

CITY OF ISSAQUAH
DEVELOPMENT SERVICES
DEPARTMENT DEVELOPMENT
COMMISSION

COMMUNITY CONFERENCE

STAFF REPORT

November 18, 2015

FILE NO.: Community Conference: COM15-00003

PROJECT: 355 Sunset Way (Issaquah Apartments)

OWNER: DJ Loveridge
Thick Brick, LLC and Which is Promise, LLC
19538 SE 51st Street,
Issaquah, WA 98027

APPLICANT: Rick Golstrom
GMS Architectural Group
1804 136th Place NE, Ste. 1
Bellevue, WA 98005

STAFF CONTACT: Amy Tarce, Senior Planner
Development Services Department, (425) 837-3097

REQUEST: First review of a redevelopment of an existing single-family into a 7-unit 4-story multi-family apartment (See Attachment 3 for plans and elevations). Staff is requesting guidance from the Development Commission on whether the proposal is:

- Consistent with the overall vision for East Sunset Way Development Area
- The treatment of the building and Sunset Way streetscape achieves the intended pedestrian-scale objectives for this subarea, including screening of parking and treatment of the ground floor visible from Sunset Way

LOCATION: 355 E. Sunset Way

EXISTING LAND USE:

| | |
|--------------------------|-------------------------------|
| <u>Subject Property:</u> | Single-family. |
| <u>North:</u> | Office |
| <u>South:</u> | Single family residential |
| <u>East:</u> | Single family residential |
| <u>West:</u> | Fourplex (2 duplex buildings) |

EXISTING CONDITIONS:

The project site is on Sunset Way, one of the main connection from the I-90 freeway to downtown Issaquah. A one-story single-family structure currently sits on the site. A chain link fence provides enclosure of the property frontage along Sunset Way. The site has no critical areas. Approximately two thirds of the site is pervious and two small trees are visible in the front yard. A carport in the rear of the house is accessed off of an existing alley; however, there is no paved driveway.

EXISTING ZONING:

The site is zoned MF-H, Multi-family, High density. It is also a receiving site for Transfer Development Rights.

COMPREHENSIVE PLAN:

The site is designated "Multi-family Residential".

SUBAREA:

East Sunset Way Development Area in Olde Town

BACKGROUND:

| | |
|------------------|--|
| March 18, 2015 | The City's Project Review Team held a Pre- Application Conference with the applicant. |
| November 2, 2015 | A Notice of Community Conference was sent to property owners within 300 feet of the project site and to parties of record. |

NEXT STEPS:

The applicant will submit an application for a Level 3 Site Development Permit (SDP). Ultimately, the SDP will be reviewed at a public hearing by the Development Commission.

COMMUNITY CONFERENCE - PURPOSE:

This meeting scheduled before the Development Commission is for the informal "Community Conference" meeting required as part of the development approval process. A Site Development Permit will be required for this project.

Section 18.04.140(A) of the Issaquah Land Use Code states the following under Community Conference Purpose:

"The Community Conference is an informal community meeting, hosted by the Development Commission. The purpose of the meeting is to generate discussion, raise issues, and propose creative options relative to the proposed project. It is intended to provide a means by which the applicant, staff, the Development Commission and the public are able to work together in a productive and creative manner. However, options and issues raised may not be all inclusive and no guarantees on the project outcome are made at this stage."

Section 18.04.140 (B) states the following under Community Conference Expectations:

"The applicant can expect the following results from the conference:

- 1) The more information an applicant can provide for a community conference, the more complete staff's review and input will be for the proposal.*
- 2) Any information or opinions expressed by the Development Commission or the staff shall not be binding on the final decision or constitute approval or denial of the proposed project.*
- 3) Inconsistency with the Comprehensive Plans, Guiding Principles or City Vision will be discussed.*
- 4) Development Commission, staff and applicant should discuss creative approaches to address challenging site constraints or potential mitigations.*
- 5) Recommended revisions or modifications to the proposal will be discussed; and*
- 6) The applicant should be aware that additional modifications will most likely be required before the project review is final and a decision has been made."*

Public Notification:

As part of the Community Conference process, public notice is required to be provided to all property owners within 300 feet of the exterior boundaries of the proposal site at least 10 days prior to the meeting. Notice of the project and the Community Conference meeting was mailed out to 66 property owners on November 2, 2015. Notice of the Community Conference Meeting was also posted on the City's web site calendar. The site plan included in the notice to neighbors has since been revised. The updated site plan is included as an attachment to this staff report.

SITE DEVELOPMENT PERMIT REVIEW:

Purpose: Per IMC 18.07.410, the purpose of the Level 3 Site Development process is to ensure public input and review by the Development Commission, the Development

Services Department and other departments for consistency with the development regulations, Design Criteria Checklist (see Chapter 18.07 IMC) and other approval criteria. A Level 3 review is required because the site's primary access and/or street frontage are located on and/or the site abuts Issaquah-Fall City Road.

