two years from spring 2014 through 2016. Construction phasing will be controlled by the requirement to fully occupy the Tiger Mountain Community High School, Clark Elementary School, and the new Issaquah Middle School on one single site while a modernization of the existing Middle School site takes place in preparation for the Tiger Mountain and Clark Elementary students to move in.

To achieve this, the new Middle School building will be constructed east of the Clark Elementary building, over the existing grass field and extending onto portions of the High School parking area and associated infiltration pond. The impacts of the Middle School building on the existing infrastructure will require replacement of the High School parking area.

Interim fire protection, including temporary fire hydrants and temporary dry fire sprinkler system if needed, as well as a 20'-0" clear fire access lane will be maintained throughout the construction process.

The dates indicated herein are approximate only and will be refined upon selection of a General Contractor. Phasing diagrams included as part of this application graphically illustrate an overall summary, as well as three primary phases of construction to align with the project schedule:

Phase 01: New Construction (Spring 2014 through Fall 2015)

- Demolish existing (high school) parking lot.
- Construct approximately 126,000 square feet of new facility.
- Construct middle school softball field, track and field, parking area (middle school only) and bus loop.
(Note the middle school parking area to the south of the building overlaps with a portion of the Clark Elementary building; therefore, it will be built out to the maximum extent feasible.)
- Re-locate Issaquah Sportsmen's Club access road to the north to accommodate the middle school softball field.
- Construction of a new entrance at the Issaquah School District Transportation Center, located west of 2nd Avenue SE. The new entrance will be located at the signalized intersection, located on 2nd Avenue SE at the northern Issaquah High School entrance.
- Construct all flatwork within construction zone boundary around building.
- Services:
Work on underground site utilities such as water, sewer, gas, power, and communications will begin. It is anticipated that only a portion of the water loop will be completed during Phase 01. Fire service (FDC, 6" Fire Line, and 6" backflow device in vault) for the Clark Elementary School will be removed during the Middle School construction and may need to be relocated until Clark Elementary demolition. Maximum hose distances for fire protection will be maintained throughout construction. The gas service connection will need to be determined by Puget Sound Energy based on mechanical demand calculations but it is anticipated that a new connection will be made with the 4" gas main in 2nd Avenue SE and routed along the north side of the site with the power and communications lines. Irrigation connection,

backflow, pump will be installed for Phase 1 areas and stubbed out for extension in Phase 3.

- Occupy new middle school.
Temporary construction laydown/staging area (to the south of the middle school) to be converted to middle school parking for Fall 2015.

Phase 02: Demolition/New Construction/Modernization

(Summer 2015 through August 2016)

Project site: Existing Middle School at 400 2nd Avenue Southeast, Issaquah, WA 98027

Building Permit for phase 02 will be under separate submittal process.

- Demolish portions of existing buildings (approximately 36,500sf), remove flatwork and grub landscaping.
- Construct approximately 50,000 square feet of addition for Clark Elementary.
- Major tenant improvements to a portion of the existing facility for Tiger Mountain Community High School and Clark Elementary School occupancy (approximately 53,750sf)
- Construct all flatwork within construction zone boundary around building.

Phase 03A/B: Demolition/Site work

(Summer 2016 through December 2016)

- Demolish Clark Elementary and Tiger Mountain Community High School buildings.
- Demolish existing parking lots, associated flatwork and landscaping related to Clark Elementary and Tiger Mountain Community High School.
- Remove temporary classroom portables and platforms.
- Construct all flatwork within construction zone boundary around building.
- Complete construction of middle school parking and pick-up/drop-off loop (to the south of the middle school).
- Construct high school parking on the west side of the project site.
- Construct high school play fields on the west side of the campus.

For more information regarding scheduled phasing, please refer to the attached tentative construction schedule.

Restrictions

During the course of phase 01 construction, a reduction in parking will necessitate that Issaquah High School, Clark Elementary and Tiger Mountain Community High School coordinate schedules so that large events are held on different days so that parking may be shared between the schools. No less than 413 parking spaces for Issaquah High School, 46 parking spaces for Tiger Mountain Community High School and 67 parking spaces for Clark Elementary will be available during phase 01 construction.

During phase 02 when Issaquah Middle School, Clark Elementary School and Tiger Mountain Community High School will occupy the site the following parking will be available:

- Issaquah High School: 413 parking spaces
- Tiger Mountain Community High School: 46 parking spaces
- Clark Elementary School: 67 parking spaces
- Issaquah Middle School: 142 parking spaces

Issaquah High School parking on the middle school site will be maintained after phase 02 of construction is completed. The minimum parking requirements of 3 spaces per classroom in the new middle school will be met.

Traffic Management

Construction fencing will demark the threshold of areas for academics purposes and for project construction.

MASTER SITE PLAN APPLICATION REQUIREMENTS

18.07.590 - Master Site Plans

The proposed development exceeds 15 acres in size and thus requires a Level 5 Review process.

18.07.640 - Application for a Master Site Plan

All materials accompanying this narrative meet the requirements set forth by the Master Site Plan review application requirements.

18.07.650 - Master Site Plan contents

- A. Area Description and Maps:
Please refer to the attached King County Parcel Assessment Maps and site plans included with this application submittal.
- B. Property Description and Maps:
Please refer to the attached site plans included with this application submittal.
- C. Subdivision Plan:
NOT APPLICABLE.
- D. Property Improvements Plan:
Please refer to the attached site plans included with this application submittal.
- E. Phasing:
Please refer to the attached phasing diagrams and schedule included with this application submittal.

18.07.660 – Approval Criteria

- A. Other Permit Approvals:
As required per the City of Issaquah, a Site Development Permit application is being provided concurrently with the Master Site Plan review application in this submittal.

B. Clearing/Grading Permit:

In the interest of meeting project milestones, an application for a clearing/grading permit will be submitted. Prerequisite criteria prior to this permit are noted as per 18.07.660(B). (Demolition Permit will be under a separate submittal).

LEGAL DESCRIPTION

PARCEL A:

THAT PART OF THE SOUTH HALF OF THE NORTHWEST QUARTER AND THE NORTH HALF OF THE SOUTHWEST QUARTER OF SECTION 34, TOWNSHIP 24 NORTH, RANGE 6 EAST, W.M., IN KING COUNTY, WASHINGTON, BOUNDED ON THE NORTH BY THE NORTH LINE OF THE SOUTH HALF OF THE NORTHWEST QUARTER OF SAID SECTION 34 AND ON THE SOUTH, EAST AND WEST BY THE RIGHT-OF-WAY OF THE NORTHERN PACIFIC RAILWAY COMPANY'S NORTH BEND BRANCH; LESS COUNTY ROAD; AND EXCEPT THE FOLLOWING PORTIONS THEREOF: BEGINNING AT A POINT WHICH, WHEN MEASURED ALONG THE SECTION AND SUBDIVISION LINES, IS 3,944.12 FEET NORTH AND 1,477.04 FEET EAST OF THE SOUTHWEST CORNER OF AFORESAID SECTION 34, SAID POINT BEING ON A LINE PARALLEL WITH AND 30 FEET SOUTH OF THE NORTH LINE OF THE SOUTH HALF OF THE NORTHWEST QUARTER OF AFORESAID SECTION 34; THENCE NORTH 88°35'08" WEST ALONG SAID PARALLEL LINE 610 FEET, MORE OR LESS, TO THE EAST LINE OF THE RIGHT-OF-WAY OF THE NORTH BEND BRANCH OF THE NORTHERN PACIFIC RAILWAY; THENCE SOUTHERLY ALONG SAID LINE OF RIGHT-OF-WAY 304.80 FEET; THENCE SOUTH 88°35'08" EAST 598 FEET; THENCE NORTH 12°21' WEST 299.60 FEET TO THE PLACE OF BEGINNING; LESS COUNTY ROAD;

ALSO, BEGINNING AT A POINT WHICH, MEASURED ALONG THE SECTION AND SUBDIVISION LINES IS 3,944.12 FEET NORTH AND 1,518.24 FEET EAST OF THE SOUTHWEST CORNER OF AFORESAID SECTION 34, SAID POINT BEING ON A LINE PARALLEL WITH AND 30 FEET SOUTH OF THE NORTH LINE OF THE SOUTH HALF OF THE NORTHWEST QUARTER OF AFORESAID SECTION 34; THENCE EAST ALONG SAID PARALLEL LINE 650 FEET, MORE OR LESS, TO THE WEST LINE OF THE RIGHT-OF-WAY OF THE NORTH BEND BRANCH OF THE NORTHERN PACIFIC RAILWAY; THENCE SOUTHERLY ALONG SAID LINE OF RIGHT-OF-WAY 300 FEET, MORE OR LESS; THENCE WEST 530 FEET, MORE OR LESS; THENCE NORTH 12°21' WEST 299.60 FEET TO THE PLACE OF BEGINNING;

ALSO, BEGINNING AT A POINT WHICH, MEASURED ALONG THE SECTION AND SUBDIVISION LINES, IS 1,985.24 FEET NORTH AND 2,098.95 FEET EAST OF THE SOUTHWEST CORNER OF AFORESAID SECTION 34; THENCE SOUTH 12°21' EAST 510 FEET, MORE OR LESS, TO THE NORTH LINE OF THE RIGHT-OF-WAY OF THE NORTH BEND BRANCH OF THE NORTHERN PACIFIC RAILWAY; THENCE WESTERLY ALONG SAID LINE OF RIGHT-OF-WAY ON A CURVE TO THE RIGHT 490 FEET, MORE OR LESS, TO A LINE PARALLEL WITH AND 30 FEET EAST OF THE CENTER LINE OF COUNTY ROAD; THENCE NORTHERLY ALONG SAID PARALLEL LINE 395 FEET, MORE OR LESS; THENCE NORTH 77°39' EAST 416 FEET, MORE OR LESS, TO THE PLACE OF BEGINNING;

ALSO, BEGINNING AT A POINT ON THE EAST LINE OF THE RIGHT-OF-WAY OF THE NORTH BEND BRANCH OF THE NORTHERN PACIFIC RAILWAY WHICH IS 1,875.60 FEET NORTH AND 1,398.72 FEET EAST OF THE SOUTHWEST CORNER OF AFORESAID SECTION 34; THENCE SOUTH 88°35' EAST 232 FEET, MORE OR LESS, TO THE WEST LINE OF COUNTY ROAD; THENCE SOUTHEASTERLY ALONG SAID LINE OR RIGHT-OF-WAY OF COUNTY ROAD, 355 FEET, MORE OR LESS, TO THE SAID LINE OF RIGHT-OF-WAY OF NORTHERN PACIFIC RAILWAY; THENCE NORTHWESTERLY ALONG SAID RAILWAY RIGHT-OF-WAY 450 FEET, MORE OR LESS, TO THE PLACE OF BEGINNING, CONTAINING 43 ACRES, MORE OR LESS;

ALSO EXCEPT A STRIP OF LAND ALONG THE NORTH SIDE OF THE PROPERTY CONVEYED TO THE CITY OF ISSAQUAH FOR S.E. EVANS STREET BY DEED RECORDED UNDER KING COUNTY RECORDING NUMBER 7701100213; ALSO EXCEPT THAT PORTION LYING WEST OF 2ND AVE. S.E. (BUS TRANSPORTATION CENTER); LESS THAN PORTION DEDICATED FOR ROAD RECORD NO. 20130117001692 & RECORD NO. 20130117001693.

