



## AGENDA

### Development Commission

**7:00 PM - Wednesday, September 2, 2015**

Council Chambers, 135 East Sunset Way, Issaquah WA

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**1. CALL TO ORDER 7:00 PM**

- a) Commission Membership

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**2. APPROVAL OF MINUTES 7:05 PM**

- a) Meeting Minutes from August 5, 2015

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**3. AGENDA ITEMS 7:10 PM**

- a) **PUBLIC HEARING: Fieldstone Memory Care**

*Presented by:*

*Jennifer R Woods, Associate Planner*

**4. OTHER BUSINESS / ANNOUNCEMENTS 8:45 PM**

**5. ADJOURNMENT 9:00 PM**

**INQUIRIES**

Please contact Kathe Geyer (425) 837-3100 or [kathleeng@issaquahwa.gov](mailto:kathleeng@issaquahwa.gov).

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Meeting room is wheelchair accessible. American Disability Act (ADA) accommodations available upon request. Please phone (425) 837-3000 at least two business days in advance.

Note: Times listed for meeting topics are approximate and items are subject to being shifted from the original order.



# Development

## About

Created in 1983, this commission reviews all land use actions requiring a Level 3 review. The Commission further serves as an advisory board to the City Council on land use actions requiring council approval (Level 5 review).

The appearance of fairness doctrine prohibits Development Commission members and City Council members from discussing the merit of specific land use development applications outside of the formal public meeting process. Citizens, however, may discuss any issue with the City’s Development Services Department. Written comments are also welcome.

## Membership

The Development Commission is comprised of seven regular members, with four-year terms; and several alternates, with two-year terms. All members are appointed by the Mayor and subject to confirmation by the City Council. Terms expire April 30 of the year listed. For more information, see [IMC 18.03](#).

## Contacts

### Staff Liaison

Christopher Wright, Project Oversight Manager

[Email](#)

### Regular Members

2016 – Melvin Morgan, Jr.  
2016 – Carl Swedberg  
2018 – Essie Hicks  
2018 – Raymond Leong  
2018 – Richard Sowa  
2019 – Michael Brennan  
2019 – Randolph Harrison

### Alternate Members

2016 – Vacant  
2016 – Vacant  
2017 – Vacant  
2017 – TJ Ginthner



**CITY OF ISSAQUAH  
DEVELOPMENT COMMISSION  
MINUTES**

August 5, 2015

City Hall South  
Council Chambers

135 E. Sunset Way  
Issaquah, WA 98027

**COMMISSIONERS PRESENT**

Randy Harrison, Chair  
Michael Brennan  
TJ Ginthner, Alt.  
Essie Hicks  
Raymond Leong  
Melvin Morgan, Jr.  
Richard Sowa  
Carl Swedberg

**STAFF PRESENT**

Amy Tarce, Senior Planner  
Christopher Wright, Project Oversight Manager  
Lucy Sloman, Land Development Manager  
Doug Schepp, Project Engineer/Consultant

**CALL TO ORDER**

HARRISON, Chair, called the meeting to order at 7:01 PM. He explained the purpose of tonight's meeting and the agenda for the meeting, including the opportunity for public comment. He encouraged audience members to sign up on the sign-up sheet if they wish to speak, and to identify themselves when speaking. HARRISON asked that speakers limit their comments to five minutes.

**APPROVAL OF MINUTES**

MOVED BY MORGAN, SECONDED BY SOWA that minutes of the Development Commission meeting on June 24, 2015 be approved as amended. MOTION CARRIED UNANIMOUSLY.

**PUBLIC HEARING: Issaquah Gateway Apartments**

**Seeking approval of SDP15-00002, Issaquah Gateway Apartments, for a multi-family residential development consisting of 400 stacked apartment units on 29.85 acres; consisting of 16 three-story buildings and two five-story buildings with four floors of residential units and garage parking on the ground floors, located at 2290 Newport Way NW.**

**Staff Presentation**

Amy Tarce made staff's presentation. She said tonight is the first of two public hearings on the Gateway Apartments project before the Development Commission, and said her goal tonight is to introduce the project and provide an opportunity to ask questions and solicit comments. No final decision will be made by the Commission on the project tonight.

She continued that the Administration has determined that the project is generally compliant with the Site Development Permit, with some conditions. She continued her remarks on the application, focusing on three themes: *connectivity*, *quality open spaces*, and *placemaking*. She noted that the Notice of SEPA Determination was published on July 30, 2015 and the comment and appeal period ends August 20, 2015. She also referred to the citywide road and crosswalk safety studies that are under way concurrently with this project application.

She described the location of the project and the surrounding parcels, and showed it on a site map. She said it is located in the Western Gateway subarea of the Central Issaquah District, and read the Western Gateway mission from the Central Issaquah Plan. She continued with more description of the proposed Gateway Apartments, as given in detail in the staff report beginning on page 3 of 83. She said phase two of this project, which is not part of this project, will be presented to the Development Commission at a later date. She showed photos of the existing conditions of the project site, including views from the site.

She said the Administration has determined that the proposed project meets the required zoning district standards, as shown on page 10 of 83. She described the proposed Administrative Adjustment to the building height requirement for this project, which will require a decision by the Council. She continued her remarks about the land use and zoning (VR—Village Residential) of the property and the properties surrounding it.

She described the comments received to date from the public, which are included in the agenda packet. She explained a graph of the density per acre of the proposed Gateway Apartments and the nearby Sammamish Pointe Condos, Spyglass Hill Condos, and Bentley House apartments.

She continued her presentation with a description of how the following project elements meet the requirement of the Central Issaquah Plan:

- how the circulation facilities will function;
- how the proposal fits into the regional roadway network;
- where two types of community spaces will be located, including *significant spaces* (a voluntarily included new neighborhood park, and a required shared-use route) and *required (private) community spaces*.

She showed drawings and renderings of what the apartment buildings would look like, including how the development would integrate into nearby green spaces and the I-90 green edge of the property. She discussed the natural edges (wetlands and stream buffers) on the western edge of the property, including enhancements to the buffer. She said all the edges of the property have some green spaces or green edges that are integrated with the specific property and the surrounding property in general.

She continued with a diagram of how the circulation facilities and open spaces work together to connect the property with existing features outside the property, such as bike lanes, as well as the new features proposed as a result of the project. She briefly explained how phase two of the project would contribute to these features. She showed more views from the site of the surrounding natural areas.

She discussed elements of the urban streetscape in the application, including the inclusion of streetwalls; examples of architectural elements that would be acceptable to meet the build-to line; ground-floor treatment of the buildings; pedestrian comfort and safety; screened parking; and modulation and articulation in support of pedestrian-scaled buildings, including early-stage drawings. She referred to the 91 conditions the Administration has recommended on these elements in the project application, as outlined in more detail in the staff report.

She concluded her presentation with next steps, leading to a tentatively scheduled decision by the Development Commission at its next meeting on August 26, 2015. She also provided two corrections to the information given in the agenda packet, on page 55 and on Condition 6. She also noted three more comments from the public were received since the packet was distributed (Comment 9, 10, and 11) and distributed them to the Commission and for the record.

### **Applicant Comments**

Greg Van Patten, The Wolff Company, 911 East Pike Street, Suite 310, Seattle, developer of the Gateway Apartments project, gave background about his third-generation business and its deep roots in the Northwest and Spokane specifically. He said this will be the tenth development Wolff has taken on in this region; those projects are in various stages of development and construction. He said we pride ourselves on being a high-quality, thoughtful developer, and we are excited to be part of the Issaquah community.

Matt Roewe, VIA Architects, 1809 7th Ave., Suite 800, Seattle, introduced the rest of the design team. He discussed the guiding principles and goals of the Central Issaquah Plan, which are key to what we are doing with this project; namely, community, connectivity, environment, and growth. He noted the project is not mixed use, just residential, and will create a real neighborhood and provide much-needed rental housing in the Issaquah community. He gave additional details about the site from his perspective, including the proximity of I-90, the “gateway” nature of the site, and the way it is bounded on both sides by creeks and wetlands.

He showed a diagram of the site and showed the surrounding natural features and existing development. He noted where significant trees and property will be left as pristine and untouched as possible, and where connections will be made to increase connectivity within the site. He continued with a discussion of the site opportunities and constraints, noting that the development will only take place on about half of the total site. He showed images including an aerial image of how the buildings will be organized on the site; a diagram where community space will be located within and adjacent to the development; where enhancements will be made to the buffer and critical areas, including restoration; an image of how a boardwalk will connect the property to the Rowley development; a diagram of the proposed neighborhood park, to be developed with the City’s Parks Department; and a site plan for the proposed interior community space targeted for the apartment tenants.

He discussed the challenges of developing parking on the site, and the efforts made to minimize the impact on the neighborhood street. He showed images of how parking will be screened by fences and trellises as well as landscaping. He noted the proposal has 64 percent impervious surface, well within the requirement, and explained how the parking design helped reduce the amount of parking required for the project. He showed a new rendering of the Village Green, a family-friendly open space for tenants, with entrances at ground level to the apartment buildings. He continued with remarks on the architecture of the project, including that the project inspiration was derived from the early character of Issaquah, including its agricultural history. He showed examples of the community clubhouse and how the character he just described would be carried forward to that building. He continued another architectural inspiration is Scandinavian villages, particularly the use of color to avoid monotony and differentiate buildings in a restrained way. He showed renderings of building design details, including entrances, berming to screen parking, and a color board.