Review Process: The Issaquah Municipal Code (IMC) Section 18.04.450 states that Site Development Permits shall be reviewed through the Level 3 Review process. The flow chart in Section 18.04 for Level 3 Review requires the Community Conference for Site Development Permits. As stated above, the Site Development Permit will ultimately be approved at a Public Hearing with the Development Commission. Through that review process, the applicants must show that the project meets the approval criteria for Site Development Permits.

PROJECT DESCRIPTION AND STAFF ANALYSIS:

Staff analysis for this meeting is focused on how the proposed multi-family development meets the intent and general standards of the Comprehensive Plan and Land Use Code.

Project Overview

The project consists of a 7-unit four-story apartment building on a single lot with a lobby accessed off of Sunset Way and from a ground level garage. The building has undulating bays and roof heights, as well as recesses for balconies on all sides. The tallest element is the elevator/stair tower at the northeast corner of the site, while the lowest section is at the rear, next to the alley. Front, side and rear yards are provided. Street trees are proposed along the front yard.

Comprehensive Plan

The project site is in the East Sunset Way Development Area of Olde Town. The Comprehensive Plan Land Use Element describes the vision for this area below:

East Sunset Way offers the most significant potential for change and transition to a pedestrian friendly, mixed-use activity area, developed to be served conveniently with transit. Sunset Way is targeted for landscaping and streetscape improvements, this will encourage pedestrian activity, transit accessibility and buffer residential areas. These improvements will enhance the neighborhood quality not only from residents in the area, but also visitors that are encouraged to explore the area for small shops or specialty services. Multifamily units above ground floor retail and services, or courtyard multifamily will keep the streets pedestrian friendly and accessible.

In Chapter 4, Circulation and Parking Element, Figure IV-A, Circulation Network identifies Sunset Way as a minor arterial and a bikeway. Figure IV-D, Trails Plan, also reinforces the bikeway along Sunset Way. More recently, a cycle track along the south side of Sunset Way has been identified by the Walk and Roll Study. Chapter 4 also noted that Sunset Way streetscape improvement was a high priority for capital improvements at the time of the adoption of the Comprehensive Plan in 1999.

In Chapter 5 of the Comprehensive Plan, the Urban Design concept for development can be found in the following subsections:

Sunset Way Activity Area (p.V-14)

East Sunset Way from the edge of the CBD to the Sunset Interchange will continue the transition towards multifamily units. The Olde Town Plan will direct, to the Sunset Way corridor, small-scale high density multifamily residential and neighborhood commercial as mixed use developments. New commercial within mixed use residential will expand both the availability of local services the neighborhood population who will use the local services. The consolidation of small lots may be required to make redevelopment feasible.

Urban Design Focus: *Use Sunset Way development to provide additional housing opportunities within Olde Town and support the CBD as a livable pedestrian-friendly and accessible activity area.*

Strategy: *Assure that new development is a sufficient size and density to economically support the desired local services, transit and pedestrian activity. New multifamily will utilize courtyard and other pedestrian-friendly designs. This area will include small scale offices and other neighborhood-oriented commercial uses to supplement the anticipated multifamily. Integrated into Sunset Way will be distinct landscaping to soften the environment to offset increasing traffic and land use intensity. Specific design guidelines for Sunset Way will be used to encourage courtyard housing and building articulation to break up building mass and reflect the surrounding small scale. New development along Sunset Way will enhance walkway links for residents to parks, open space, and trails. Sunset Way development will maintain direct connection to Front Street and improve the streetscape to emphasize that connection. The Transit Center along Sunset Way will be supported by transit friendly development that eases use of transit services.*

Staff Analysis:

To summarize, the key principles embodied in the Comprehensive Plan policies and design concept for East Sunset Way are as follows:

- Small-scale high density mixed-use development with neighborhood-oriented commercial uses
- As a transition/buffer for single-family residential area
- Pedestrian-friendly streetscape
- Multi-family units with ground-floor retail
- Pedestrian connection to Front Street, parks, open space and trails

The intent for the East Sunset Way Development Area is to allow for small-scale office and higher density multi-family developments to co-exist side by side within the whole subarea and not necessarily within one development. This is reinforced by the way the Permitted Uses Table identifies single-use land uses as allowed in the CBD. It is expected that treatment of massing and height, as well as landscaping, will be used to minimize the impact of the more intense uses from the single-family uses in the CBD.

There is also a strong emphasis in creating a pedestrian-friendly streetscape and including ground floor retail in multi-family developments to encourage walking and transit use. The buildings are to be scaled down through the use of articulation and modulation to break the building mass.

Land Use Code (Issaquah Municipal Code Article 18)

I. Zoning District (IMC 18.06) The review of this project for compliance with the Land Use Code will be based on its designation of the property in a Multifamily- High (MF-H) zoning district. In addition to standards found in IMC 18.06.100, this project is subject to the Multi-family and Duplex development standards in IMC 18.06 and 18.07.440.