PARCEL B:

ALL OF THE SOUTHEAST QUARTER OF THE NORTHWEST QUARTER AND THE NORTHEAST QUARTER OF THE SOUTHWEST QUARTER OF SECTION 34, TOWNSHIP 24 NORTH, RANGE 6 EAST, W.M., IN KING COUNTY, WASHINGTON, LYING EASTERLY OF THE EASTERLY LINE OF THE RIGHT-OF-WAY OF THE NORTH BEND BRANCH OF THE NORTHERN PACIFIC RAILROAD; TOGETHER WITH ALL THAT PORTION OF THAT CERTAIN 100 FOOT RIGHT-OF-WAY IN SECTION 34, TOWNSHIP 24 NORTH, RANGE 6 EAST, W.M., IN KING COUNTY, WASHINGTON, CONVEYED FROM BURLINGTON NORTHERN INC. TO PUGET SOUND POWER AND LIGHT COMPANY BY QUIT CLAIM DEED RECORDED UNDER KING COUNTY RECORDING NUMBER 7607220547, LYING SOUTH OF THE NORTH LINE OF THE SOUTHEAST QUARTER OF THE NORTHWEST QUARTER OF SAID SECTION 34 AND NORTH OF A LINE WHICH IS RADIAL TO THE CURVE AT THE SOUTHERLY MOST POINT WHERE THE EAST LINE OF THE NORTHEAST QUARTER OF THE SOUTHWEST QUARTER OF SAID SECTION 34 INTERSECTS THE EASTERLY LINE OF SAID 100 FOOT RIGHT-OF-WAY, SAID POINT BEARS NORTH 01°57'55" EAST A DISTANCE OF 1,870.27 FEET FROM THE SOUTH QUARTER CORNER OF SAID SECTION 34 AS MEASURED ALONG THE EAST LINE OF SAID SUBDIVISION; (ALSO KNOWN AS PARCEL B OF CITY OF ISSAQUAH LOT LINE ADJUSTMENT NO. 90-01, RECORDED UNDER RECORDING NUMBER 9004050882).

PARCEL C:

THAT PORTION OF THE SOUTHWEST QUARTER OF SECTION 34, TOWNSHIP 24 NORTH, RANGE 6 EAST, W.M., IN KING COUNTY, WASHINGTON, DESCRIBED AS FOLLOWS: BEGINNING AT POINT WHICH WHEN MEASURED ALONG THE SECTION AND SUBDIVISION LINES IS 1,985.24 FEET NORTH AND 2,098.95 FEET EAST OF THE SOUTHWEST CORNER OF SAID SECTION; THENCE SOUTH 77°39' WEST 177.28 FEET TO THE TRUE POINT OF BEGINNING; THENCE SOUTH 08°43' EAST 199.24 FEET; THENCE SOUTH 85°31' WEST 255.24 FEET, MORE OR LESS, TO THE EASTERLY MARGIN OF COUNTY ROAD; THENCE NORTHERLY ALONG SAID ROAD MARGIN 165.80 FEET, MORE OR LESS, TO A POINT THAT BEARS SOUTH 77°39' WEST FROM THE TRUE POINT OF BEGINNING; THENCE NORTH 77°39' EAST 240.53 FEET, MORE OR LESS, TO THE TRUE POINT OF BEGINNING;

ALSO, THAT PORTION OF THE SOUTHWEST QUARTER OF SECTION 34, TOWNSHIP 24 NORTH, RANGE 6 EAST, W.M., IN KING COUNTY, WASHINGTON, DESCRIBED AS FOLLOWS: BEGINNING AT A POINT WHICH WHEN MEASURED ALONG THE SECTION AND SUBDIVISION LINE IS 1,985.24 FEET NORTH AND 2,098.95 FEET EAST OF THE SOUTHWEST CORNER OF SAID SECTION; THENCE SOUTH 77°39' WEST 177.28 FEET; THENCE SOUTH 8°43' EAST 214.28 FEET; THENCE NORTH 89°31' EAST 192.67 FEET; THENCE NORTH 12°21' WEST 240.22 FEET TO THE POINT OF BEGINNING; TOGETHER WITH AN EASEMENT FOR ROAD PURPOSES OVER A STRIP OF LAND 30 FEET IN WIDTH DESCRIBED AS FOLLOWS: THAT PORTION OF THE SOUTHWEST QUARTER OF SECTION 34, TOWNSHIP 24 NORTH, RANGE 6 EAST, W.M., IN KING COUNTY, WASHINGTON, DESCRIBED AS FOLLOWS: BEGINNING AT A POINT WHICH WHEN MEASURED ALONG THE SECTION AND THE SUBDIVISION LINE IS 1,985.24 FEET NORTH AND 2,098.95 FEET EAST OF THE SOUTHWEST CORNER OF SAID SECTION; THENCE SOUTH 77°39' WEST 177.28 FEET; THENCE SOUTH 08°43' EAST 214.28 FEET TO THE TRUE POINT OF BEGINNING; THENCE NORTH 08°43' WEST 15.04 FEET; THENCE SOUTH 85°31' WEST 255.24 FEET, MORE OR LESS, TO THE EASTERLY MARGIN OF COUNTY ROAD; THENCE SOUTHERLY ALONG SAID EASTERLY MARGIN 30 FEET;

THENCE NORTH 85°31' EAST 257.87 FEET, MORE OR LESS; THENCE NORTH 8°43' WEST 15.04 FEET TO THE TRUE POINT OF BEGINNING; LESS THAN PORTION DEDICATED FOR ROAD RECORD NO. 20130117001693.

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IMC 18.07.480-E Community Facility Standards

E. Approval Criteria, Nonutility Community Facilities: Development, including retail/commercial uses, is permitted in community facilities only when all of the following criteria are met and as permitted in the Table of Permitted Land Uses (IMC 18.06.130). Accessory retail/commercial uses are permitted as allowed in the accessory use criteria in the Land Use Code.

1. Architectural Format and Character: Community facility buildings and structures must be compatible with the architectural form of surrounding buildings. Community facility buildings and structures must meet the applicable sections of the Design Criteria Checklist (Chapter 18.07 IMC, Appendix 2);
2. Development Standards: All buildings and structures shall conform to development standards including setback, height, and impervious surface of the most restrictive contiguous zoning district as established in the District Standards Table (IMC 18.07.360).
 - a. Exception – Downtown City Facilities Site: The Downtown City Facilities Site, on property that is City-owned, shall meet the development standards of the CBD zoning category. The Downtown City Facilities Site includes, but is not limited to, City property currently known as: Centennial Park, Rainier Blvd. Greenway, Depot Park, Memorial Park, King County Library (10 W. Sunset Way), Police Department (130 E. Sunset Way), Fire Department (190 E. Sunset Way), City Hall South and adjoining parking lots (135 E. Sunset Way), Lewis House (115 SE Bush St.), Food and Clothing Bank and Historical Society (179 1st Ave. SE), Parks Dept. Offices (235 1st Ave. SE), Trailhead Park and Stationmaster's House (110 SE Bush St.), Community Center (500 1st Ave. SE), Greenway loop/trail, and Julius Boehm Pool (50 SE Clark St.), as well as contiguous City-owned property;
3. Access: Existing or proposed motorized and nonmotorized access to facilities, including barrier-free, pedestrian and bike, are provided and identified;
4. Environmental Impacts: The existing natural environment of the area shall be identified, along with impacts of the proposed facility upon the natural environment, and required mitigation shall also be identified;
5. Linkage to Community Facilities: Linkage, if any, by pedestrian and/or bike trail to other community facilities is provided and identified;
6. Maintenance: Long-term maintenance requirements are identified, funding options are noted, and a long-term maintenance program is provided;
7. Phasing: Phasing, if any, of the construction of the facility is identified;
8. Safety: The safety of all users is ensured through the use of posted regulations and user directions, adequate lighting, marked access points and other methods;
9. Users: The potential users and general percentage of community that will benefit from the facility are identified, with potential conflict among user groups minimized;
10. Waste/Recycling: Waste/recycling receptacles are provided and identified;
11. Parking/Traffic: Provision for adequate on-site parking, with impacts of the proposed facility upon neighborhood traffic, and required mitigation shall also be identified;
12. Signs: All signs shall be kept to a minimum size which is compatible with the surrounding neighborhood and uses, while providing adequate visibility;
13. Site: For new facilities, it has been established that alternative sites have been considered and it has been established that the proposed site is best suited for the development;
14. Nonconforming Situations: Existing, legally nonconforming situations may be expanded as allowed (IMC 18.08.085, Expansion or reconstruction of nonconforming situations within the Community Facilities Recreation and Community Facilities – Facilities zones); if the aforementioned criteria of this section and IMC 18.08.060 are met, and it is determined that the public interest will best be served by an expansion of the nonconforming situation.

- G. Additional Information: Such additional and/or more detailed information as may be required by the City in order to evaluate the proposal in relation to the approval criteria. (Ord. 2447 § 50, 2005).