He discussed the issue of seeking an adjustment to the building height restriction, and his hope that a change can be approved to allow the use of pitched roofs in the design rather than flat roofs. He showed more graphics of the site views and sight lines, particularly from I-90 and from residential dwellings on the other side of Newport Way. He showed photos of views from Pine Cone Lane, Oak Ridge Drive, from I-90 looking southeast and northwest, from NW Pacific Elm Drive, and so on, both before and after development of the Gateway Apartments project.

He summarized it has been great working with City staff to find the right fit for this project with the Central Issaquah Plan and the Issaquah community.

## Public Comment

HARRISON opened the meeting for public comment at 8:08 PM.

Hart Sugarman, 2550 NW Oak Crest Drive, said he has been a resident of Issaquah for 19 years. He spoke about his concerns about the additional traffic this project will create on Newport Way. He said he did a quick calculation that the Gateway Apartments project will outnumber all the dwellings that already exist on the stretch of road from Oak Crest Drive to SE 54th. He continued Newport already has high traffic volumes, and adding more traffic plus the existing 40 miles per hour speeds that are allowed will create a huge problem. He said he would like consideration to be given to putting this project on the frontage road of I-90, and to ensure that no cars are allowed to park along Newport Way at any time.

Ghadeer Beghai, 1240 Oakwood Place NW, spoke about his concerns with access, noting that right now there is only one lane in each direction. Another lane will be needed for emergency vehicles and to facilitate people going in and out of the development, he said. He said the project also has to take into account that people will be going in and out at all times of the day, and that parking will spill over onto Newport Way, creating an unsafe situation. He also said that he would be interested in knowing more about the park facilities that are planned. Would the park be usable or accessible to residents only, or for others to use as well. In summary, he said his basic concerns are the increased traffic volume if two lanes in each direction are not available, and what kind of park would be developed.

Laura Millikan, 820 Front Street S. #204, spoke about her concern for the availability of low-income housing in Issaquah generally, and gave details about her experience trying to get on a list for low-income housing. She also said she favored encouraging Metro to increase transit service that would serve the development and the community generally. She suggested that the development landscaping in the playground area include shade trees and native plantings that are also edible, such as apple and fruit trees, grape vines, and so on.

Connie Marsh, business owner at 1175 N.W. Gilman Blvd., Suite B-11, and Issaquah resident, said this project does not really support the Central Issaquah Plan's vision in that residents of the Gateway Apartments will have to get in their cars to go anywhere. She continued this application should not go forward until the Newport Way conditions have been perused and studied. She noted that this plan would remove sidewalks from one side of the road, which is not allowed within the Central Issaquah Plan. She suggested including a more attractive interface with the WSDOT mitigation area, which currently is fenced with posts and wire. She said she did not see a condition that addressed allowing wildlife access under the proposed boardwalk, as discussed at the Rivers and Streams Board meeting. She noted the Central Issaquah Plan places a priority on using trees to create a Northwest feel, and the only ones proposed in this development are along I-90. Finally, she said the clubhouse building looks somewhat interesting, but the apartment buildings themselves do not evoke any specific sense of Issaquah's character, but rather could be apartments in, say, Renton or any other community.

Jim Lippincott, 2258 Newport Way, said he is a Sammamish Pointe condominium resident who lives close to the west entrance of Sammamish Pointe where the proposed interchange for the Gateway Apartments will be. He said his main concern is being able to continue to use the egress from the Sammamish Pointe development. Some consideration of a traffic circle has been discussed, he continued, and his concern is not losing the usability of the entrances and exits that Sammamish Pointe residents now have.

Toni Conforti, (no address provided) spoke about her concern about traffic and safety on Newport Way. She said the whole Newport Way corridor raises concerns about safety, particularly the

safety of children, and she encouraged the Commission and applicant team to carefully address traffic safety in the development design.

Jon Sheridan, 675 Jasmine Place NW, said the applicant seems generally pretty fair minded about the proposed development, and appears to have given a lot of careful thought to the proposal. He said he was glad to hear about preserving space with green trees, for example. But what are the ramifications for existing residents, he continued. He mentioned big housing projects that already exist at the Highlands, Talus, and now on Gilman, and said it seems like Issaquah has already done a lot to accommodate growth. Maybe we need to just slow down and catch our breath, he added. He continued with his concerns about the increased need for police, fire, schools, and so on, as well as the impact on property taxes. Why are we in such a rush, he added. It seems like this 50-year plan is being packed into a five-year plan. He spoke of his preference to keep Issaquah's charm and grow at a more reasonable pace. He said he does think there is genuine pride on the part of the applicant to be part of a good development, but urged the Commission and the City to think through the ramifications on existing residents.

Carol Lopez, 2262 Newport Way NW, said she has lived at the Sammamish Pointe Condominiums for about 17 years. She said she can see that a lot of care has gone into the planning for the Gateway Apartments development, and understands the growth that is taking place in Seattle and nearby communities like Issaquah. However, she continued, attention must be paid to the existing homeowners, who, unlike renters, pay property taxes. Existing homeowners have concerns about crime, traffic, congestion, and so on. She said in 17 years, she has watched an entirely new Exit 13 from I-90 be created and huge increases in traffic to the point where now commute-hour traffic moves at a crawl. She said she is trying not to be selfish, but her front door literally will face this proposed development. Our backyards have already experienced radically increased noise over the years from Newport Way. She said she hasn't heard any mention of any proposals for additional exits and entrances to I-90, so all the new traffic will end up on Newport Way. She said existing homeowners have worked hard to have a home in the City, and encouraged the applicant team and City staff to think about the needs of existing homeowners.

Hearing no additional requests to speak, HARRISON closed the meeting for additional public comment at 8:33 PM.

### **Commissioner Discussion**

BRENNAN said recently traffic on Newport Way has been getting lots of attention, and while solutions to problems on Newport Way may be outside the scope of this project, where is the City's thinking now about making improvements there. Sloman replied three to five traffic improvement projects have recently been proposed and are in various stages of discussion by the City. She said City staff has been looking at traffic issues in the Newport corridor, and the results of a study are expected that will take a look at both that corridor as well as other problem spots around the City. BRENNAN asked is there any capital funding available for a capacity project on Newport Way at this point, or will Newport stay the way it is now for the foreseeable future. Sloman replied we expect to have the results of the study soon, which we hope will help us prioritize projects to improve capacity. The study will include Newport Way; one of the proposals is to add a central turn lane, for example.

HARRISON suggested that Commissioners focus on one topic at a time, and for each Commissioner to continue with comments about traffic before moving to a new topic. BRENNAN continued his comments by asking about school bus access, as was brought up during public comment. Tarce said the School District has been involved in discussions about student access, pick-up locations, and so on for the Gateway Apartments project. She continued City staff has

reached out to the School District's Transportation Division, and they are aware of the project needs and are looking at options for bus stop locations.

HICKS addressed the access road for fire trucks, and asked has staff had subsequent conversations about fire truck access since the staff report was prepared. Tarce showed where fire trucks and other emergency vehicles will access the site on a diagram. She said staff has analyzed the emergency vehicle access for the site using a model, and would not be bringing it forward for your consideration without having gone through that process. The emergency vehicle access is adequate, she said. HICKS said so there is no issue with fire trucks being able to reach the open playground area as well as all the buildings. Tarce replied staff has done the modeling work, and no problem exists.

SOWA said his observation is that the project appears to be adding a lot more traffic to what is already a crowded Newport Way where traffic moves at relatively high speeds. He said it would probably serve the City well to look carefully at whether it might be creating future liabilities by adding traffic there.

MORGAN said he is hearing concerns being expressed about ingress and egress to the site, and also about adequate capacity on Newport Way for the additional traffic, but he assumes that the capacity is adequate or the project would not be going forward. Sloman said that is correct; access to the project site is a separate concern, and the traffic study will address how best to ensure ingress and egress to the site. MORGAN asked whether ingress and egress are part of the Development Commission's purview. Sloman said staff is interested in hearing your thoughts and observations, but the final determination is probably going to be an engineering solution based on a technical review. She continued that in studies done so far, staff is not seeing that Newport Way cannot handle increased traffic. MORGAN said so the increased volume of traffic on Newport Way will not be a deciding factor for the Commission in making a recommendation on this project. Sloman said that is correct, unless the traffic study doesn't support it. Tarce said the Central Issaquah Plan is based on higher densities than we are currently experiencing, and this project actually falls on the lower end of the FAR requirements in that Plan. She added that this project has already been accounted for in the current concurrency study.