A. Intent. IMC 18.06.100.H establishes the intent of the Multi-family High Density zoning district as follows:

The primary purpose of this zoning district is to provide for high density multifamily neighborhoods, in direct proximity to a variety of urban services and commercial uses. Duplexes and multifamily units are permitted uses. Senior housing, recreational uses, small scale retail, and lower intensity urban services are permitted as governed by the Table of Permitted Land Uses; provided, that traffic and other related impacts are not detrimental to the district. In addition to the objectives stated in the Purpose and Intent section of this chapter, the following objectives also apply to this district:

1. Provide opportunities for high density multifamily dwellings in direct proximity to commercial and retail services while preserving open space areas;
2. Require landscape buffers which separate neighboring land uses and arterial roadway from dwelling units;
3. Locate housing in close proximity to a range of urban services, to facilitate transportation alternatives and encourage pedestrian access;
4. Require that traffic generated from high density multifamily use will not pass through single family neighborhoods; and
5. Provide opportunities for limited scale retail, office, and service uses as part of a mixed use development. (Ord. 2462 § 17, 2006; Ord. 2447 § 31, 2005; Ord. 2108 § 6.3.4, 1996).

B. General Standards

| <u>ITEM</u> | <u>CITY CRITERIA</u> | <u>PROPOSED</u> |
|----------------------------|----------------------|-------------------------------------|
| Maximum Density: | 29 du/acre | 23.33 du/acre (7 units in 0.3 acre) |
| Minimum Lot Size: | None | NA* |
| Max. Impervious surface: | 50% | 50% |
| Min. Pervious surface: | 50% | 50% |
| Max. Base Building Height: | 40 feet | 45 feet |

| <u>SETBACKS</u> | <u>CITY CRITERIA</u> | <u>PROPOSED</u> |
|-----------------|----------------------|-----------------|
|-----------------|----------------------|-----------------|

| | | |
|--------------------|---------|---------|
| Front Yard (North) | 10 feet | 10 feet |
| Rear Yard (South) | 20 feet | 20 feet |
| Side Yard (East) | 5 feet | 5 feet |
| Side Yard (West) | 5 feet | 5 feet |

Staff Analysis:

As currently proposed, the application complies with the development standards.

C. Building Height (IMC18.07.060)

Additional discussion is being provided for the building height standards in this staff report because the proposed height at approximately 46 feet exceeds the maximum base height of 40 feet allowed for the zoning district. An Administrative Adjustment of Standards will be required to allow the building to exceed the base height per IMC18.07.355 (A). The elevator and stair tower is included in the height exceptions, specifically called out in IMC 18.07.060.4 (g) and (h).

IMC 18.07.855.A provides the criteria to allow building heights to be increased up to 10 feet above the base height in the zoning district. The base height of those structures may be adjusted by the Planning Director/Manager through an Administrative Adjustment of Standards, if all of the following criteria are met in addition to the criteria for Level 2 Review:

1. Building Design:
 - a. The adjustment of height will enhance the architectural design by:
 - (1) Modulating the roof of the structure through varied heights or pitches,
 - (2) The use of varied exterior materials, or
 - (3) Allowing parapets, gables, bell/clock towers or other features; and

Staff Analysis: The roof is broken up into smaller pitched roofs at various heights. The East and West Elevations show the cascading roof form. The most critical side of the building is the rear, given the single-family zoning across the alley from the project site. This side of the building is well modulated, as shown in Figure 1, East Elevation. The elevation drawing also shows the variety of building materials, including windows and balconies, that are used to further articulate the building to address the human scale. Figure 2 shows the view of the building from the single-family properties across the alley. The tallest roofs are significantly set back and will not be visible from the alley because the building. While the height of the elevator and stair tower exceeds the maximum height allowed, this is exempt from the height limit per IMC18.07.060.B.4 (g) and (h). The elevator and stair tower should be further refined to mitigate the experience of its height from Sunset Way (see Figure 3), such as creating a stronger “top”.

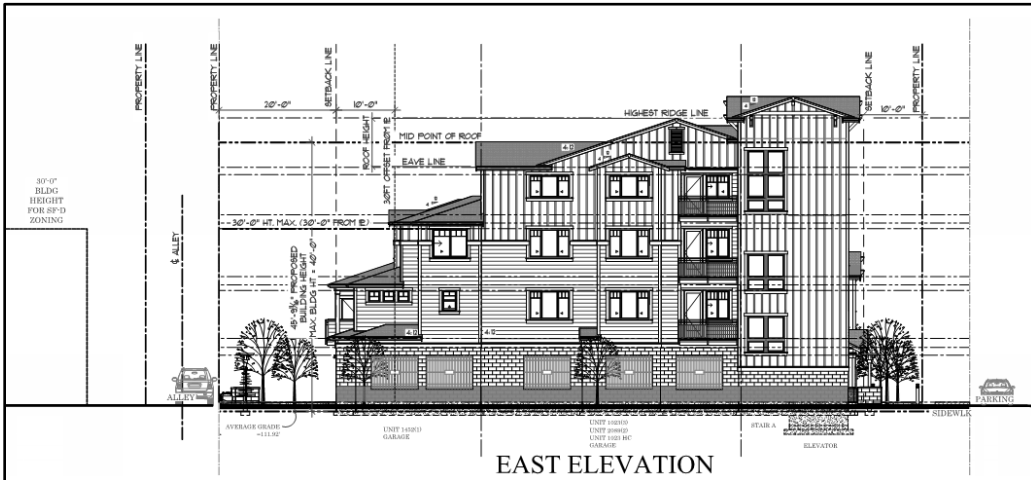


Figure 1. East Elevation showing relation of building heights from the less intense single-family use across the alley (left) and the proposed multi-family building.