18.07.660 Approval criteria.

The following approval criteria apply to all Master Site Plan proposals:

- A. Other Permit Approvals: The Master Site Plan shall be approved prior to, or in conjunction with, all other development review procedures or permits as required by the City, including but not limited to the following: subdivisions, short subdivisions, Site Development Permits, Clearing/Grading Permits, Shoreline Substantial Development Permits, variance requests, and zone reclassifications.
- B. Clearing/Grading Permit: A Clearing/Grading Permit may be issued prior to Master Site Plan approval only when all of the following approval criteria are met:
1. Application: A complete Master Site Plan application has been filed with and accepted by the Permit Center; and
 2. Clearing/Grading – Erosion and Sediment Control: All requirements of Chapters 16.26 (Clearing/Grading Permits) and 16.30 (Erosion and Sediment Control) IMC have been met; and
 3. Drainage: The applicant must provide an analysis of the property's regional drainage basin with a plan for implementing regional storm drainage control and water quality management; and
 4. Critical Areas: There shall be no filling or clearing/grading, prior to Level 5 Review approval, within any environmentally critical area. All critical areas will be delineated through critical area review and approved as set forth in the regulations concerning critical areas; and
 5. Fill: All fill material must be approved by the Public Works Department and must be graded to uniform contours, as approved by the Public Works Department, with no mounding such as might be used for preloading for buildings and other structures; and
 6. Costs: The applicant must pay all costs of plan review, field staking of environmentally critical areas, and/or inspection; and
 7. FEIS/Mitigation: The final environmental impact statement for the project, if required, has been issued by the City; and impacts that are associated with filling and grading can and will be mitigated; and
 8. Binding Agreement: All conditions of approval, including the requirement for substantial development, shall be contained in an agreement which shall bind future owners of the property. The agreement must be approved by the City and recorded with the King County Department of Records and Elections; and
 9. Bond: The bond or other security required by this Code and/or other related regulations must include an amount required to restore the property to its existing grade and conditions, or to provide for landscaping and revegetation of disturbed surfaces should substantial development not occur as provided in subsection (C)(2)(j) of this section; and
 10. Timing: Substantial development of buildings has commenced within eighteen (18) months from the issuance of the Fill and/or Grading Permit; provided, that upon request of the applicant made prior to expiration of the eighteen (18) month period, and due to circumstances beyond the control of the applicant, the City Council may extend the time for commencement of substantial development in increments not to exceed twelve (12) months, and not to exceed a total of thirty-six (36) months in extensions.
- C. Site Development Permit: A Site Development Permit may be issued through the appropriate review process, including the Design Criteria Checklist (see Appendix 2 of this chapter), allowing for the construction of an initial building as defined in subsection (C)(1) of this section prior to Master Site Plan approval only when all approval criteria set forth in subsection (C)(2) of this section have been completed.
1. "Initial building" means one (1) building or one (1) connected building group whose square footage does not exceed ten (10) percent of the maximum gross floor area proposed. The site or location of an "initial building" shall:
 - a. Be served by existing City streets and utilities; and
 - b. Be on the perimeter of the Master Site Plan.

2. Approval Criteria: All of the following approval criteria must be satisfied prior to making an application for a Site Development Permit herein:
 - a. Need: The need for the initial building substantially outweighs any detrimental effects caused by the building;
 - b. Complete Application: A Master Site Plan application has been filed and deemed complete by the Permit Center;
 - c. Completion of Certain Master Site Plan Processing Steps: The following review process steps for the Master Site Plan application are completed:
 - (1) Stage of Master Site Plan Review: A Master Site Plan application and all associated permits have been filed and deemed complete by the Permit Center and the Development Commission review of the project through the Community Conference step is complete;
 - (2) Environmental Review: An environmental checklist for the Master Site Plan has been filed with the Permit Center and the SEPA Responsible Official has completed review of the final EIS, if one is required;
 - d. Public Agency Review: All public agencies with an interest, or upon which the proposed development may pose potential impacts, have had adequate time to respond to the City with comments, concerns and/or conditions;
 - e. All Permit Applications Filed: All required permit applications for the site have been filed with and processed by the Permit Center and comprehensive staff reports on the same have been completed. The Development Commission and any other appropriate reviewing board or commission have recommended approval of said permit(s);
 - f. Transportation: The initial building is in compliance with the transportation component of the Comprehensive Plan and that the necessary right-of-way, easements and facilities have been provided for on or near the proposed initial building;
 - g. Dedication/Common Areas: The "initial building" application shall include provisions for the dedication, reservation, or installation of: common areas, buffers and other identified natural areas, environmentally critical areas, easements and facilities on, and in proximity to, the initial building site have been established and assured in a manner approved by the Planning Director/Manager;
 - h. Maintenance: Provisions for the maintenance and upkeep of all natural areas, buffers, landscape areas, environmental conditions and environmentally critical areas on, and in proximity to, the initial building site have been established and assured in a manner approved by the Planning Director/Manager;
 - i. Initial Building Site Mitigation: Land, easements, facilities and voluntary cash contributions and/or impact fees given as mitigation for the initial building site project impacts are required pursuant to the applicable regulations and are to be satisfied in an amount calculated upon a pro rata basis of the requirements identified for the entire Master Site Plan; and
 - j. Mitigation in Excess of Pro Rata: Mitigation measures required may exceed the calculated pro rata amount if deemed necessary. Contributions in excess of the pro rata share may not be permitted when contrary to an existing mitigation agreement, e.g., as part of a concomitant zoning agreement. Such determination shall be made on a case-by-case basis; provided, however, any excess or deficient contribution required herein shall be applied toward the remaining mitigation requirements established for the entire Master Site Plan.
3. Landscape Upgrading: As a part of final Master Site Plan approval, it may be determined that the initial building site landscaping and/or other accessory design-related site elements require upgrading. The owner/developer shall submit revised landscaping and accessory site plans for review and shall install all landscaping and other accessory design-related site details required by the approval. Changes to building footprint, modulation and materials are excluded from this provision.
4. Approval of Initial Building: Approval of a Site Development Permit for an initial building prior to Master Site Plan approval shall have no effect on the final Master Site Plan decision.

- D. Design Criteria Checklist: All elements listed in the Design Criteria Checklist (see Appendix 2 of this chapter) are reviewed by the staff and the Development Commission. The “initial building” shall be consistent with the scale and character of the permitted land uses adjacent to the site, and with the surroundings, both manmade and natural.
- E. Cost of Improvements: The following are approval criteria which apply to the cost and construction of improvements related to the Master Site Plan development:
1. The costs of construction and installation of all required on-site and off-site improvements shall be paid for by the owners and/or developers of the site (or by other means).
 2. The determination of the nature and extent of required off-site improvements shall be made by the City Council upon consideration of the recommendation by the Administration and other reviewing agencies. Such determination shall be based upon projected impacts of the entire development. In projects which are developed in phases, the improvements required may, upon approval by the City, be built in phases that are related to the need for such improvement created by each phase.
 3. Costs of on-site and off-site improvements shall include those which are anticipated because of development impacts upon existing facilities requiring present expenditures, or from impacts upon the demand for new or expanded facilities which will require future expenditures. In the case of anticipated impacts requiring present expenditures, the City shall not approve any application in connection with the development until such improvement has been made or until satisfactory arrangements for paying the costs of such improvement have been approved by the City. In the case of impacts which will require future expenditures, the City shall not approve any application in connection with the proposed development prior to obtaining a binding and secured commitment from the proponent to pay that portion of the cost of the needed improvement, which, when undertaken, is determined by the City to be reasonably related to the portion of the total need or demand for the improvement which will be created by impacts from the proposed development.
- F. Review and Recommendation Responsibility: The Planning Director/Manager and Development Commission shall recommend approval of a Master Site Plan to the City Council if the following elements are demonstrated by the applicant in the Master Site Plan application:
1. Comprehensive Plan Consistency: The project is compatible with and permitted by the Issaquah Comprehensive Plan and any other applicable area plan adopted by the City;
 2. Permitted Use Compatibility: The proposed project will be compatible with permitted land uses in the vicinity of the project site;
 3. Site Plan Contents: The following areas are clearly identified and marked on the Master Site Plan:
 - a. Environmentally critical areas and any required buffer and/or setback area;
 - b. Future development areas and the proposed land use in the form of a project development site plan;
 - c. Areas of historical or cultural significance;
 - d. Required buffer and setback areas (per this chapter), and required and proposed easements;
 4. Density: Specific densities have been identified for each phase of the proposed development;
 5. Streets and Sidewalks: Streets and sidewalks, existing and proposed, are: suitable and adequate to carry anticipated traffic within the proposed project and in the vicinity of the proposed project, including sidewalks and other planning features that assure safe walking conditions for students who only walk to and from school; are adequately designed and delineated on the proposed project development site plan; and are to be completed by the completion date of the development;
 6. Utility Services and Other Improvements: Utility services and other improvements, existing and proposed, are adequate for the development and are to be completed by the estimated completion date of the development as designated in Covenants, Conditions and Restrictions;
 7. Phasing: Each phase of the proposed development, as it is planned to be completed, provides for the required parking spaces, streets and sidewalks, recreation facilities and park land, landscape and open spaces, critical area designations and buffers and utility service areas, and rights-of-way necessary for creating and sustaining a desirable and stable environment;

8. Subdivision: If a subdivision application is being processed concurrently with a Master Site Plan, conformance with the requirements of Chapter 18.13 IMC shall be required;
9. Design Continuity: Design continuity is achieved through repetition of certain plant species and other landscape materials, certain building materials and other design concepts;
10. Accessory Structures: Accessory structures, including street furniture, mailboxes, kiosks and street lighting, will be designed to be part of the overall project design component and will provide uniformity and linkage through the site;
11. Nonmotorized Circulation: Linkages for safe circulation for pedestrians and bicycles shall be consistent with IMC 18.07.080, Nonmotorized facilities;
12. Public Access: Appropriate provisions are made for public access to any lakes, streams and scenic corridors within the site. The access provided must be environmentally sensitive in its design and implementation; and
13. Signage: The signage has consistent elements, such as color, shape, size, and graphics, which maintain consistency and uniformity throughout the project. (Ord. 2447 § 51, 2005).

18.07.670 Final recommendation.

Repealed by Ord. 2501. (Ord. 2447 § 52, 2005).

18.07.680 City Council final decision.

Repealed by Ord. 2501. (Ord. 2447 § 53, 2005).

18.07.690 Termination of approval and extensions of time.

Repealed by Ord. 2501. (Ord. 2447 § 54, 2005).

18.07.080 Nonmotorized facilities.

A. Purpose: The purposes of requiring nonmotorized facilities including bicycle lanes, shared-use corridors and walkways within developments are to:

1. Increase safe nonmotorized access to and mobility through all parts of the City;
2. Help remove nonmotorized and vehicular movement conflicts; and
3. Support transportation options that contribute to reduced traffic congestion, improved transit connections, improved air quality, reduced fuel consumption and improved physical fitness.

B. Requirements: All new multifamily residential and nonresidential developments shall provide nonmotorized facilities consistent with this section. Changes to existing multifamily residential and nonresidential developments that are nonconforming to these requirements shall be addressed by Chapter 18.08 IMC, Nonconforming Situations. These developments shall provide connections of required facilities with any existing public nonmotorized facility and/or provide a stub for future connection to any proposed public nonmotorized facility as documented in the Issaquah Comprehensive Plan, whether said connection is within the City or an adjacent jurisdiction. If none exist or are proposed in the immediate vicinity, the need for stubs for future connection to adjoining properties, if any, shall be determined by an evaluation of the land use designation and zoning and other relevant information by the permit official.

1. Pedestrian Facilities:

a. Sidewalks: Any required sidewalks in public rights-of-way shall be provided consistent with the adopted and/or amended Issaquah Standards and Specifications: Streets and Related Work.

b. Walkways: Barrier-free walkways providing the most direct route through a development shall be provided between public entrances and the nearest public sidewalk, trail, or shared use corridor.

(1) Walkway Connection Frequency: One walkway from a nonresidential or multifamily building to a public sidewalk is required in all instances with at least one (1) additional walkway required to the public sidewalk for each two hundred fifty (250) feet of street frontage.

(2) Multiple Building Walkway Systems: Developments containing more than one (1) building shall provide walkway systems that allow safe and efficient pedestrian circulation within the development. In addition to other requirements of this section, the walkway system shall:

(A) Link all public entrances of the buildings to each other and to the nearest public sidewalk, trail, or shared use corridor.