LEONG said the Commission and the public comment tonight are focusing on traffic, and there are several important issues that the City should be looking at, especially on Newport Way. He said he heard staff's response about bus stops not being located in the development itself, but doing so would help alleviate some of the traffic on Newport Way. Sloman agreed, and said we are working with the Issaquah School District but they have very specific working conditions. She noted that staff will be consolidating all the comments we are hearing tonight into broad topics and will provide a written briefing response in the next two weeks or so. She encouraged anyone in the audience who would like to get a copy of the responses to leave their e-mail on the sign-in sheet or check the City's Web site.

SWEDBERG said he would like to know how many peak-hour trips this project will generate and how it will affect concurrency. Sloman clarified the simplified concurrency methodology the City now uses, and said staff will prepare a response in the briefing memo.

SWEDBERG asked what a "half-neighborhood street design" is, and noted that one of the conditions refers to the need for the Fire Marshall to approve the street design. Has the Fire Department seen this yet or not, he asked. Tarce replied that language is included to ensure that staff has the prerogative to seek approval from the Fire Marshall if the street design submitted by the applicant shifts significantly over the life of the application. The Fire Marshall has seen the version you are seeing tonight, she stated. She explained what "half-neighborhood street design"

means, and said it is a term that can be misleading and she will not continue to use it. HARRISON asked whether the appropriate fire and police authorities have reviewed the provisions for emergency access in the application for this project. Tarce replied yes, they have looked at every iteration of the site plan.

SWEDBERG asked for details about the hammerhead provisions for Fire Department vehicle access. Tarce showed it on a diagram and explained emergency access on the site. She noted that traditional hammerhead configurations tend to add to impervious surface totals and are single-use only. Sloman added the goal was to embed emergency access in other features so that access would not just be a single use.

SWEDBERG noted that the documents for the project refer to both pedestrian crossings that are of “a distinctive material” and “only striping of pedestrian crosswalks shall be allowed.” Tarce replied we can address that in the briefing memo. In some cases where actual striping is the only option, it will be allowed, but the intention is to make developments that fall under the Central Issaquah Plan more pedestrian-friendly, which requires something more “special” than striping.

HARRISON said he would like to have a conceptual understanding of the status of Newport Way at build out. He said he understands that traffic studies are under way now, but asked what does staff think Newport Way will look like when it has accommodated these 400 new units. Tarce referred to the requirements in the Central Issaquah Plan. Sloman added Newport Way will have a median and a turn lane at the intersections that provide access to Gateway Apartments. So the frontage of this property along Newport Way will include improvements and those improvements will transition across adjacent properties.

HARRISON asked whether the intersections at Newport Way and SR 900 are “green” or “red” in terms of meeting congestion standards. Sloman said we can address that in the staff response, although SR 900 is under WSDOT’s jurisdiction and are not “graded” in the same way.

BRENNAN commented that this project will result in a five-story building adjacent to I-90, and will make an important statement at the entrance to the City. He continued he doesn’t have any huge issues with the architecture as presented, but was hoping for something more striking. He said flat roofs are not acceptable in his view. He continued the buildings are somewhat interesting but do not really make a strong enough statement about the character of the City, nor are they particularly unique. He referred to the Group Health facility in Bellevue and other examples as visually more reflective of the Northwest, particularly those that are heavily timbered at the roofline. He said in general he would like to see more of Issaquah’s character reflected in the design, particularly as this development will serve as the gateway to the City and the Cascades.

HICKS said she agrees that a pitched roof is preferable over a flat roof. She said she would like to see more description of the wetlands identified in the staff report. She said she would also like to have a future conversation about a multi-use trail in lieu of sidewalks, as mentioned in the public comments. She asked the developer to reconsider item 22 on page 23 (“Include annuals at strategic locations such as the community center, high-volume pedestrian paths and at building entries”), and spoke in favor of integrating some shade trees and edible plantings in the design. She spoke in favor of using striping for crosswalks, referring to a recent tragic accident at a crosswalk, and said striping is what drivers are accustomed to seeing. She closed her remarks by noting that August 26 is too soon for her, as a Commissioner, to feel comfortable with the level of review she can give the material, and that August is also a time many members of the public are on vacation. She said she would like the public to understand what the Development Commission’s role is in reviewing this application, as well as the role of the other boards and commissions, such as the Rivers and Streams Board. She said she would like to ensure ample time is provided for all

parties, including the public and the Commission, to review the material, including the SEPA review which will not be available until August 20.

SOWA asked for clarification of the chart depicting dwelling units per acre in the staff report. Tarce explained how the calculations were made for the Gateway Apartments project. Sloman said staff will include that clarification in the staff response.

MORGAN asked for clarification of the number of parking spaces referred to on page 37 of 83 and elsewhere. Sloman said staff will clarify that in the staff response. He asked what the phrase “must be located...” means on page 19 of 83, which states that Building 17 must be located along I-9 to provide a sound barrier. Tarce replied there are many good reasons for it to be located there, but agreed that the phrase “must be there...” is somewhat misleading.

MORGAN said shared use trail lighting is referred to in condition 38, but noted he couldn’t find anything specific in the application that addressed lighting on the shared use trail. Tarce said there are no standards for that, and we would coordinate that level of detail with the Parks Department in the next stage of planning. Sloman agreed, and said trail lighting would require a discussion between Parks staff and the City biologist to maintain adequate safety but not negatively impact the critical areas.

MORGAN asked for clarification of the shared use trail. It appears to end at the Rowley property line in the diagrams provided in the agenda packet, he noted, and asked will it extend across Tibbetts Creek. Sloman replied the City cannot require the developer to build on other private property. That said, the applicant has had meetings with Rowley, and the City’s intent is to build all the way to 19th, which Rowley has indicated it is open to doing. MORGAN said could the City exercise eminent domain if necessary. He added this is a critical piece of this development project that represents a huge benefit to the public. Sloman said she will do some research and get back to the Commission.

HARRISON said he recalled some significant discussion about a pedestrian overpass over SR 900 around Newport several years ago. The concept was to allow bicyclists and pedestrians to get over SR 900 into downtown and to the transit center. Has there been any movement on that idea, he asked. Sloman replied it has been discussed with Rowley, but she is not aware of any project under consideration right now that would address it. She added it seems unlikely because of the need to include ADA-compliant ramps, and it’s not likely that the ramping required could get up that high.

MORGAN said he agrees with other Commissioners that flat roofs next to I-90 would not be desirable. He referred to Condition 64 on rooftop gardens (“Consider providing rooftop gardens for the two taller buildings...”) and noted that conditions that use the term “consider...” don’t really mean anything. He referred to an Administrative Adjustment of Standards on page 18 for sidewalks, and noted that Ms. Marsh said in her comments that such an adjustment is not allowed under the Central Issaquah Plan. Sloman said the priorities Ms. Marsh referred to are guidelines, which are taken under consideration in design and planning. Tarce described the property in question, and showed where the shared use route would be located and why it makes sense for the overall project design. MORGAN said he feels it is important to maintain eight-foot-wide stalls. There is no compelling reason to reduce them to seven feet, particularly as the sidewalk requirements are already being reduced from 12 to ten feet. Tarce said staff will look into that and address it in the staff response.

MORGAN asked clarifying questions about how the building height will be measured, and asked whether the existing grade will be used as the default for measurement. Tarce said staff is looking

at options, including case studies from other jurisdictions. MORGAN gave examples of how the measurement could be manipulated to allow exceeding the building height regulations. Sloman said staff shares that concern, and is looking at other options, including focusing on the sidewalk and road grades as the basis for measurement.

MORGAN said the appearance of the buildings and roofs next to I-90 is very important, and the design displayed tonight looks inexpensive and reminiscent of World War II buildings. He noted the lack of eaves and use of board-and-batten siding in the design, and said adding eaves would make a big difference.

MORGAN asked the applicant team whether they would like an opportunity to respond to any of the Administrations conditions. Jim Bodoia, VIA Director, said his team has not had an opportunity to thoroughly study all 91 conditions and so he can't speak to all of them definitively today. However, he continued, he would like to leave the door open for further design discussion, particularly with a literal interpretation of "tripartite." He said he thinks the design treats the base, middle, and top of the buildings uniquely and gave ways that it does that. He said he would argue in favor of good design and a more abstract treatment of the architecture generally rather apply a tripartite approach.

Sloman noted the applicant can also respond to comments they have heard tonight at the next meeting, and staff will meet with the applicant team as well to discuss revisions to the conditions. MORGAN said it would be helpful to work out some of those issues before the Commission's next discussion.

Roewe made some comments about how good, well-written code can be applied in circumstances that don't make much sense. For example, he continued, this project is located at the edge of the area within the Central Issaquah Plan, not in the center, and some of the requirements in the Central Issaquah Plan for ten-foot-wide sidewalks and street walls don't make sense for this project. He gave other examples of how the CIP standards are good for a downtown urban area but not necessarily for this project. He noted that the applicant team is discussing some of those differences with the City now.

LEONG noted the calculations for bike parking in the table on page 176 do not seem to add up correctly. Tarce said staff will take a look and get back to the Commission on that.