Figure 2. View across the alley from the single-family lots.



Figure 3. Sunset Way view.

- b. The gross floor area for each story above the base height is reduced by twenty-five (25) percent of the gross floor area of the story beneath it; or the percentage of pervious surface for the site is increased by ten (10) percent over the minimum for that zone; for example, a forty (40) percent pervious surface ratio for a site shall be increased to fifty (50) percent as a condition of approval for additional height;

Staff Analysis: The calculation demonstrating compliance with this criterion is on sheet A3.2 of the plans. The third floor, which exceeds the base height limit, has a floor area of 3254 s.f. The second floor has a floor area of 4334 s.f. (A quarter of 4334 s.f. is 1083.5 s.f. $4334 - 1083.5 = 3250.5$ s.f.). The third floor has to be adjusted to meet the floor area of 3250.5 s.f. or less.

- c. Design features, such as transparent windows and doors, artwork, fountains, street furniture, varied exterior materials, and/or landscape elements or plazas are used to give the ground floor of the building a pedestrian scale;

Staff Analysis: Transparent windows and doors are provided for the small lobby, approximately 15 feet wide. A variety of materials are proposed on the ground floor, including stone veneer, concrete masonry units and fiber cement board. Ground floor decorative elements include the double-column wood and stone posts that support the entry canopy and a small porch in front of the garage window grills. A window is shown for the northwest corner where the storage room is located. It is not clear whether this will be transparent or a decorative element designed to look like a window. Staff recommends that this storage room be replaced with a use that allows for a real window to be installed at this corner of the building.

- d. Approved street trees are incorporated into the landscape design for the project;

Staff Analysis: Street trees, at 30 feet on-center, are shown on the back side of the sidewalk. The location of the street trees is consistent with the intended design of Sunset Way right-of-way.

- e. Highly reflective glass shall not exceed seventy (70) percent of the length of the first floor adjacent to pedestrian way;

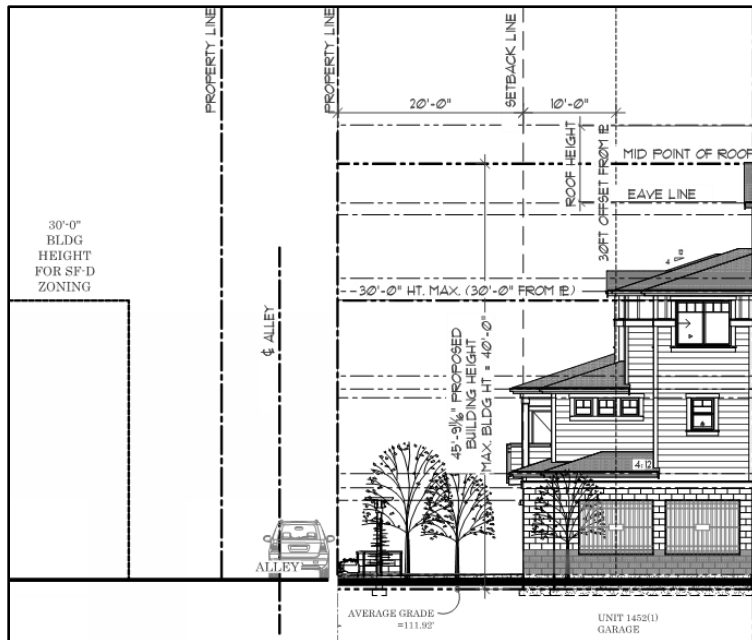
Staff Analysis: No reflective glass is proposed.

- f. Solid walls on the first floors of buildings shall not exceed twenty (20) feet in length and shall be softened by a combination of design details, modulation and dense landscaping; and

Staff Analysis: Design details for the solid wall of the exterior of the storage room and the elevator shaft are not shown on the elevations. A landscape plan is not provided at this time so this criterion will be reviewed at the SDP submittal.

- g. When the building is adjacent to a lower density residential zone, the maximum building height for the first thirty (30) feet from the property line shall be the maximum base building height of the adjoining lower density zone.

Staff Analysis: The properties across the alley from the project site is zoned at the lower density of SF-D, Single-Family Duplex. The maximum height allowed for the SF-D zoning district is 30 feet. The height of the portion of the building within 30 feet of the rear property line is within the base height limit of 30 feet, as shown on Sheet A4.1, East Elevation and Sheet A4.2, West Elevation.