(B) Provide a perimeter walkway that is generally parallel to and continuous along all building facades with public entrances or associated landscaping areas.

(C) Connect at least one (1) walkway through the parking lot that is generally perpendicular to buildings and provides a walkway route between buildings in addition to perimeter walkways.

(D) In instances where building facades with any associated outdoor display and storage face the parking lot and exceed two hundred fifty (250) feet in length, provide an additional walkway through the parking lot for each increment of two hundred fifty (250) linear feet.

(E) Provide a continuous walkway on at least one (1) side of parking lot aisles that do not contain angle parking.

(F) Provide a continuous walkway on both sides of private roadways through a development that are not part of a parking lot.

(G) Not result in walkway dead ends that result in a pedestrian being unnecessarily required to cross a street or other vehicular area and/or take a circuitous route in order to resume travel on a walkway.

(3) Large Building Walkway Systems: Subsections (B)(1)(b)(2)(A), (B), (E), (F) and (G) of this section apply to individual buildings of fifteen thousand (15,000) square feet or greater in addition to other requirements of this section.

(4) Size: All walkways must be at least five (5) feet wide, excluding vehicular overhang, displays, and storage.

(5) Materials: All walkways must be composed of materials that are permanent and visually distinctive from parking lot paving. Said materials shall also be consistent with ADA access.

(6) Safety: All walkways must be physically separated from vehicular area by grade, landscaping strips, berms, barriers, curbs or similar means, provided in a manner that retains pedestrian visibility.

(7) Lighting: See IMC 18.07.107, Outdoor lighting, for minimum lighting requirements.

(8) Transit Access: A walkway connection to the public sidewalk must be made within one hundred fifty (150) feet of an adjacent bus stop.

c. Crosswalks: A crosswalk composed of materials that are permanent and visually distinctive from parking lot paving, including but not limited to materials or techniques such as concrete, aggregate, paving stones, and pavement imprinting, shall be required whenever a walkway crosses any driveway or paved area accessible to vehicles. Paint is not acceptable as a sole means of marking crosswalks.

d. Benches: Where a building entrance or entrances are more than two hundred fifty (250) feet from the public right-of-way, at least one (1) fixed bench or equivalent seating area shall be required near the midpoint along the private walkway.

2. Bicycle and Shared Use Nonmotorized Facilities:

a. Bicycle Lanes: Any required bicycle lanes in public rights-of-way shall be provided consistent with the adopted and/or amended Issaquah Standards and Specifications: Streets and Related Work and the Comprehensive Plan Bicycle and Shared Use Corridor Map.

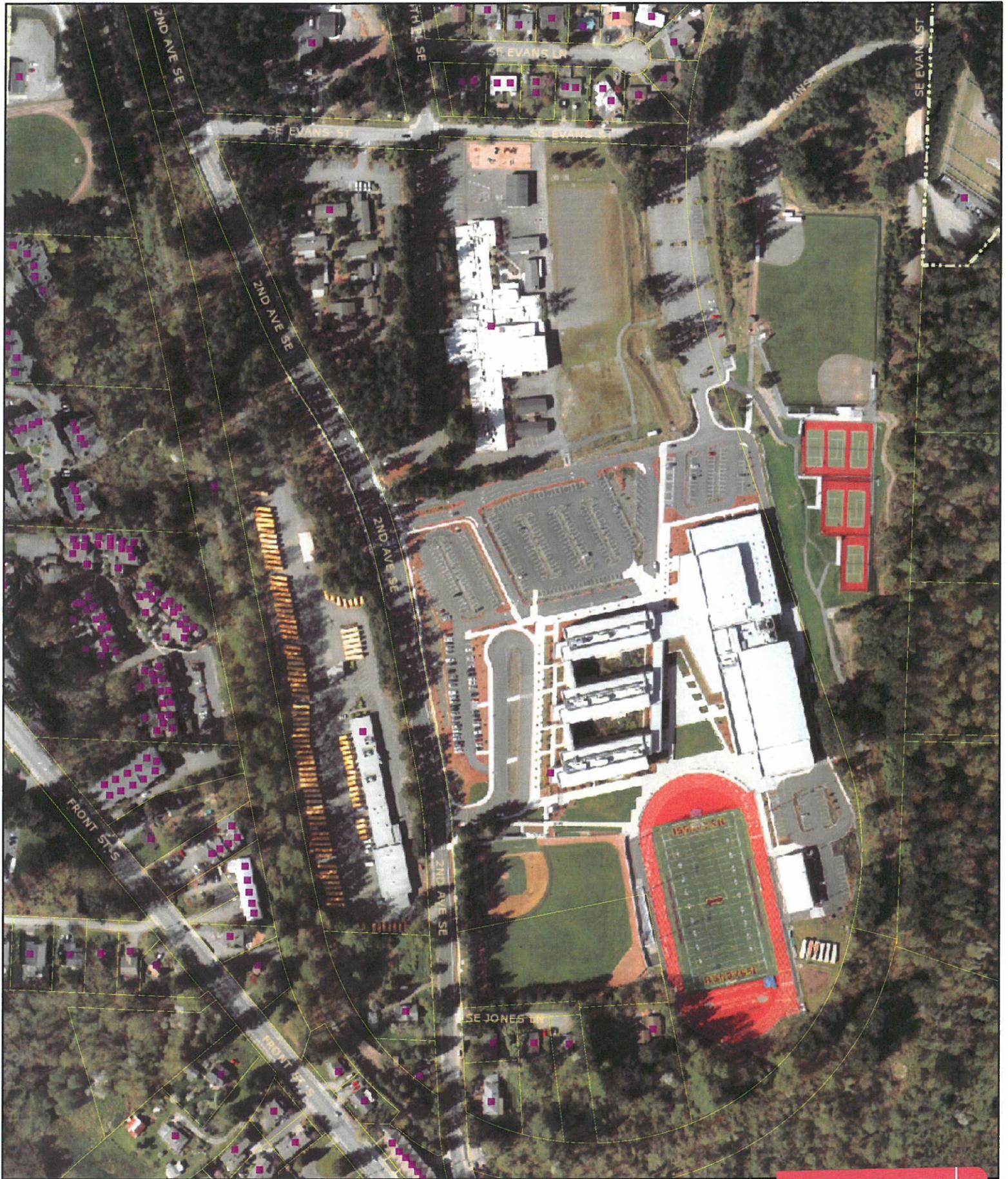
b. Shared Use Corridors: All shared use corridors shall comply with all applicable requirements for walkways in subsection (B)(1)(b) of this section. Shared use corridors shall be marked with directional signs to indicate shared use in a manner consistent with IMC 18.11.170. Shared use corridors shall be at least ten (10) feet wide.

c. Bicycle Parking: Bicycle parking shall be provided consistent with IMC 18.09.030(l), Bicycle Parking.

3. Single Family Privacy: Nonmotorized facilities or trails on multifamily or commercial property must be located the maximum distance possible from adjacent single family property lines that will still allow meeting the other requirements of this chapter. In any case, a minimum setback of at least fifteen (15) feet from any adjacent single family property line is required. Landscaping shall be provided within the setback in accordance with the requirements of Chapter 18.12 IMC, Landscaping and Tree Preservation. All public trails required as a condition of site development shall be designed and built using the guidelines within the Comprehensive Plan.

C. Administrative Adjustment of Standards: An applicant may request an Administrative Adjustment to these standards as established in IMC 18.07.250 and reviewed by the criteria in IMC 18.07.350. These additional considerations may also be used in any AAS review:

1. Adjustment is necessary for compliance with historic requirements.
2. Adjustment is necessary to avoid encroachment into a critical area or preserve a significant natural feature such as a large tree.
3. Adjustment is supported by public dedication of nonmotorized facilities.



School property aerial

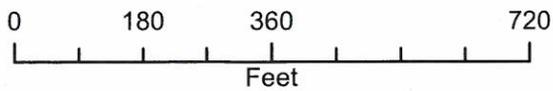
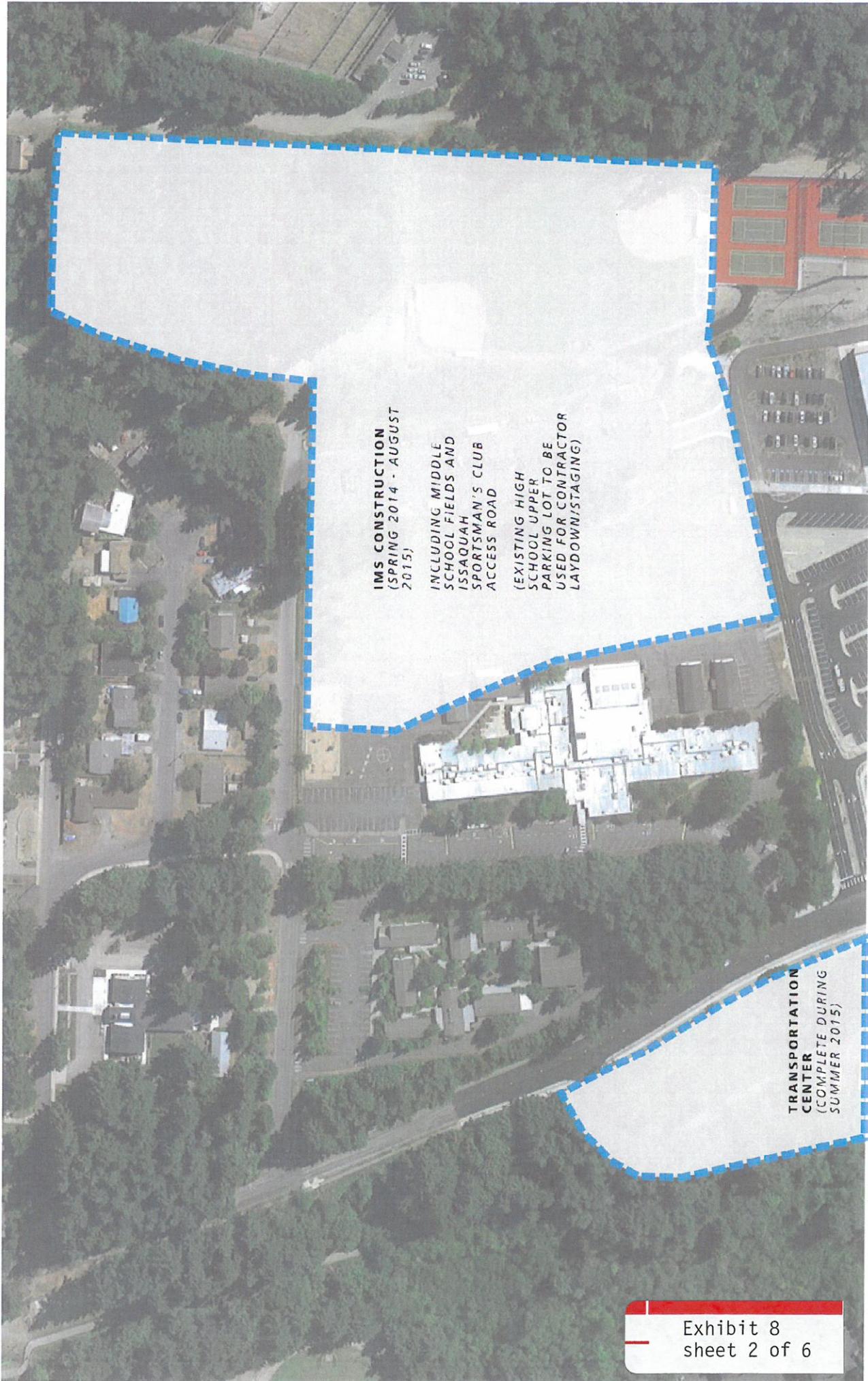