LEONG asked how high would the proposed boardwalk to the Rowley property be. Roewe gave details about the importance of the boardwalk in ensuring walkability for apartment residents. He said the boardwalk would definitely cross Tibbetts Creek, as the creek is on the project property. LEONG referred to the photo displayed earlier of the boardwalk at the Nisqually estuary. Roewe said the comment about wildlife access made during public comment raised an important point. The boardwalk has not been designed yet, he continued, but would need to allow wildlife to cross underneath, so the height would likely be five or six feet or so. There is also a grade change of eight to ten feet in some places, he added.

LEONG gave his observations about the project design. He said he likes the pitched roof rather than flat roof design, and the distinct look of the community clubhouse building, but in general his reaction was that the project includes a lot of straight streets and straight rows of buildings. He said the appearance to him is one of row houses that lacks interest. He spoke in favor of adding more parking stalls to serve the community clubhouse. He asked for clarification of fire truck access. Tarce said the fire access road will be gated. Sloman added there is an existing easement there that is only used for emergency access, which will continue. LEONG concluded he likes the separation of pedestrian and vehicle access throughout the design, but has concerns about

making the clubhouse stand out even more and the row-house appearance of the apartment buildings.

MORGAN noted page 6 of 9 shows the location of Tibbetts Creek as being on the Rowley property, and asked for clarification. Roewe replied there has been some discussion of relocating the Creek as part of the development agreement with Rowley, which would relocate the Creek on the Gateway Apartments property. That is still being worked out, he noted.

SWEDBERG asked for a response to the question raised during public comment about low-income or affordable units. Tarce replied there are no plans in this project for low-income units; the City does not require affordable housing in the Central Issaquah Plan standards, and while the City does have a bonus density option, the applicant is not partaking of it.

SWEDBERG agreed with other Commissioners that a pitched roof is preferable to a flat roof. He asked about a notation in the conditions that "roof colors shall be a light color with a Solar Reflective Index of 78 or greater." Tarce said the standards for roof colors are very specific and will be worked out in detail with the applicant. SWEDBERG said he had a similar concern with Condition 78, "The five-story buildings shall be further refined during the construction permit review to employ a tripartite articulation of the façade." Tarce replied that means that additional modulation will be discussed in more detail with the applicant during the construction permit phase. She said staff will have additional details for the Commission at its next meeting on the project. SWEDBERG also asked for clarification of Condition 90, "The offsite sewer main must be upsized to provide sufficient capacity for the project and maintain the capacity reserved for the offsite properties benefitting from the existing system." Does that mean that Newport Way will need to be dug up for a period of time. Doug Schepp, Engineer, said no; the sewer line is located on Poplar, a private road owned by Rowley. The City will work with Rowley and the Arena Sports complex on making this improvement during construction.

SWEDBERG added he agrees with HICKS that August 26 is coming up very soon, and that with the SEPA review period not ending until August 20, we might want to push back making a recommendation. HARRISON said he agrees, and noted it sounds as though the applicant team has not had an opportunity to really look over all the conditions. He said he would like the Commission to have more review time generally, and particularly for final deliberation opportunities. He continued with a question about the definitions used by the City of parking stall widths for urban village, village residential, and other designations. He said he has observed that parallel parked cars in the Highlands sometimes stick out in traffic because the parking spaces aren't wide enough. Sloman replied most of the requirements are similar across different designations, and noted that the Highlands began with a six-foot parking space requirement and eventually raised it to seven feet in most places and eight in others. Seven feet is widely used in Talus as well, she noted. The City feels that seven feet is more generous and would not recommend six feet, she added.

HARRISON spoke in favor of the pitched roof and the five-story buildings, and said clarification is needed for how elevation of proposed buildings is measured. He said we are seeing this same situation with nearly every project. He said from a marketability standpoint as well as an aesthetic one, he said he thinks that using very large trees that screen the project from I-90 would improve both the appearance of the project and the livability of units along I-90. He said he agrees with other Commissioners that there is an opportunity here to really seek out a Northwest look for the development, and referred to the Kelkari condominiums off Wildwood Road as a good example of a design that successfully reflects the Northwest.

HARRISON continued his comments about using “generally compliant” in the staff report, noting that the term contradicts itself, and asked what does it really mean. Tarce replied the term is used when an aspect of the project is, say, 90 percent compliant but 10 percent is still being worked out. In that case, she said, it gives a false impression to say either “non-compliant” or “compliant” without some kind of qualifier. HARRISON said he understands that, and suggested “compliant at this stage” or “compliant with conditions” or some other phrase. Tarce made a note of it.

HARRISON asked for clarification of “dog run” and “dog park” in the staff report. Tarce said normally she uses “dog run” to mean an enclosed area for dogs to be off leash, and may have inadvertently referred to a dog park as well. HARRISON asked would the dog run be for Gateway Apartment residents only. Tarce replied yes. HARRISON said that is not clear in the staff report. He asked for clarification of the “proposed senior project” on the adjacent property to the west. Tarce replied that is phase two, and has the same ownership as the Gateway Apartments project, but plans for it have not been finalized yet. HARRISON asked would it use the same egress and ingress from Newport Way as the Gateway Apartments. Tarce replied no.

HARRISON said on the school bus issue, it seems the safest way to address kids getting on buses safely is to have buses pick them up inside the development. Sloman said the Issaquah School District has very specific regulations about that, and the City wants to understand their needs and expectations and accommodate them.

HARRISON asked for clarification of what the term “review at construction,” used in the staff report, means. Sloman replied it is applied to elements that will be reviewed with the construction permit, normally because it is premature to be reviewing them at this stage.

HARRISON thanked the audience members for their comments and interest in the project, and also thanked the applicant team for their work on the project. He said the staff report shows a lot of work and was very impressive in its detail. MORGAN agreed, and said the exhibits and explanations really helped the Commission’s understanding of the project. HARRISON added he likes the name “Gateway Apartments” because the development really will be located at the entrance to the City, and represents a tremendous opportunity to make a statement about Issaquah.

BRENNAN asked about the timing of seeking a change in the code to allow for a pitched roof, as discussed earlier in the meeting. He asked for clarification about whether the Development Commission would be deliberating a recommendation on this application without having that change formalized. MORGAN said perhaps the Commission could address it as a condition. Sloman said staff will go back and study the code, determine what the options are for changing the code, and lay out options for the Commission. Roewe added his perspective about the impact on the design if the code adjustment isn’t made, and said he hopes the issue can be resolved soon.

#### **OTHER BUSINESS/ANNOUNCEMENTS/ADJOURNMENT**

With no further business to conduct, HARRISON adjourned the meeting at 10:16 PM.

Respectfully submitted,

Susan Lowe  
Recording Secretary



**CITY OF ISSAQUAH  
DEVELOPMENT SERVICES DEPARTMENT  
DEVELOPMENT COMMISSION**

**STAFF REPORT**

**August 27, 2015**

**APPLICATION:** Site Development Permit: SDP15-00001

**PROJECT:** **Fieldstone Memory Care**

**APPLICANT:** Justin Younker  
Cascadia Development, LLC  
4120 Englewood Ave  
Yakima, WA 98908

**CIVIL ENGINEERING:** Barghausen Consulting Engineers, Inc.  
Don Dawes, P.E.  
801 Second Ave, Suite 900  
Seattle, WA 98104

**TRAFFIC ENGINEERING:** Transportation Engineering Northwest (TENW)  
11400 SE 8<sup>th</sup> St, Suite 200  
Bellevue, WA 98004

**STAFF CONTACT:** Jennifer R. Woods, Associate Planner  
Development Services Department, (425) 837-3086  
Email: [jenniferrw@issaquahwa.gov](mailto:jenniferrw@issaquahwa.gov)

**REQUEST:** Approval for a Site Development Permit (SDP) to construct a new approximately 45,000 sq. ft. building for an Alzheimer's and memory care community on property south of the intersection of Issaquah-Fall City Rd. and Highlands Dr. NE.

The site contains two contiguous parcels totaling 6.81 acres. Parcel #222406-9002 is 95,832 sq. ft. and is zoned MF-M (Multifamily - Medium). This parcel is located closest to the intersection and is owned by Maclean Family LLC. Parcel # 222406-9149 is 200,812 sq. ft. and is zoned SF-SL (Single Family – Small Lot). This parcel contains a wetlands area that was recently delineated and surveyed



propose access to Highlands Dr. NE.

The site is wooded adjacent to the trail to the east of the property, around the wetland near the center of the property and at the southern edge of the property with the exception of the northwest portion of the property, which is sparsely treed.

Cascadia plans to boundary line adjust the northern parcel to encompass the area that will be improved, including the building and parking area. This will leave a second parcel that includes the wetlands area and buffer.

**EXISTING ZONING:**

The zoning of the property is split between SF-SL and MF-M. In accordance with IMC 18.07.360, the Development Standards for the site (i.e. setbacks, impervious surface coverage, building height, etc.) are MF-M.

North of the site is unincorporated King County. East of the site is unincorporated King County, SF-E (Single Family-Estate) and SF-SL zoning. West of the site is MF-M and SF-SL zoning. South of the site is SF-SL zoning. Refer to Vicinity Map in Exhibit 7.