2. Consistency with the Shoreline Management Program: If located within a Shoreline Management District, the height shall not exceed the limitations of the Shoreline Management Program.

Staff Analysis: Not applicable.

3. Sun and Shadow Analysis: Shadows created by the additional building height and bulk will not adversely affect the surrounding area. The Planning Director/Manager may require a sun/shadow analysis in order to determine if this criteria is met.

Staff Analysis: Sun and shadow analysis has not been required at this time. The building is set back from the side property line by as much as 16.5 feet on the east side and 18 feet on the west side. Furthermore, the west elevation is amply modulated vertically and horizontally, so that the upper floors step back further. A sun and shadow analysis may be required to determine the impacts of the elevator tower and the east elevation to Sunset Way and the adjacent property to the east.

4. Views: The taller structure will not significantly obstruct scenic corridors. The Planning Director/Manager may require a view analysis study in order to determine if this criteria is met.

Staff Analysis: There are no scenic corridors associated with this property; however, significant views of the hillsides from the properties across Sunset Way may be blocked by the building. Staff determined that if the building stayed within the base height of 40 feet, it would have blocked the scenic view and the additional 6 feet of height does not exacerbate this future condition.

D. Nonmotorized facilities (IMC 18.07.080)

All new multifamily residential and nonresidential developments are required to provide nonmotorized facilities in accordance with IMC 18.07.080 B. These developments shall provide connections of required facilities with any existing public nonmotorized facility and/or provide a stub for future connection to any proposed public nonmotorized facility as documented in the Issaquah Comprehensive Plan.

1. Pedestrian Facilities:

- a. Sidewalks: Any required sidewalks in public rights-of-way shall be provided consistent with the adopted and/or amended Issaquah Standards and Specifications: Streets and Related Work.

Staff Analysis:

The existing sidewalk along the south side of Sunset Way is 6-foot wide. The sidewalk is flushed with the roadway. The future streetscape plan for Sunset Way, as noted in the discussion of the Comprehensive Plan above, will require a 17-foot wide combined sidewalk and bike lane along the south side of Sunset Way. The timing for the construction of the streetscape improvements is still to be determined. City staff will update the Development Commission at the time of the public hearing whether the applicant will be required to construct the frontage improvements or an alternative arrangement will be required instead.

- b. Walkways: Barrier-free walkways providing the most direct route through a development shall be provided between public entrances and the nearest public sidewalk, trail, or shared use corridor.
 - (1) Walkway Connection Frequency: One walkway from a nonresidential or multifamily building to a public sidewalk is required in all instances with at least one (1) additional walkway required to the public sidewalk for each two hundred fifty (250) feet of street frontage.
 - (2) Multiple Building Walkway Systems: Not applicable to this proposal

- (3) Large Building Walkway Systems: Subsections (B)(1)(b)(2)(A), (B), (E), (F) and (G) of this section apply to individual buildings of fifteen thousand (15,000) square feet or greater in addition to other requirements of this section.
- (4) Size: All walkways must be at least five (5) feet wide, excluding vehicular overhang, displays, and storage.
- (5) Materials: All walkways must be composed of materials that are permanent and visually distinctive from parking lot paving. Said materials shall also be consistent with ADA access.
- (6) Safety: All walkways must be physically separated from vehicular area by grade, landscaping strips, berms, barriers, curbs or similar means, provided in a manner that retains pedestrian visibility.
- (7) Lighting: See IMC 18.07.107, Outdoor lighting, for minimum lighting requirements.
- (8) Transit Access: A walkway connection to the public sidewalk must be made within one hundred fifty (150) feet of an adjacent bus stop.

Staff Analysis:

The Walkways standards are meant to ensure pedestrian facilities are provided within the site to encourage walking and pedestrians have safe, convenient and easy access to the building/s from the street. The proposed multi-family building has one entrance from Sunset Way and one entrance from the garage. The street entrance is provided with a 5-foot wide walkway. The project will not be required to provide an additional connection to a bus stop. The Sunset Way sidewalk provides connection to the existing bus stop a block east, in front of the multi-family building at 487 Sunset Way. Sidewalks and walkways will be further reviewed for compliance with the above standards as part of the construction permit review.

2. Bicycle and Shared Use Nonmotorized Facilities:

- a. Bicycle Lanes: Any required bicycle lanes in public rights-of-way shall be provided consistent with the adopted and/or amended Issaquah Standards and Specifications: Streets and Related Work and the Comprehensive Plan Bicycle and Shared Use Corridor Map.
- b. Shared Use Corridors: All shared use corridors shall comply with all applicable requirements for walkways in subsection (B)(1)(b) of this section. Shared use corridors shall be marked with directional signs to indicate shared use in a manner consistent with IMC [18.11.170](#). Shared use corridors shall be at least ten (10) feet wide.
- c. Bicycle Parking: Bicycle parking shall be provided consistent with IMC [18.09.030\(I\)](#), Bicycle Parking. (see staff analysis under "Parking")

Staff Analysis: A cycle track, or protected bike lane, is planned for the south side of Sunset Way. The image below provides a general idea of how the cycletrack will

affect the streetscape of Sunset Way. The plan is for a two-way cycletrack with a 6-foot wide sidewalk along the south side of Sunset Way. It is unlikely that this development will be required to construct the cycle track for their portion of Sunset Way since a bike route along Sunset Way needs to be continuous and consistent in design in order to be functional. Staff will update the Development Commission at the time of the public hearing of the final requirements for frontage improvements for this project.