Exhibit 7



EXISTING SITE
ISSAQUAH MIDDLE SCHOOL
ISSAQUAH SCHOOL DISTRICT | 19 FEBRUARY 2013

mahlum

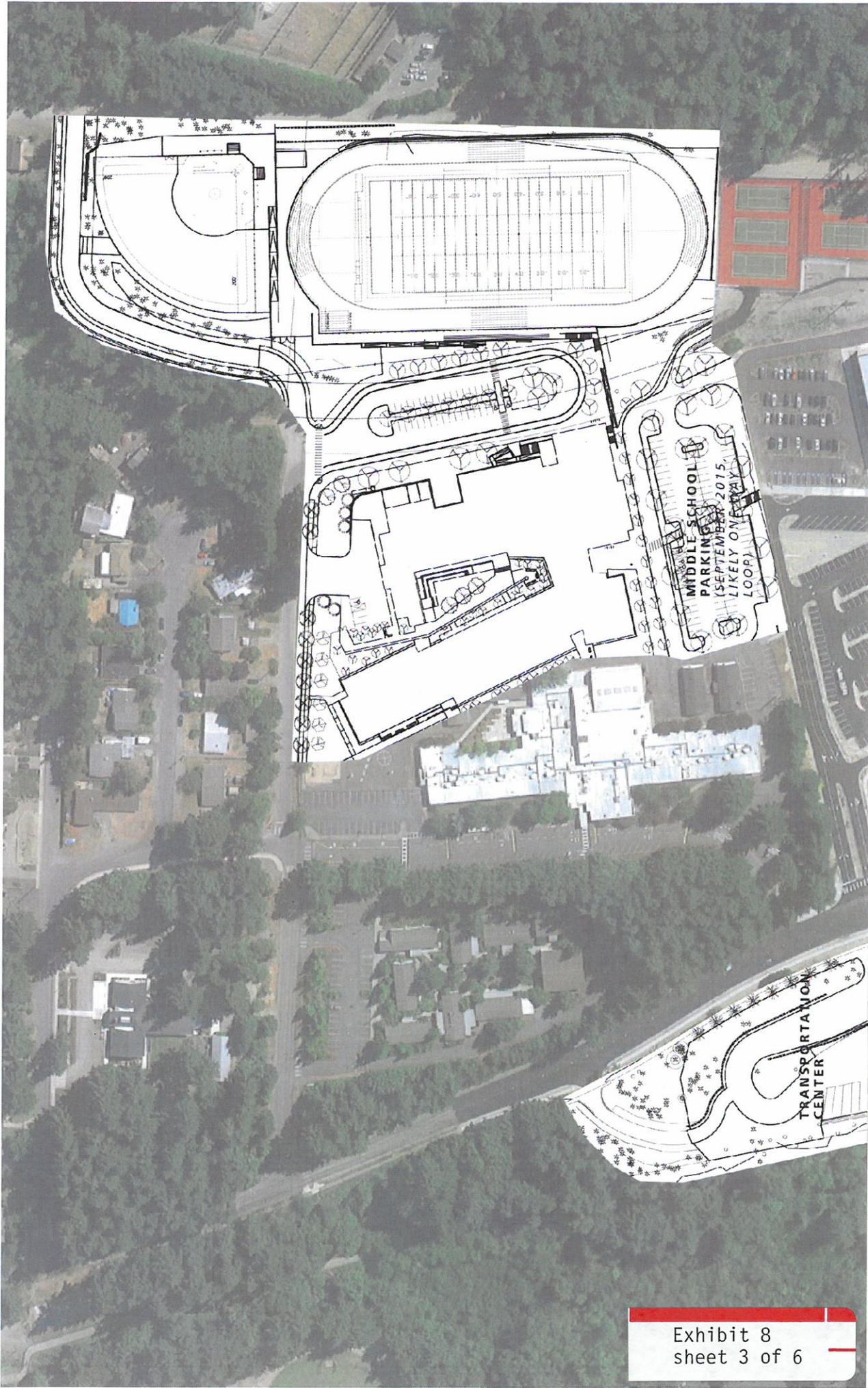


IMS CONSTRUCTION
 (SPRING 2014 - AUGUST 2015)
 INCLUDING MIDDLE
 SCHOOL FIELDS AND
 ISSAQUAH
 SPORTSMAN'S CLUB
 ACCESS ROAD
 (EXISTING HIGH
 SCHOOL UPPER
 PARKING LOT TO BE
 USED FOR CONTRACTOR
 LAYDOWN/STAGING)

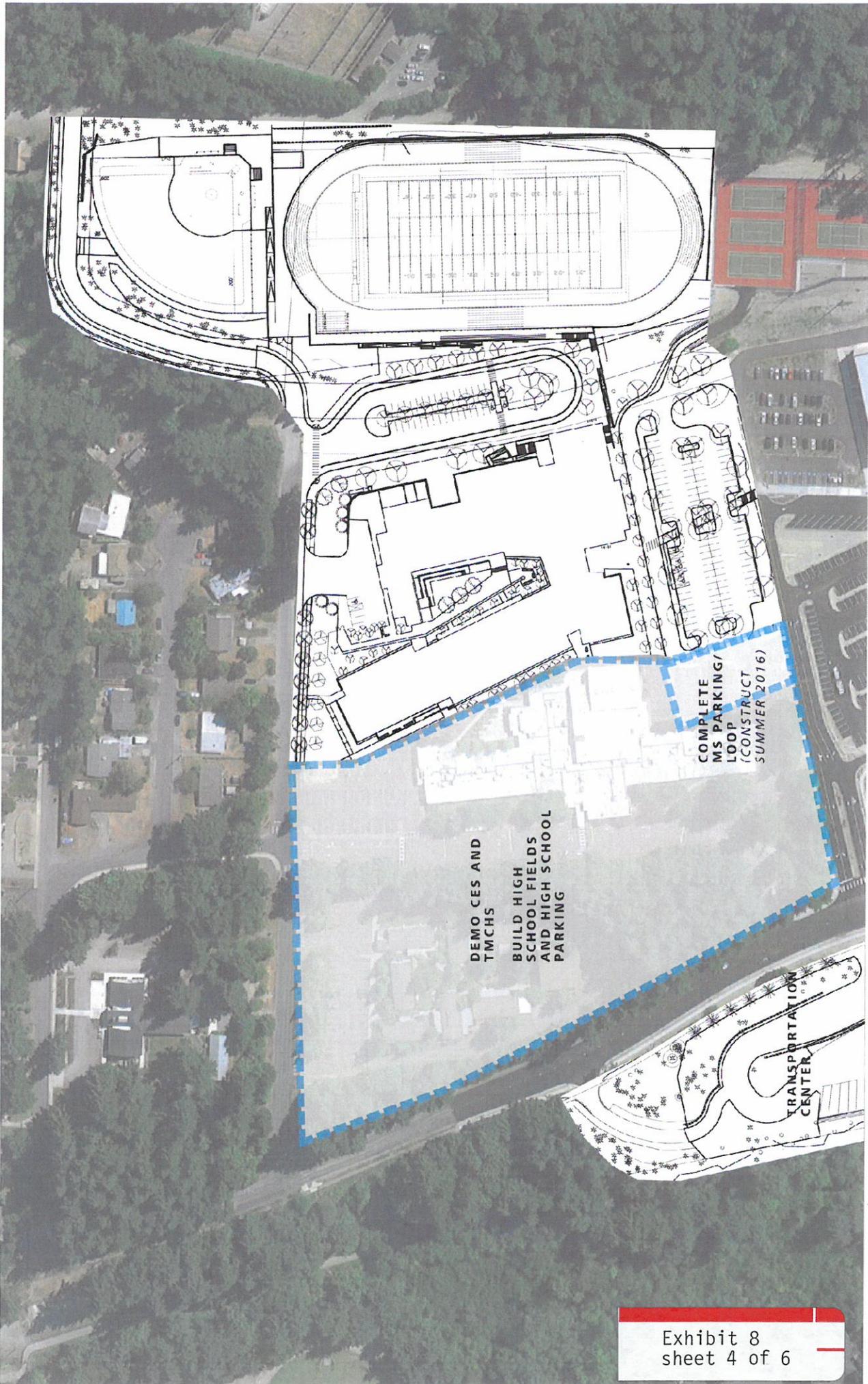
**TRANSPORTATION
 CENTER**
 (COMPLETE DURING
 SUMMER 2015)

PHASE 01 (SPRING 2014-AUGUST 2015)
 ISSAQUAH MIDDLE SCHOOL
 ISSAQUAH SCHOOL DISTRICT | 19 FEBRUARY 2013





PHASE 02 (SCHOOL YEAR: 2015-2016), CES + TMCHS UNDER CONSTRUCTION (AT EXISTING MIDDLE SCHOOL SITE)
ISSAQUAH MIDDLE SCHOOL
ISSAQUAH SCHOOL DISTRICT | 19 FEBRUARY 2013