**COMPREHENSIVE PLAN:**

The site is designated “Low Density Residential and Multi-Family Residential” by the Issaquah Comprehensive Plan, adopted in 1995 and as amended 2015.

**SUBAREA:**

North Issaquah

**BACKGROUND/HISTORY:**

- 4-10-2014 (1<sup>st</sup>) Pre-Application Conference Meeting, File No. PRE14-00003
- 6-11-2014 (2<sup>nd</sup>) Pre-Application Conference Meeting, File No. PRE14-00003
- 3-4-2015 Community Conference with the Development Commission, File No.COM15-00001.

**REVIEW PROCESS:**

Cascadia Development has applied for a Site Development Permit. As required by IMC 18.04.430 staff shall analyze and make a recommendation to the Development Commission based on the compliance of the proposal with the Comprehensive Plan, the standards and provisions of the Municipal Code, and other uniform codes in effect and administered by the City and applicable jurisdictions, and the

criteria set forth in the Design Criteria Checklist. Staff's recommendation is based on the information provided by the applicant and the best professional judgment of the Administration. As required by the code, the staff report should state the specific reasons and cite the specific chapters and sections of this Code and any other applicable rules or regulations, upon which the recommendation to the Development Commission is based. The report shall demonstrate that the recommendation complies with the purpose and intent of the Code. Staff may add new information to the report provided through public testimony, the applicant, or other means. The Administration may also modify the recommendation or proposed conditions of approval.

The decision to approve, approve with conditions or deny the project is the responsibility of the Development Commission, and is based on the staff report, applicable criteria, public comments, and discussion of the issues.

As required by IMC 18.04.450 a Level 3 Review is required for development proposals where the site's primary access and/or street frontage are located on and/or the site abuts Issaquah-Fall City Road to which the subject property is adjacent.

**PUBLIC NOTIFICATION:** A Notice of Application was mailed out to adjacent property owners on April 8, 2015. Notice was mailed out to approximately 57 residents.

On August 12, 2015, the applicant posted the site with the 4' x 4' Proposed Land Use Action sign, and the Affidavit of Installation was submitted on August 13, 2015.

A notice of the public hearing was mailed to the Parties of Record and property owners on August 18, 2015. A notice of the public hearing for the Site Development Permit was published in the Issaquah Press on August 20, 2015.

As of the date of this staff report, no comments have been received regarding this proposal.

**TIME LIMIT:** Per IMC 18.04.220-D, the final decision approving the SDP application shall be valid for three (3) years from the date of application approval as specified in the Notice of Decision for the project.

**LIST OF EXHIBITS:**

Exhibit 1	Building Material Board
Exhibit 2	SEPA Environmental Checklist, March 23, 2015

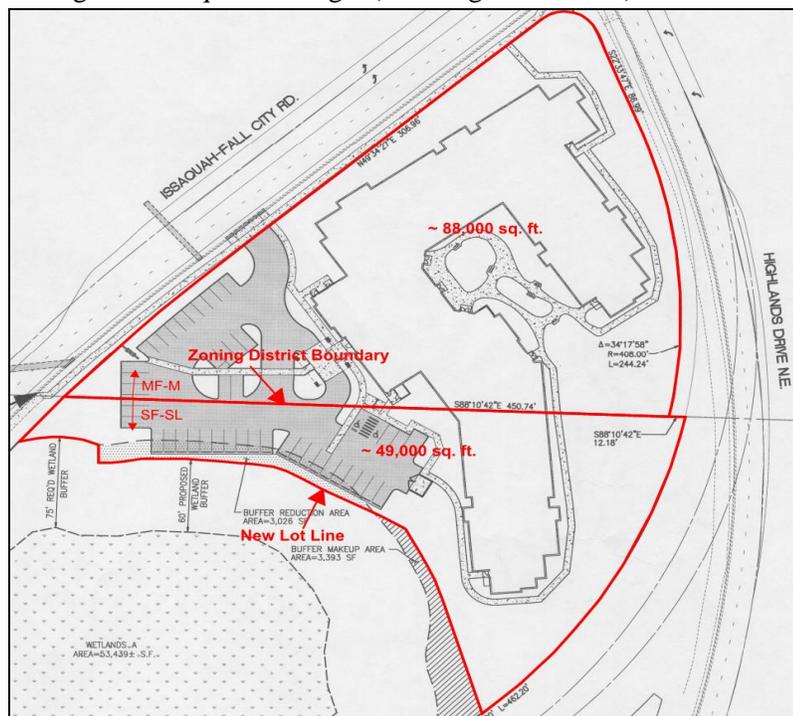
- Exhibit 3 SEPA Mitigated Determination of Non-Significance
- Exhibit 4 Plans, Elevations and Details, revised June 26, 2015.
- Exhibit 5 Appendix 2, Design Criteria Checklist
- Exhibit 6 Construction Conditions
- Exhibit 7 Vicinity Map

**DESCRIPTION OF PROJECT**

The applicants are seeking a SDP (SDP) for the construction of an Alzheimer’s and memory care facility. This facility will be located at the southern corner of the intersection of SE Issaquah-Fall City Rd. and Highlands Dr. NE. The new facility will include an approximately 45,000 square feet of building area with sixty (60) units and forty-six (46) parking stalls.

Zoning of the property is Multifamily- Medium (MF-M), and Single Family-Small Lot (SF-SL). In accordance with IMC18.06.040, development regulations for the site (i.e. setbacks, impervious surface coverage, building height, etc.) are determined by the most restrictive zoning if fifty (50) percent or more of the square footage of the lot is within the within the most restrictive district. Additionally, a boundary line adjustment is required to adjust a lot line around the building and parking area and separate the critical area, buffer and residual property south of the wetland. The critical area and its buffer are required to be placed in a Native Growth Protection Easement prior to the issuance of the Site Work Permit. Additionally, this adjustment will create a lot divided by district lines; however, fifty (50) percent or more of the square footage of the lot will not be within the most restrictive zoning district, SF-SL (Refer to SDP Condition 3 at the end of this staff report).

**Figure 3. Snapshot of zoning after lot line adjustment** This illustration shows the conceptual location of the adjusted lot line and approximate zoning district square footage. (Drawing not to scale)



The applicable district regulations for this project are MF-M. The use, an assisted living facility, is an allowed use in the MF-M zone.

### **PROJECT REVIEW**

The Issaquah Comprehensive Plan recognizes the housing opportunities of creating care facilities in while maintaining the character of the neighborhood and allows for the provision of housing opportunities for those with special housing needs (Policy HS-D1). To mitigate for the potential impacts of the development, staff is recommending conditions of approval using provisions in the Issaquah Municipal Code Chapters 18.07 Required Development and Design Standards, 18.12, Landscaping and Tree Retention, and Appendix 2, Design Criteria Checklist. The conditions of approval can be found at the end of this Staff Report.

This project is being reviewed for compliance with the requirements and criteria for approval in the SDP. Additional staff analysis for compliance with the Design Standards Checklist (Appendix 2 of the Issaquah Municipal Code Title 18, Land Use Code) can be found after the Recommendation section of this Staff Report. The Appendix 2 Design Criteria is Exhibit 5 of this staff report.

### **SEPA**

**Findings of Fact:** Refer to Exhibit 3 SEPA Mitigated Determination of Non-Significance.

**Condition of Approval:** See SDP Condition 1 at the end of this staff report and Construction Conditions 1 through 3 in Exhibit 6.

### **SITE DEVELOPMENT PERMIT**

#### **A. USE AND ZONING DISTRICT STANDARDS:**

In accordance with IMC18.07.360, the district standards for the project site (i.e., setbacks, impervious surface coverage, building height, etc.) are determined by the most restrictive zoning. The majority of the proposed building resides on property zoned MF-M. Assisted living facilities are an allowed use in MF-M.

The development standards of those two zones are very similar, one difference being SF-SL has a six (6) foot side yard setback and the MF-M zone has a seven (7) foot rear setback and another is the SF-SL zone base building height is thirty (30) feet and the MF-M zone base height is forty (40) feet. The MF-M district standards will be used as the Development Standards for the project.

A comparison of the development standards for the SF-SL, MF-M and the Fieldstone Memory Care project is shown below:

**Table 1. Comparison of SF-SL and MF-M Zone Standards and Fieldstone’s Proposal**

<b>District Standards per 18.07.360</b>	<b>SF-SL, Single Family Small Lot</b>	<b>MF-M, Multifamily - Medium</b>	<b>Applicant’s Proposal</b>
Density or dwell. unit/acre	4.5 du/ac	14.52 du/ac	N/A
Minimum lot size	9,600 sq. ft.	2,500 sq. ft.	2.5 +/- acres, construction limits (6.81 +/- acres total site area)
Front Setback (West)	10 feet (Minimum)	10 feet (Minimum)	19 feet
Side Setback (East)	6 feet (Minimum)	7 feet (Minimum)	24 feet
Side Setback (South)	6 feet (Minimum)	7 feet (Minimum)	NA**
Rear Setback (Southeast)	20 feet (Minimum)	20 feet (Minimum)	36 feet
Impervious Surface	50% (maximum)	50% (maximum)	25.5%
Pervious Surface	50% (minimum)	50% (minimum)	74.5%
Base Building Height	30 feet	40 feet	20 feet
Maximum Building Height	N/A	N/A	N/A

*\*Lot size to be determined after the boundary line adjustment is complete. The lot is currently 282,717 square feet and with the boundary line adjustment, the lots will be required to meet the minimum lot size.*

*\*\*Currently the building is shown over a property line. The adjustment of that lot line through the boundary line adjustment process will be reviewed to ensure it meets the required setback.*

**B. ASSISTED LIVING FACILITIES STANDARD**

Specific requirements for assisted living facilities are found in IMC 18.07.380. Development, including assisted living facilities, is permitted only when all of the following criteria are met and as permitted in the Table of Permitted Land Uses (IMC 18.06.130).