Source: peopleforbikes.org

E. Multi-family Development Standards (IMC 18.07.440)

- A. **Purpose and Intent:** The purpose and intent of requiring specific standards for multifamily and duplex development are to:
1. Ensure multifamily and duplex residential development which is compatible with the scale and character of the neighborhood or community in which it is located;
 2. Provide a safe source of housing for residents of the City which includes amenities such as usable open space;
 3. Provide the opportunity for urbanized lifestyles that would be located close to a range of urban services;
 4. Encourage a sense of community within each cluster and within each overall development; and
 5. Provide that impacts to public services and facilities are mitigated.

Staff Analysis:

Design standards in the Olde Town for Multi-family development and the development standards for the MF-H will ensure that the proposal will be compatible in scale and character to the multi-family zone along Sunset Way and the properties across the alley from the project. The 50% pervious area requirement will be designed as usable open space and a sense of community for the residents of the apartment.

Item 3 above implies that the character of the multi-family zones is intended to be more urban. The design of the ground floor of the building and the Sunset Way

streetscape requires additional refinements to create a more urban character. Currently, the main design feature that engages the street is the walkway and entrance canopy to the building lobby. The ground floor is occupied by a storage room, the garage wall and to a lesser extent, the elevator shaft. This treatment is not consistent with an urban character, which typically has more windows and active spaces on the ground floor and the street. Staff recommends that the applicant consider locating an active use instead of the storage space, such as a workshop, gardening room or a kitchenette for outdoor social activities in summer months. This would allow windows to be provided at the ground level. The streetscape could have benefited from a plaza or outdoor seating area along the Sunset Way facade but the site is constrained by the 50% pervious area requirement, and the IMC does not allow for an administrative adjustment to the impervious area standards.

B. Diversity:

1. Unit Type: Diversity of unit types is encouraged, including, but not limited to, senior housing, special needs and the low income rental market as defined by current King County Standards [Countywide Planning Policies];
2. Location: Multifamily shall be considered a desirable component of mixed use developments and shall be encouraged.

Staff Analysis:

The proposed apartment is a mix of one-, two-, and three-bedroom units. Given the size of the development, the proposal provides a relatively diverse mix of units.

C. Permitted Locations and Review Required: The permitted locations and review required for multifamily and duplex development are governed by the ***Table of Permitted Land Uses*** (IMC 18.06.130).

Staff Analysis:

The proposal for a 7-unit multi-family apartment building is consistent with the zoning district for the property. The properties along Sunset Way between the I-90 ramp and Front Street in downtown Issaquah has been evolving into a more intense land use from its former single-family use, as intended by the Comprehensive Plan vision for this area.

D. Approval Criteria: Approval for all proposals for multifamily or duplex developments shall be permitted only if all the following approval criteria are met:

1. Access and Circulation:
 - a. Motorized: Vehicular access shall be provided such that it does not negatively impact adjacent land uses. Internal circulation shall also be provided, such that it does not interfere with pedestrian access or internal circulation;
 - b. Nonmotorized: Pedestrian walkways shall be provided within a project and as linkages to adjacent projects.

Staff Analysis:

Vehicular access is through an existing alley where it will have the least impacts to Sunset Way. The driveway width is also reduced to the minimum required, at 17 feet, and has a length of 20 feet, allowing for ample depth for a vehicle parked on the surface parking space to back out, if necessary.

Pedestrian walkways to the building from Sunset Way and the alley are limited due to the impervious area limitations of the site. There is an existing 6-foot wide sidewalk along Sunset Way that leads directly to the building entry from the street. The City's future plans for Sunset Way indicate that a 17-foot wide sidewalk combined with a cycletrack (protected bike lane) will be installed on the south side of the right-of-way.

2. Building Modulation: Building modulation shall occur on all multifamily structures, including townhouses and residential development associated with residential mixed use development and is intended to break up the overall bulk and mass of the exterior of a multifamily building. Modulation should also add character to the overall building exterior as well as to individual units.
 - a. Building facade modulation should occur at every twenty-five (25) feet of wall length. The modulation can take the form of decks, balconies, indentations, extrusions and other various forms; and
 - b. Minimum modulation depth shall be approximately three (3) feet; and
 - c. Minimum modulation width shall be approximately eight (8) feet.