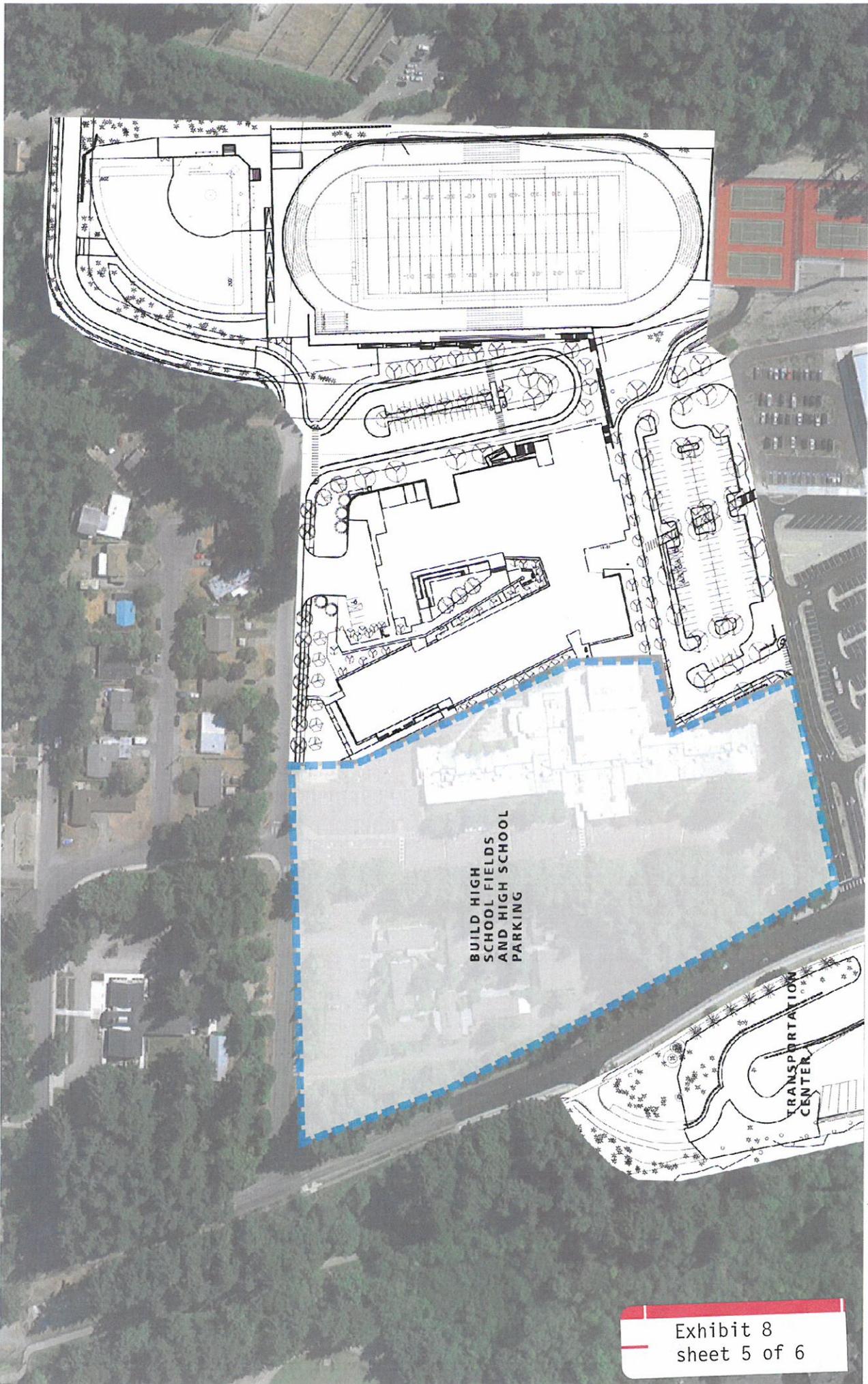
DEMO CES AND
TMCHS
BUILD HIGH SCHOOL FIELDS
AND HIGH SCHOOL
PARKING

COMPLETE MS PARKING/
LOOP
(CONSTRUCT
SUMMER 2016)

TRANSPORTATION
CENTER

PHASE 3A (SUMMER 2016)
ISSAQUAH MIDDLE SCHOOL
ISSAQUAH SCHOOL DISTRICT | 19 FEBRUARY 2013

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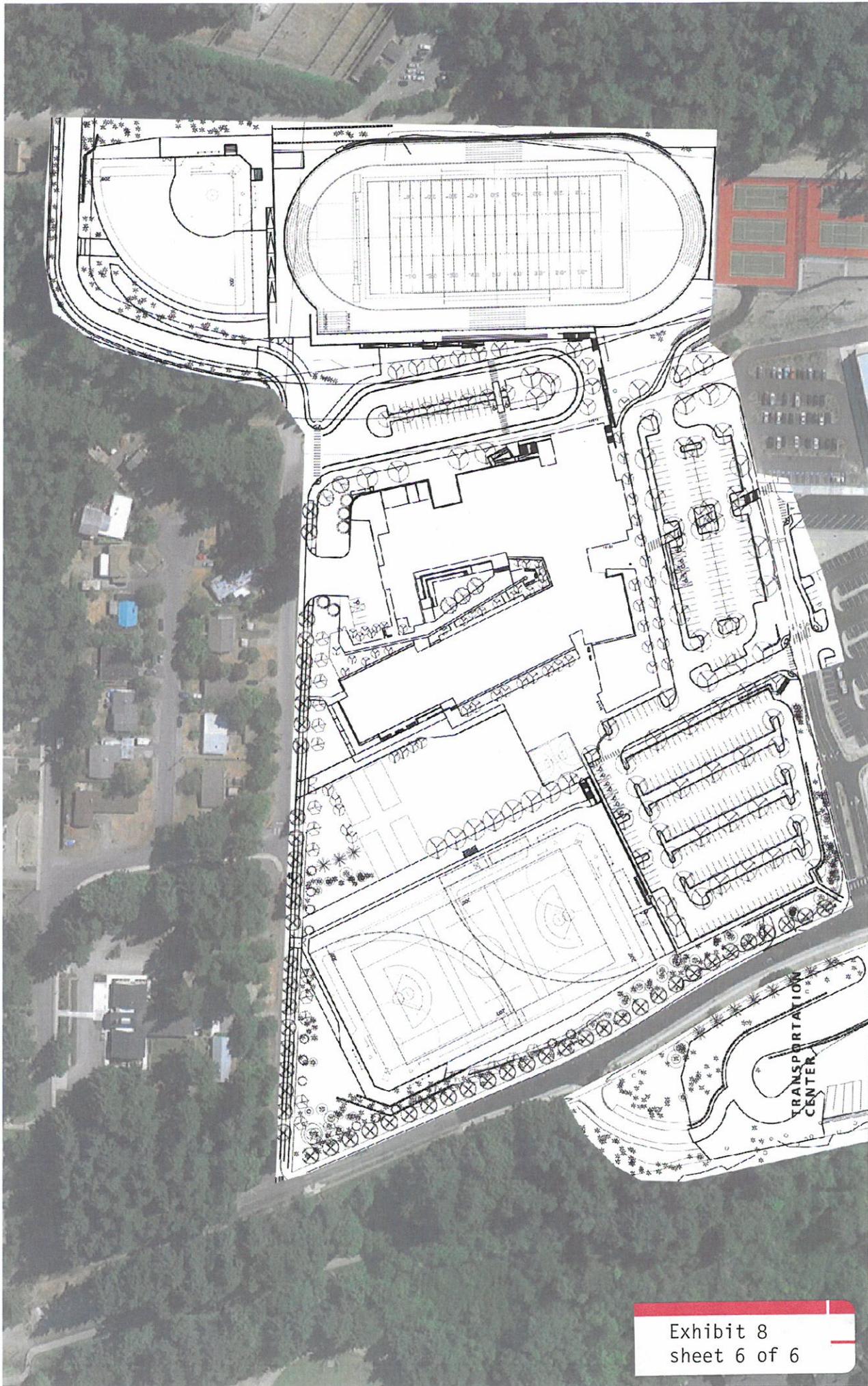


BUILD HIGH SCHOOL FIELDS AND HIGH SCHOOL PARKING

TRANSPORTATION CENTER

PHASE 3B (SUMMER - DECEMBER 2016)
ISSAQUAH MIDDLE SCHOOL
ISSAQUAH SCHOOL DISTRICT | 19 FEBRUARY 2013

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CONSTRUCTION COMPLETE (DECEMBER 2016)
ISSAQUAH MIDDLE SCHOOL
ISSAQUAH SCHOOL DISTRICT | 19 FEBRUARY 2013

Exhibit 8
sheet 6 of 6

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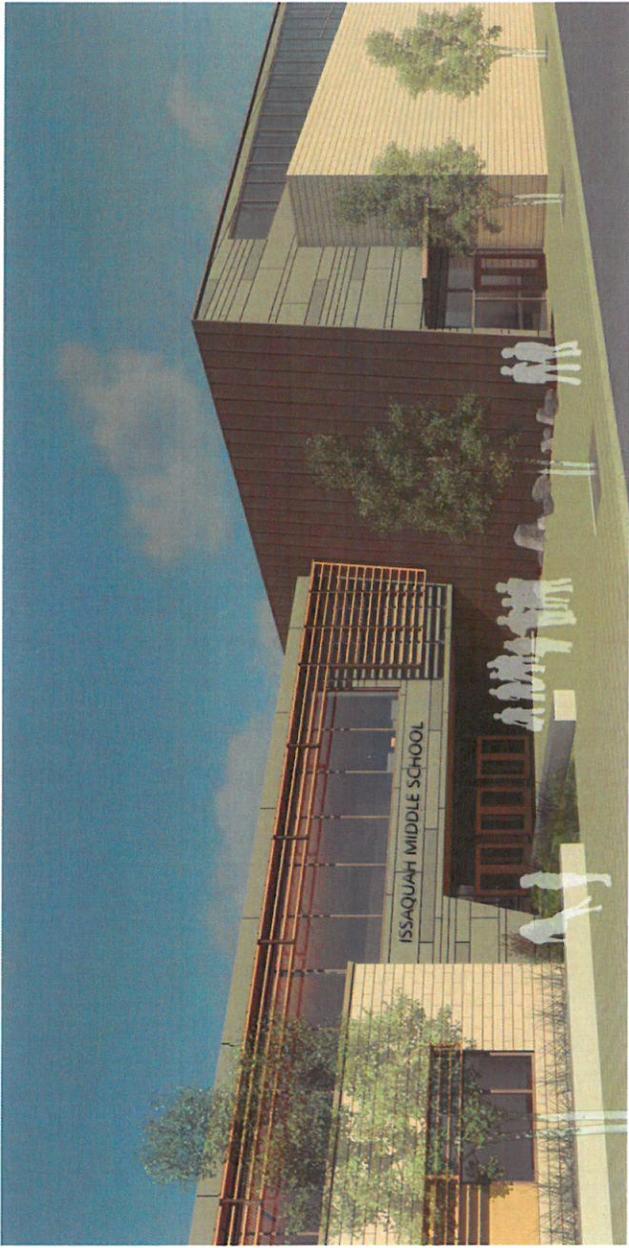
71 COLUMBIA | FLOOR 4
SEATTLE, WA 98104
(206) 441-4131 OFFICE
(206) 441-4949 FAX
WWW.MAHLUM.COM
1000 MOUNTAIN VIEW RD
SUITE 100
ISSAQUAH, WA 98257
(360) 274-4030 OFFICE
(360) 274-0918 FAX
ISSAQUAH.MAHLUM.COM

FOR REFERENCE
ONLY

ISSAQUAH SCHOOL DISTRICT
ISSAQUAH MIDDLE SCHOOL



MARK DATE	DESCRIPTION
7 AUGUST 2013	7 AUGUST 2013
ISSUE	COMMUNITY CONFERENCE
ISSUED BY	ISSAQUAH SCHOOL DISTRICT
ISSUED FOR	ISSAQUAH MIDDLE SCHOOL
ISSUED BY	ISSAQUAH SCHOOL DISTRICT
ISSUED FOR	ISSAQUAH MIDDLE SCHOOL
ISSUED BY	ISSAQUAH SCHOOL DISTRICT
ISSUED FOR	ISSAQUAH MIDDLE SCHOOL