- (1) Barrier-Free Standards: Every unit within the project must be designed and built in conformance with the Uniform Building Code (UBC); the State Barrier-Free Design regulations, as amended; the Americans with Disabilities Act of 1990, as amended (ADA); and the current state and City regulations.

Findings of Fact: The parking lot shall be required to provide ADA parking stalls and two (2) are proposed. Barrier-free (ADA) parking stalls are shown to have direct access to building entries or site access points. The building and parking lot design will be reviewed at Site Work and Building Permit submittal in accordance with the Uniform Building Code (UBC); the State Barrier-Free Design regulations, as amended.

- (2) Community Space Requirements: All assisted living facilities of five (5) units or more shall provide the required community space both indoors and outdoors for persons who, in some cases, may be restricted to the immediate vicinity of the development.

- a. **Indoor Space Requirements:** Indoor community space shall provide a minimum of forty-eight (48) square feet per unit, in a contiguous area no smaller than two hundred (200) square feet to include:
- (1) Seating and table space for a minimum of thirty (30) percent of the total number of units provided (for example, six (6) units would provide seating/table space for two (2) units or four (4) persons);
  - (2) Kitchen facilities, including at minimum a sink, cabinet and counter space;
  - (3) A bulletin board no smaller than two (2) feet by three (3) feet. The bulletin board shall be placed in an area accessible to the residents for notice-posting purposes;
  - (4) Access and use consistent with Barrier-Free Standards.

Findings of Fact: The building is a single-story building. The proposed floor plan provides for two (2) dining and lounge areas with six (6) to eight (8) tables each, a café, theater with more indoor activity areas. The proposal complies with the requirement.

- b. **Outdoor Space Requirements:** The provision for outdoor community space provides that usable open space is provided to the residents. Area used for outdoor community space shall be calculated as part of the impervious surface for the proposal, and not considered an additional requirement. The hard-scape material shall be considered as impervious surface, in addition to any other areas of the outdoor space which is impervious. Outdoor community space shall provide a minimum of forty-eight (48) square feet per unit, in a contiguous area no smaller than two hundred (200) square feet:
- (1) Seating space for a minimum of thirty (30) percent of the total number of units provided (for example, six (6) units would provide seating/table space for two (2) units or four (4) persons);
  - (2) Landscaping integrated with the seating and table area. Not less than thirty (30) percent of the outdoor community space shall be landscaped with plant materials, while the remaining seventy (70) percent can be hard-scape materials which are barrier-free, such as pavers, textured concrete, and brick;
  - (3) Access consistent with Barrier-Free Standards.

Findings of Fact: The required amount of outdoor space to be provided is 2,880 sq. ft. and seating for eighteen (18) single units. The proposed site plan provides for an approximate 5,000 square foot enclosed, landscaped outdoor courtyard with ample seating and a walkway around the building perimeter. The proposal complies with the requirement.

- (3) **Parking:** Required parking for assisted living facilities is established in the Table of Off-Street Parking Standards (IMC 18.09.050).

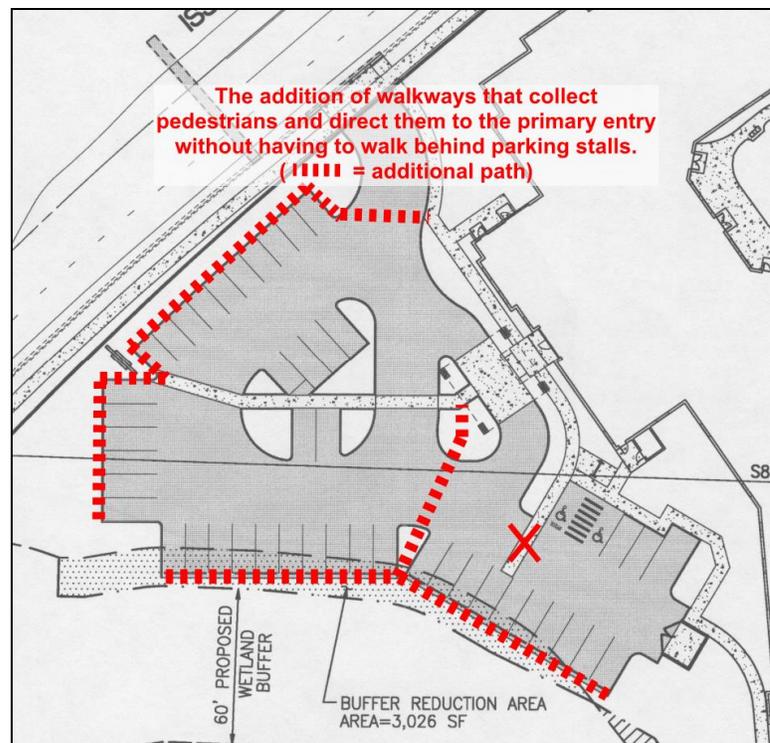
Findings of Fact: The site plan proposes forty-six (46) stalls. The code requires one (1) space for each two (2) units and one (1) space per employee at max shift. A 45,000 sq. ft. assisted living care building with sixty (60) units and fifteen (15) employees at max shift is required to have a maximum

of forty-five (45) stalls. The appropriate number of ADA stalls will be provided in accordance with the Uniform Building Code.

(4) Access and Circulation:

- a. Motorized: Vehicular access shall be provided such that it does not negatively impact adjacent land uses. Internal circulation shall also be provided, such that it does not interfere with pedestrian access or internal circulation;
- b. Nonmotorized: Pedestrian walkways shall be provided within a project and as linkages to adjacent projects.

**Figure 4. Alternative Pedestrian Circulation Through Parking Lot** This illustration shows the conceptual location of walkways to meet the code requirement. (Drawing not to scale)



Findings of Fact: Pedestrian walkways shall be provided within a project and will link to adjacent properties and Issaquah-Fall City Road. The location of parking lot walkways shall be revised so that pedestrians can walk from their parking space to a walkway that connects to the primary entry. Additionally parking lot walkways shall be physically separated from vehicular areas by grade, landscape, curbs or other similar mechanisms to separate the walkway from the vehicular way. The loading stall shall incorporate enough maneuvering space to meet the code requirements. Finally, rolled curbs shall be installed within the emergency vehicle clearance zone, so emergency vehicles can drive over them.

The development access is required to maintain the City's adopted Level of Service (LOS) standard "D." The City will evaluate if an alternative location is need or if other improvements are required

to maintain the level of service required. Refer also to the Access/Street Improvements section below.

(5) **Building Modulation:** Building modulation is intended to break up the overall bulk and mass of the exterior of a multifamily building, including townhouses. Modulation should also add character to the overall building exterior as well as to individual units.

- a. Building facade modulation shall occur at every twenty-five (25) feet of wall length. The modulation can take the form of decks, balconies, indentations, extrusions and other various forms; and
- b. Minimum modulation depth shall be approximately three (3) feet; and
- c. Minimum modulation width shall be approximately eight (8) feet.

**Findings of Fact:** The building is well-modulated, as shown in the perspective drawings in Exhibit 4. Modulation techniques used include recessing walls and using different materials to break up the facades into multiple horizontal planes. Windows are present at regular intervals as shown on the elevations. The building has provided the required modulation. Staff Analysis on the project's compliance with Appendix 2 of the Land Use Code, Design Criteria Checklist, can also be found in Exhibit 5.