Staff Analysis:

The building design meets the dimensional requirements for modulation. This will be reviewed again during the SDP process (See sheets A 4.1 and A4.2 of the attached plan set for elevation drawings).

(Criteria 3 is for duplexes and does not apply to this project)

4. Parking: Parking shall be determined for multifamily developments, as established in the Table of Off-Street Parking Standards (IMC [18.09.050](#)).

Staff Analysis:

The proposal complies with the parking requirement. Table of Off-Street Parking Standards (IMC 18.09.050) indicates that multi-family residential projects with units other than studio apartments are required to provide 2 spaces per unit. The project proposes 7 dwelling units; 13 spaces are provided in a garage, including one van accessible stall, and one space located outside of the garage, for a total of 14 parking spaces.

Per IMC 18.09.090.H, Compact Stalls, 60% of parking spaces provided may be compact stalls. This project is proposing a total of 14 spaces; therefore, up to 8 stalls can be compact spaces. Currently, the site plan shows 5 compact stalls inside the garage and one in the rear yard.

Bicycle parking is required per IMC 18.09.030.I at the ratio of 5% of the required automobile parking spaces for the first 300 required auto stalls. Therefore, a minimum of 1 bicycle space is required. Bike parking is provided in the garage, next to the storage room.

Per 18.09.110, no loading spaces are required for multi-family developments. However, a space for moving trucks near the elevator should be provided. It is expected that moving trucks will park on the street. However, when the new Sunset Way cycletrack is constructed, parking along the south side of the street will be removed. The applicant should identify an alternative plan for moving trucks to park and access the elevator.

5. Private or Common Usable Outdoor Space: Every multifamily development, including townhouses, shall provide private outdoor space for individual units or a larger common, outdoor space for the residences. The purpose of requiring usable open space is to provide this amenity for the residents of these developments. The usable outdoor space, whether provided on an individual or community basis, may consist of pervious and impervious surfaces.
 - a. Common Outdoor Space: Common outdoor space shall be provided in the form of one or several outdoor balconies, patios, decks or gardens. Common outdoor space shall be easily accessible to all residents of the complex and no common outdoor space shall be attached to any individual unit.
 - b. Containment of Activity Areas: Activity areas shall be designed so that they do not interfere with incompatible on-site uses; for example, children's play areas shall not overflow into parking lots or pedestrian pathways.
 - c. Minimum Outdoor Space per Unit: At a minimum, each unit shall have a total of forty-eight (48) square feet of outdoor space, whether provided for individual units as private outdoor space or as common open space.
 - d. Private Outdoor Space: Private outdoor space shall be provided in the form of private outdoor balconies, patios, or decks attached to individual units. Minimum length and width of each private outdoor space shall be eight (8) by six (6) feet.

Staff Analysis:

Each residential unit is provided with a balcony ranging in size from 60 s.f. to 70 s.f., with a standard depth of 6 feet. The balconies are located where they will not be impacted negatively by any activities that are incompatible with the residents' enjoyment of the outdoors from their units.

6. **Roofline Variation:** Roofline variation is intended to break up the overall bulk and mass of a multifamily building. Roofline variation shall occur on all multifamily structures with rooflines which exceed fifty (50) feet in length. Roofline variation shall be achieved using one (1) or more of the following methods:
 - a. Vertical off-set ridge line;
 - b. Horizontal off-set ridge line;
 - c. Variations of roof pitch; or
 - d. Any other technique approved by the Planning Director/Manager which achieves the intent of this section.

Staff Analysis:

The roof is well-modulated and broken into smaller roofs. The required rear setback from the property line for building height above 35 feet also created a softer edge along the alley side of the property (see staff discussion about height and setbacks).

7. **Screening:**
 - a. **Parking Area:** The parking area shall be screened to visually buffer areas within the project complex and adjacent properties;
 - b. **Structures:** Screening of structures from adjacent properties shall be provided, such as landscaping, fences, berms or other similar materials and/or designs. (Ord. 2471 § 5, 2006; Ord. 2108 § 7.4.8, 1996).

Staff Analysis:

It is not clear what the intended treatment of the garage walls is. No screening from adjacent properties is shown on the side yards. The one parking space outside of the garage is screened with a trellis element and a low wall. The proposal shall be required to meet both the intent – to provide buffer between adjacent uses – and the specific screening requirements here and in the Olde Town Design Standards.

F. DESIGN STANDARDS

There are two sets of design standards that govern the project site:

- Appendix 2 of IMC 18.07, Design Standards
- Appendix 3 of IMC 18.07, Design Guidelines for the Cultural and Business District of Issaquah

Staff analyzed the proposal for compliance to the design standards at the pre-application phase. In general, the proposed design complies with the requirements for:

1. Building modulation and application of architectural details to reduce the scale of the building
2. Clearly-defined building entry
3. Pitched roofs with a min. 4/12 slope and gabled roof facing a street
4. Driveway and garage access in the alley (driveways are prohibited from Sunset Way)
5. Screening of parking space from residential units across the alley
6. Walls facing Sunset Way shall be modulated with bays and recesses at least 8 ft. wide and no more than 25 ft. wide and at least 3 ft. deep.
7. Windows facing street have substantial trips and vary in size
8. Durable exterior materials include concrete masonry units at the base and fiber cement board for siding. The lap siding and board and batten siding provide texture, pattern and articulation to the building façade.