ISSAQUAH MIDDLE SCHOOL
ISSAQUAH SCHOOL DISTRICT

7 AUGUST 2013
COMMUNITY CONFERENCE

mahlum

11500 NE 138TH STREET, SUITE 4
KENT, WA 98025
PHONE: 425.471.7212 FAX: 425.471.7213
WWW.MAHLUM.COM

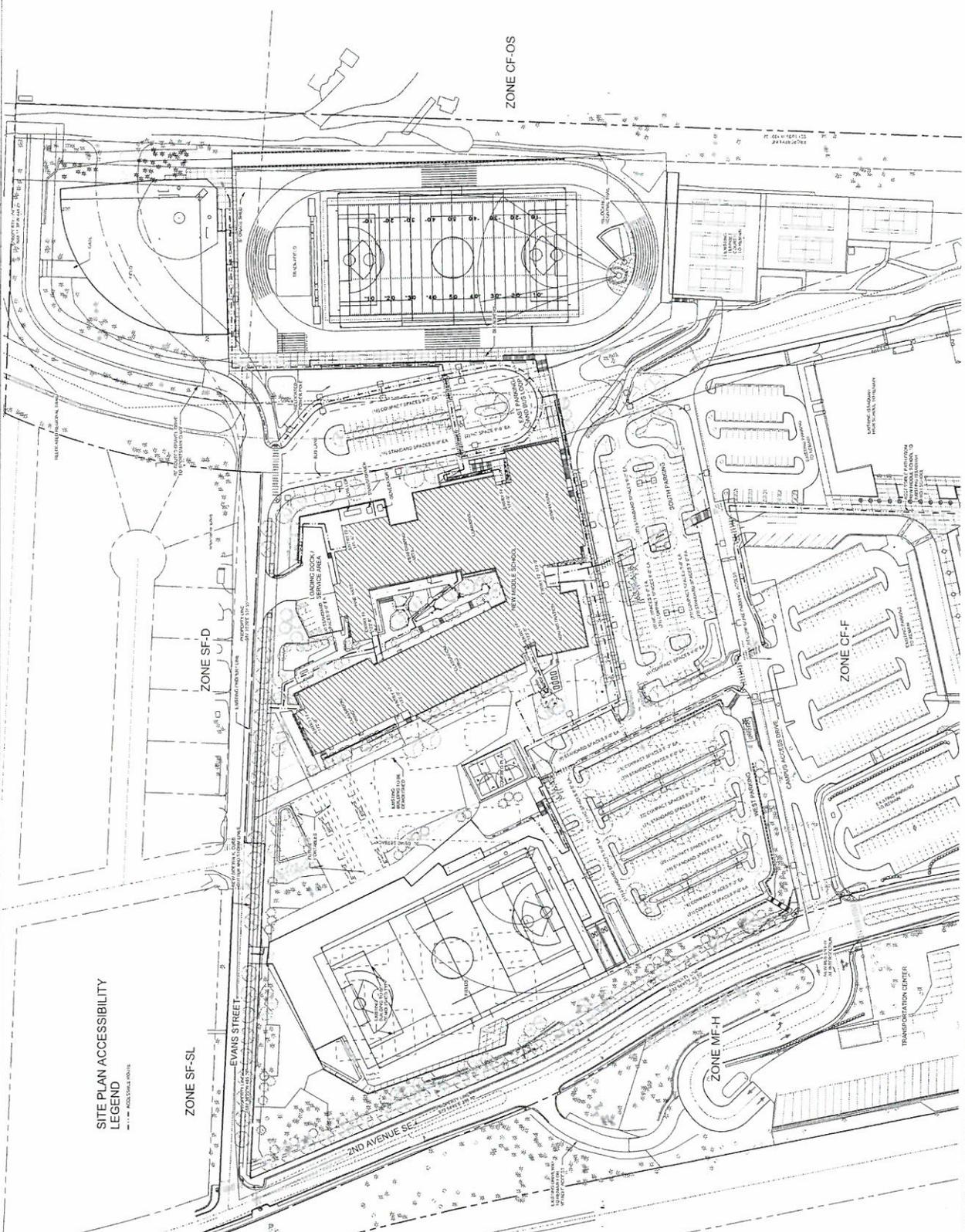
FOR REFERENCE ONLY

ISSAQUAH SCHOOL DISTRICT
ISSAQUAH MIDDLE SCHOOL



DATE	2/15/2013
PROJECT	DESIGN DEVELOPMENT
PREPARED BY	JULIE DILLON
CHECKED BY	BOB BROWN
SCALE	AS SHOWN
SITE PLAN - ACCESSIBILITY PLAN	

G-006



SITE PLAN ACCESSIBILITY LEGEND

1 SITE PLAN - ACCESSIBILITY PLAN

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31 COLUMBIA, SUITE 4
MAYFIELD, WA 98144
PH: 425 455 0100
WWW.MAHNUM.COM

224 W. NORTH 10TH ST
SEASIDE, WA 98134
PH: 206 441 0100
WWW.MAHNUM.COM

COUGHLIN
PORTER
LUNDEEN

4000 1ST AVE, SUITE 100
SEASIDE, WA 98134
PH: 206 441 0100
WWW.MAHNUM.COM

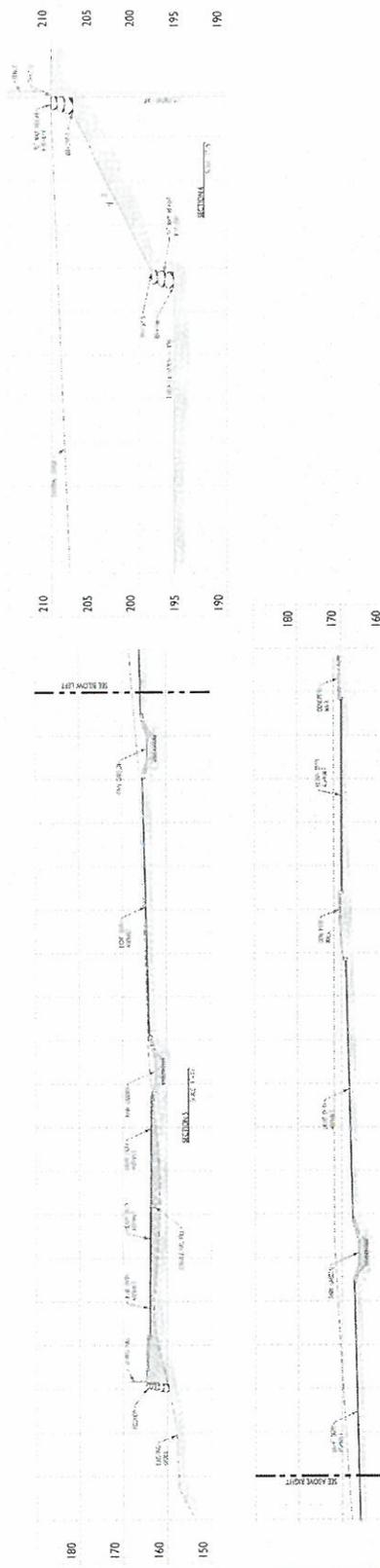
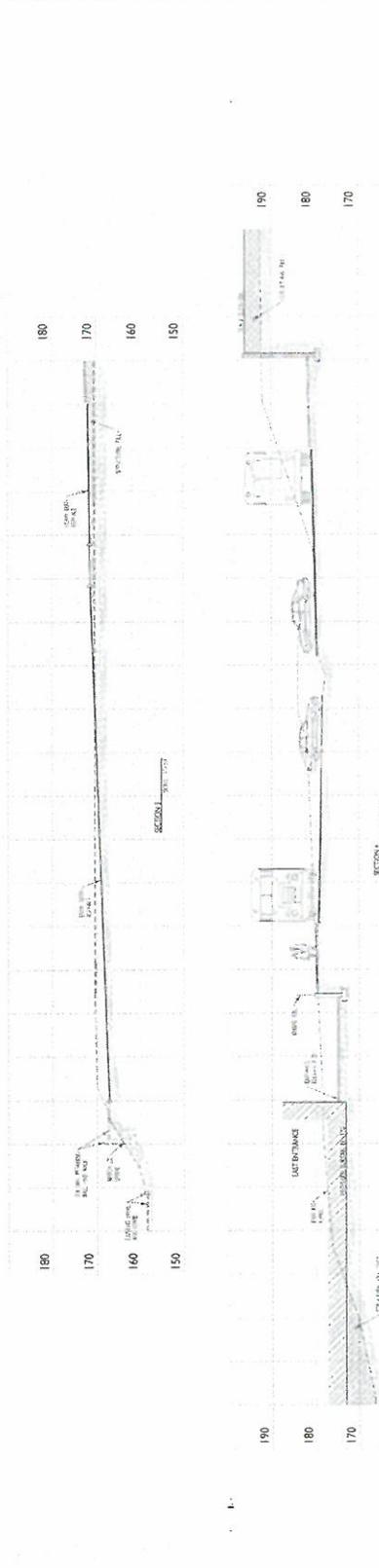
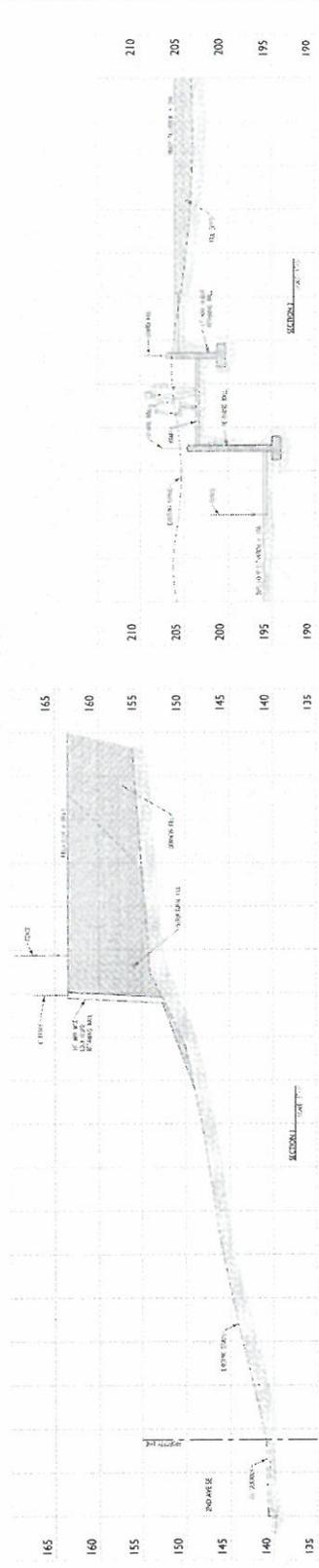