**Figure 5. Elevations** The following renderings are from Exhibit 4 Plans, Elevations and Details.



*Rendering of South (Front) Elevation*



*Rendering of West (Side) Elevation*



*Rendering of East (Side) Elevation*

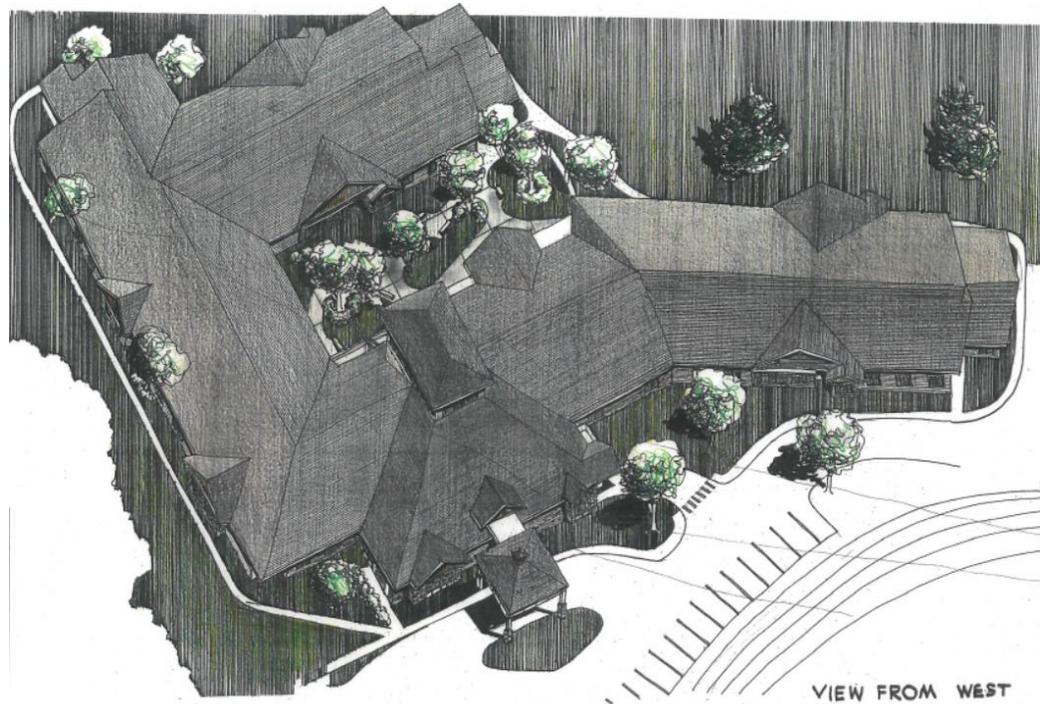


*Rendering of North (Rear) Elevation*

- (6) **Roofline Variation:** Roofline variation is intended to break up the overall bulk and mass of a multifamily building and to provide a visual relief as viewed against the skyline. Roofline variation shall occur on all multifamily structures with rooflines which exceed fifty (50) feet in length. Roofline variation shall be achieved using one (1) or more of the following methods:
- a. Vertical offset ridge line;
  - b. Horizontal offset ridge line;
  - c. Variations of roof pitch; or
  - d. Any other technique approved by the Planning Director/Manager which achieves the intent of this section.

Findings of Fact: The building design shows variations in roof pitch breaking the building into sections versus appearing as one large building.

**Figure 6. Roof Form** The following renderings have been selected from Exhibit 4 Plans, Elevations and Details.



*Rendering of Overhead View of the Building*

- (7) **Screening:**

- a. **Parking Area:** The parking area shall be screened to visually buffer areas within the project complex and adjacent properties;
- b. **Structures:** Screening of structures from adjacent properties shall be provided, such as landscaping, fences, berms or other similar materials and/or designs.

**Findings of Fact:** The parking area is adequately screened from the street. No structures that require screening are visible from the street.

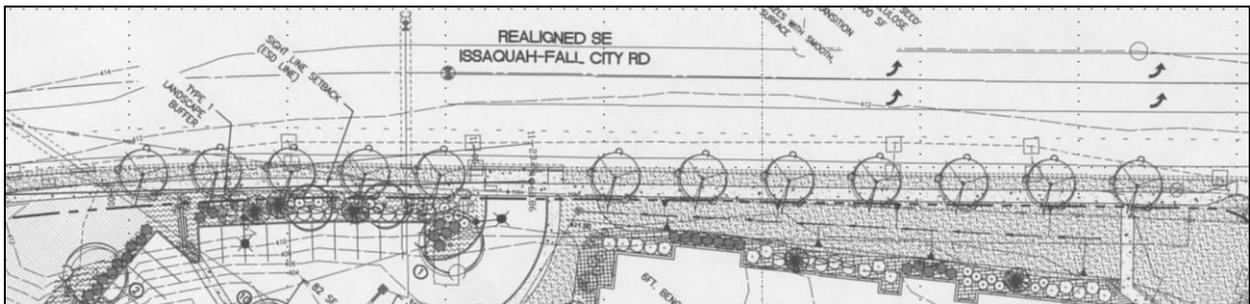
**Conclusions:** The Issaquah Comprehensive Plan recognizes the value of providing various housing opportunities in enhancing the quality of life of its residents in Policy HS-D1. The development of the site for an assisted living facility is consistent with these Comprehensive Plan policies, as well as the land use and zoning district designations for the site, as discussed in the “Project Review” section of this Staff Report. Access and other transportation improvements required for the project will be addressed in the Conditions of Approval. Similarly, site and building design compatibility will be ensured through conditions of approval included in Exhibit 5 Appendix 2, Design Criteria Checklist and Exhibit 6 Construction Conditions.

**Conditions of Approval:** See SDP Condition 1 and Construction Conditions 4 through 7.

**C. ACCESS/STREET IMPROVEMENTS:**

**Findings of Fact:** The single vehicular access to the development will be from Issaquah-Fall City Rd. This is also the main entrance for delivery and garbage trucks. Frontage improvements along Issaquah-Fall City Rd. include relocating the sidewalk behind a new five (5) foot landscape strip between the curb and sidewalks, including street trees. The applicant has requested a deviation to the street standards to allow for modified improvements along Issaquah Fall City Rd. in order to be relieved of the requirement to relocate a portion of the existing sidewalk adjacent to the critical area and buffer. The request is being reviewed according to the process required by the City’s street standards for which a decision will be rendered by the City’s engineer prior to the submittal of the site work permit. There are no improvements required for Highlands Dr. or the existing King County Trail to the east of the project site as that infrastructure is fully improved.

**Figure 7. Snapshot of sidewalk improvements at the north section of the project site along Issaquah-Fall City Rd.** This detail plan also shows the conceptual location of a new 5-foot wide sidewalk and 5-foot landscape strip with street trees. (Drawing not to scale)



*Transportation Concurrency*

A Traffic Impact Study (TENW, June 24, 2015) was provided to estimate traffic trip generate on from the project, evaluate the site access, and to address traffic impacts and mitigation related to the development proposal. The study concludes the proposal would generate 186 weekday daily trips, with twelve (12) trips occurring during the weekday AM peak hour, and twenty (20) trips during the PM peak hour. The trip generation estimate is based on the methodology included in the institute of Transportation Engineers (ITE) Trip Generation Manual.

As detailed in the SEPA Mitigated Determination of Non-Significance (Exhibit 3), the subject proposal is consistent with the growth assumptions in the traffic concurrency model and twenty (20) trips falls within the available trip bank and passes concurrency. Therefore, the proposed development can withdraw trips from the "trip bank" that was calculated for concurrency and can mitigate their traffic impacts by payment of the traffic impact fee. The traffic impact fee will be used by the City to fund transportation improvements identified in the concurrency model and on the City's Transportation Improvement Program (TIP).

However, the concurrency assessment doesn't address traffic operations and safety at the project site driveway access or at non-concurrency intersections. The site would be accessed from a driveway off Issaquah-Fall City Road, approximately 300 feet southwest from the intersection with Highlands Drive NE. The traffic report evaluated the site access for level of service (LOS) and queuing, site entering and stopping distance, and turn lane analysis. The analysis assumed turn movements from the access drive would be restricted to right-in/right-out turns only.

The traffic study concludes the site driveway would operate at LOS A in the AM peak and LOS C in the PM peak. Intersection sight distance and stopping sight distance would meet applicable standards. The traffic study concludes no site access improvements, (i.e. inbound right-turn pocket or outbound acceleration lane) on Issaquah-Fall City Road are needed. The site access shall maintain the City's adopted level of service (LOS) standard "D." The City will evaluate if an alternate driveway location is necessary or if channelization improvements on Issaquah-Fall City Road are needed to maintain the LOS and safe access operations.

This Finding serves as the transportation concurrency certificate for this development. As a result twenty (20) trips will be logged into the City's trip bank.

#### *Fire and Emergency Vehicle Access*

Emergency vehicle access and circulation has been reviewed by Eastside Fire and Rescue. Curbs adjacent to the emergency vehicle clearance zone are required to be rolled instead of vertical, so that an emergency vehicle may easily drive over them. Additionally, lights and trees shall also be removed or relocated outside of the emergency access clear zone.

**Conditions of Approval:** See SDP Condition 1 and 2 and Construction Conditions 6, 10 and 16 in Exhibit 6.

#### **D. TRAFFIC:**

Findings of Fact: An access study was prepared and reviewed by staff. After receiving comments and input from the City's Public Works Department, access to the development is required to meet the City's sight line setbacks and stopping distance requirements. Additional analysis is required to

ensure these requirements can be met or mitigated should the current road configuration not provide the ability to comply. Please see further discussion of traffic impacts and mitigation under the SEPA Findings of Fact in the Determination of Non-Significance in Exhibit 3.