Some elements, to be reviewed at the Site Development Permit phase, include:

1. Front yard provided transitional elements: location of trellis element; covered porch and other ground floor treatment – how they relate to the sidewalk
2. Welcoming building entry
3. Pedestrian-scale lighting where walkways are provided
4. Garage walls and blank walls
5. Landscape screening required
6. Colors of exterior materials

LAND USE ITEMS TO BE REVIEWED AT SITE DEVELOPMENT PERMIT

The project shall comply with following additional requirements. Compliance with the City's standards will be determined during the Site Development Permit:

- A. Environmental Review:** SEPA review is required for construction of buildings greater than 4,000 SF. An environmental checklist will be required with the land use permit application. A site access analysis will be required for the driveway entries and channelization improvements.
- B. Parking:** The project will need to meet the requirements of section 18.09, Parking. Staff review of parking for the Community Conference was limited to compliance with the required minimum parking. Other standards, including size of stalls, drive aisles, and construction details will be reviewed and approved at the Site Development Permit and construction permit reviews.

C. Lighting: The project will need to meet the requirements of the Outdoor Lighting Standards, IMC 18.07.107 with regard to exterior lighting of the parking lot, the plazas and the building. The requirements for Low Density Residential/Multifamily Residential would apply. A lighting plan, meeting the submittal requirements of IMC 18.07.107(D) will need to be submitted.

D. Landscaping and Tree preservation: The landscape plan will need to meet the requirements of the City's Landscape Code, IMC 18.12. Some of these requirements include an evaluation of the worthiness to preserve existing trees and other landscaping on site; a soil analysis and evaluation of the practical use of the existing soil; a water budget analysis and irrigation system design.

A preliminary landscaping plan will be required with the Site Development Permit submittal. Existing trees that will be preserved will need to be identified with the SDP plans. The exact location, type and planting specifications will need to be submitted with the Site Development Permit. The landscaping should complement the views and focal points of the building and site and the parking garage should be enhanced with landscape areas to buffer the views of the garage from abutting residential properties. Right-of-way landscaping will also be required.

Staff Note:

No existing trees are shown on the plans; however, the aerial photo of the site shows several small trees on the property. The project will be required to submit a tree retention plan and shall be required to meet the tree density requirements. The removal of trees will be mitigated with new trees.

The site plan will either need to be revised to provide further tree retention or an Administrative Adjustment of Standards will need to be applied for and granted to compensate for inability to comply with the tree retention and tree density requirements.

Section 18.12.1385 (A) of the Landscaping and Tree Preservation code requires with a commercial development, that 25% of the total caliper of all significant trees in developable site area is retained. Section 18.12.1385(B) allows a reduction of up to 50% of the tree retention requirements through a Level 2 Review process (Administrative Adjustment of Standards).

E. Waste Facilities & Recycling:

The waste and recycling facilities will need to be properly screened and appropriately sized. The location shall require approval from CleanScapes/Recology.

F. Signage:

No signage is currently proposed. Any signage requires a separate sign permit.

OTHER CITY REQUIREMENTS (Outside of the Land Use Code)

A. Street Improvements

Staff is currently working with Public Works Engineering staff to determine the appropriate requirements for this project. Staff will provide the Development Commission an update at the time of public hearing for the SDP.

B. Storm Drainage

Storm drainage will be required to follow the applicable King County Surface Water Design Manual. All runoff from the site will be infiltrated onsite. Roof runoff will be collected in downspouts and conveyed to underground storage facility. Water quality requirements shall be met for all runoff from all driving surfaces.

C. Utility Improvements (Water and Sewer):

The project will be required to meet all current City of Issaquah standards. Location of existing utilities, as well as proposed new utilities will need to be shown on the civil drawings submitted with the Site Development Permit. Compliance with the City's standards will be determined during the Site Development Permit. None of the utility improvements will have a big impact on the site plan, as proposed.

- A. Power: Existing powerline shall be relocated underground.
- B. Sewer: Since the project has less than 10 units, the existing sewer may be used if it is a 6-inch diameter line and in good working condition.
- C. Water. Water supply for fire department use and for the residential units will be required.

D. Fire access: Fire trucks can park on Sunset Way or access the rear of the building through an existing alley. A concrete walkway from Sunset Way allows access to the sprinkler room located near the front of the building.

E. Impact and Mitigation Fees:

The developer will be required to pay Impact and Mitigation Fees. Impact fees are required at the Issuance of Building Permits and by the mitigation/impact fee schedule in effect at that time.

Attachment:

1. Application, COM15-00003, received 9/16/2015.
2. Project Narrative, received 9/16/2015.
3. Plans, elevations and renderings, received 9/16/2015.