ISSAQUAH SCHOOL DISTRICT
ISSAQUAH MIDDLE SCHOOL



NO.	DATE	DESCRIPTION
1	21 MAY 2013	ISSAQUAH MIDDLE SCHOOL
2	02 JUN 2013	ISSAQUAH MIDDLE SCHOOL
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C-520



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71 COLUMBIA TOWER A
1221 NW 124TH AVE #102
PORTLAND, OR 97269
503.274.0118 FAX
503.274.0118 FAX
MAHLUM.COM

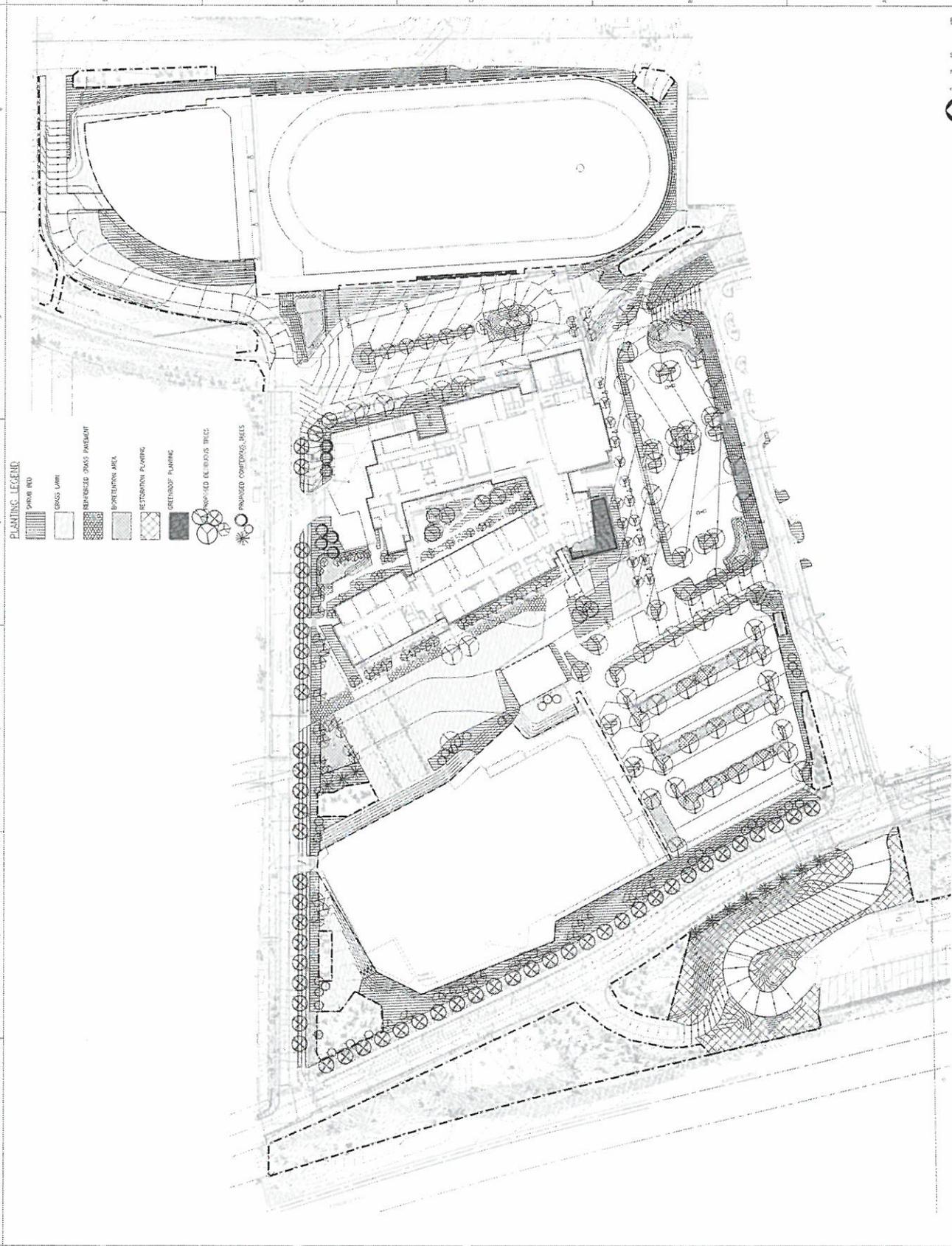


ISSAQUAH SCHOOL DISTRICT
ISSAQUAH-MIDDLE SCHOOL



DATE: 04/20/10
21 MAY 2010
PROJECT: ISSAQUAH MIDDLE SCHOOL
PROJECT NO.: 000000
SHEET NO.: 000000
SCALE: 1" = 30'-0"

L-500





SEE SHEET 4

SEE SHEET 2

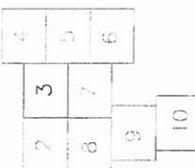
SEE SHEET 4

SEE SHEET 5

- LEGEND**
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 - 2. EXISTING
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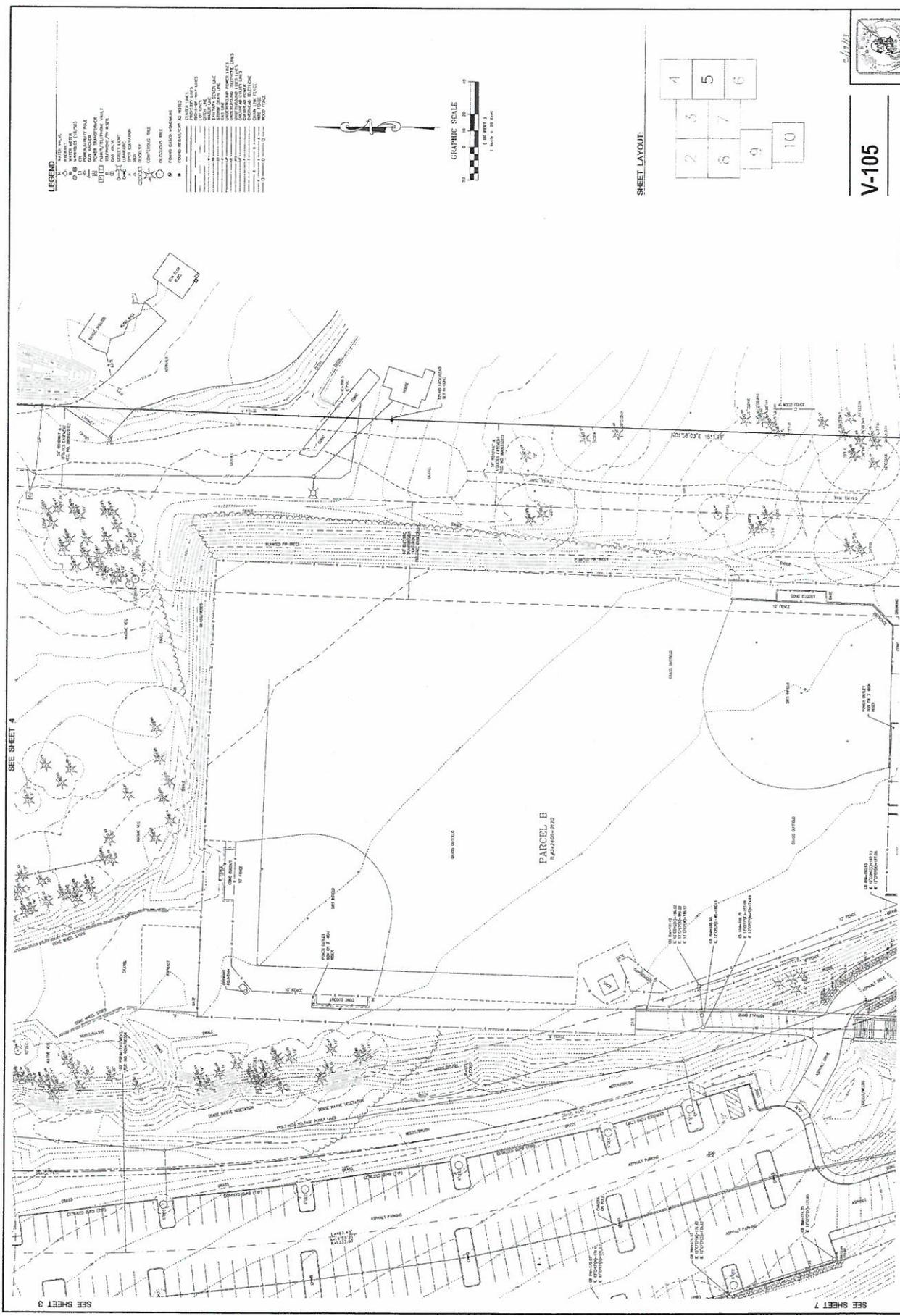


SHEET LAYOUT:



V-103

		PROJECT NO. 07428.14	SHEET NO. 10
TITLE TIGER HIGH SCHOOL / CLARK ELEMENTARY SCHOOL		TOPOGRAPHIC SURVEY FOR ISSAQUAH SCHOOL DISTRICT #411	
DATE NOVEMBER 20, 2011		POSITION OF 1/2 SECTION 34, T. 24 N., R. 6 E., S. 10 W. PORTION OF 1/2 SECTION 34, T. 24 N., R. 6 E., S. 10 W.	
DRAWN BY J. J. [Name]		CHECKED BY [Name]	
PLOTTED BY [Name]		SCALE 1" = 40'	
PROJECT LOCATION 11225 Mountain Meadows Blvd, Suite 300 Issaquah, WA 98029 Phone: (206) 206-1111 Fax: (206) 206-1112 Email: info@pace-survey.com			



V-105

PORTION OF S. 1/2 NW 1/4 SECTION 34, T. 24 N., R. 8 E., W. 4
 PORTION OF S. 1/2 SW 1/4 SECTION 34, T. 24 N., R. 8 E., W. 4

TOPOGRAPHIC SURVEY
 OR
ISSAQUAH SCHOOL DISTRICT #411

PROJECT NO.
0742814

SHEET 5 OF 10

DATE	NOV 12, 2013
TIME	10:00 AM - 12:00 PM
SCALE	1" = 100'
BY	PAUL BROWN, P.E., S.E., L.S.
CHECKED BY	PAUL BROWN, P.E., S.E., L.S.

TIGER HIGH SCHOOL /
CLARK ELEMENTARY SCHOOL

11225 SW 10th Ave, Suite 200
 Portland, OR 97225
 P: 503.222.8111 | F: 503.222.8643
 Cell: 503.222.8111 | Fax: 503.222.8643
 Paul Brown, P.E., S.E., L.S.
 paul@pace-engineers.com



DATE	NOV 12, 2013
TIME	10:00 AM - 12:00 PM
SCALE	1" = 100'
BY	PAUL BROWN, P.E., S.E., L.S.
CHECKED BY	PAUL BROWN, P.E., S.E., L.S.