**Conditions of Approval:** See SDP Condition 1 and 2 and Construction Condition 3 in Exhibit 6.

#### **E. PARKING:**

Findings of Fact: Requirements for off-street vehicular parking, bicycle parking and loading spaces are governed by IMC Chapter 18.09. The purpose of parking standards is to assure adequate off-street parking, reduce on-street parking, increase traffic safety, maintain smooth traffic flow, and reduce the visual impact of parking lots. These standards are also designed to achieve safe and efficient vehicular and non-motorized circulation and economy of space (IMC 18.09.010.A). The intent of the parking standards is to promote effective use of transportation facilities with the goal of moving people from place to place. Emphasis shall be given to alternate methods of moving people which will: deter traffic congestion; promote environmental quality through less use of fossil fuels and potentially less impervious surface needed for parking areas; and provide convenience and reliability to commuters, residents, pedestrians, employees, tourists, shoppers, students, bicyclists, special populations and service providers (IMC 18.09.010.B).

The parking area standards in IMC 18.09.090 specifies stall sizes for standard and compact stalls, drive aisle widths, parking lay-out, barrier-free access design and construction standards. For non-residential developments, surface parking material must be hard-surfaced, consistent with the City's construction standards. Similarly, driveways must be constructed to City of Issaquah Street Construction Standards per IMC 18.09.090.F.1.

Light standards must be located with adequate clearance from parking stalls, stacking areas, driveways and ingress/egress points (IMC 18.09.090.E) in addition to emergency vehicle access routes. The exterior lighting must also comply with additional lighting design requirements in IMC 18.07.107.

#### ***Location of Parking*** (IMC 18.09.030.F)

The minimum required off-street parking area shall be provided within eight hundred (800) feet of the building or use for which the parking area is required. Parking and the storage and/or display of vehicles are prohibited in any required landscape planting area unless otherwise allowed by Chapter [18.12](#) IMC, Landscaping.

Visitor and employee parking is located within approximately thirty (30) feet of the main entry. A continuous and safe pedestrian connection to through parking lot will be required at the time of site work permit approval for this parking lot.

#### ***Required Off-Street Parking*** (IMC 18.09.040)

Table 18.09.050, *Table of Off-Street Parking Standards*, requires one (1) space for each two (2) units and one (1) space per employee at max shift. A 45,000 sq. ft. assisted living care building with fifteen (15) of employees at max shift is required to have a maximum of forty-five (45) stalls. The site plan proposes forty-six (46) stalls. See also staff's findings under "Assisted Living Facility Standards," (4) Parking above.

***Design Standards and Stall Dimensions*** (IMC 18.09.090.H)

The proposed parking plan for the facility shows 90-degree parking stalls for the visitor/employee parking area. IMC 18.09.090.H and IMC 18.09.095 provide the minimum dimensions for compact and standard stalls, design and construction standards for parking areas. The landscape plans and site plan for the site shows 90-degree parking spaces.

Maximum standard parking stall sizes shall be nine (9) feet wide by twenty (20) feet long and compact stalls shall be nine (9) feet wide by fifteen (15) feet long. Wheelstops shall be positioned eighteen (18) inches into the parking stall. As an alternative to the wheelstop, the applicant may extend the landscape eighteen (18) inches into the parking stall, so that cars may overhang the landscaping. All regular and compact stall sizes shown are too long and will need to be revised. Compliance to design standards and stall dimensions will be reviewed during the landscape plan and Site Works Permit review.

***Barrier-Free Parking Spaces*** (IMC 18.09.090.I)

The project is required to provide barrier-free parking spaces, in accordance with the Washington State Amendments to the Uniform Building Code, Chapter 11, Regulations for Barrier-Free Facilities. Additional standards for barrier-free parking are found in IMC 18.09.090.I:

Accessible parking spaces are required to be located on the shortest possible accessible route of travel to an accessible building entrance. Whenever practical, the accessible route of travel shall not cross lanes of vehicular traffic.

The proposal shows the location of all the barrier-free (ADA) parking spaces, both for a van and an automobile, as well as the location of the ADA ramps. The location of these spaces is adjacent to the primary building entry. The design of the ADA parking spaces and ramps will be reviewed during the construction permit review.

***Required Bicycle Parking*** (IMC 18.09.030.I)

IMC Chapter 18.09.030(I) contains the required standards for bicycle parking. The code states that all sites required to provide non-motorized facilities shall provide bicycle parking spaces equal to five (5) percent of required automobile parking spaces for the first 300 required auto stalls and one (1) percent of auto stalls in excess of 300. No less than two (2) bicycle parking spaces shall be provided for each project.

The IMC also requires bicycle parking to be placed in a publicly visible location within fifty (50) feet of a primary building entrance. Bicycle parking shall not block pedestrian use of a walkway. The proposal is required to provide three (3) bike parking stalls. One bike rack, including four (4) stalls, is proposed to be provided at the building entry.

***Landscaping and Screening*** (IMC 18.09.090.L)

Parking lot landscaping and screening is required per IMC 18.12.160. IMC 18.12.100, *Additional landscape requirements for parking areas*, also provides for sight barriers and adequate shading of parking lots. Trees and vegetation are required in parking areas to break up large impervious areas

and mitigate the negative impacts created by vehicles on the public realm, including noise, heat island effect, glare and views from residential areas and public rights-of-way.

***Loading Spaces Requirements*** (IMC 18.09.110)

According to Table 18.09.110, the project is required to provide at least one (1) Type A loading space for a nonresidential use exceeding 30,000 sq. ft. A Type A loading space has a dimension of 25 feet deep by 10 feet wide. A loading space is located adjacent to the trash enclosure space on the southeast corner of the building. Maneuvering space of at least fifty-two (52) feet in length, and exclusive of off-street parking or other obstructions, is required adjacent to the loading space. The current loading spaces do not provide fifty-two (52) feet of clearance for maneuvering. The loading stall shall be signed and striped accordingly. Additional staff analysis of parking area and waste enclosure site design are found in Exhibit 5 Appendix 2, Design Criteria Checklist.

***Screening of parking areas***

Table 18.12.060(B) – Schedule – Landscape Types by Land Use Districts, Additional Requirements for Specific Situations, indicate that parking areas and loading areas require Type 1 Landscape Buffers for parking adjacent to the street. This requirement applies to western boundary perimeter of the parking lot.

The landscape scheme generally shows that the parking lots are adequately broken up with perimeter landscaping and islands/peninsulas that can adequately support trees and shrubbery. Type 1 Landscape is provided along Issaquah-Fall City Rd. to screen the parking area.

**Conditions of Approval:** See Construction Conditions 8, 7, 12, 19, 20 and 21 in Exhibit 6.

**F. DRAINAGE/GRADING:**

Findings of Fact: The Development Services Department has reviewed the proposed stormwater system and concluded that the detention system will be effective in meeting current stormwater regulations. The system appropriately implements these standards by detaining the stormwater volume, resulting in a substantial reduction in peak flow rates leaving the site

There is additional information required, however, regarding the wetland hydrology and impacts. Total runoff volumes for pre-developed and proposed conditions should be documented for each outfall, to demonstrate the effect of changed hydrology (if any) on off-site wetlands.

Water Quality will be provided for the pollution generating impervious surface.

**UTILITIES:**

Findings of Fact:

Per Issaquah City Municipal Code Chapter 12.32, the contractor will need to provide and install conduit for cable television if it does not already exist. All new on-site overhead utilities (power, telephone, CATV, etc.) shall be constructed underground along and throughout the project. Water and Sewer service is available and will be provided by Sammamish Plateau Water and Sewer District. New or upgraded water mains must be approved by the district, to provide domestic service, fire and irrigation water.

**G. LANDSCAPING AND TREE RETENTION:**

Findings of Fact: Landscape, tree preservation, and irrigation plans are required as part of the Landscape Permit for the project. Approval of the Landscape Permit is contingent on the applicant complying with the requirements of IMC 18.12, Landscaping and Tree Preservation.

Landscape Plans, including a plant schedule, and locations of fences, bike racks, seating, walkways and the courtyard have been reviewed at a conceptual level by the City. The Tree Plan is integrated into the overall landscape plans. Revisions to the planting scheme and replacement of plant types will be reviewed and approved as part of the Landscape Permit approval.

The plant lists for the new landscape areas include large trees, understory trees, shrubs and ground covers. The choice of landscaping materials is satisfactory, in terms of the general types of plants and how they are applied in the landscape. Other areas of the site shows a landscape planting plan that provides good coverage around the site. Many of the proposed plants are Northwest native and the planting pallet includes a mix of non-native that are adaptable to this region.

IMC 18.12 requires Type 1 Landscaping along the street frontage and Type 2 Landscaping in and around the interior parking lots. IMC 18.12.070, Schedule-General Requirements by Landscape Type, list those landscape requirements, including type, spacing, planter width and plant sizes. In addition, Type 1 landscaping is required as screening for mechanical equipment and loading areas.

IMC 18.12.120 specifies the use of native vegetation for plantings near critical areas and their associated buffers. Where native vegetation cannot be retained, all vegetation is required to be planted and maintained so that no plant material or runoff of irrigation water and fertilizers will be diverted into the critical areas or their associated buffers.

***Landscape Screening required for mechanical equipment, loading areas, trash enclosures, blank walls and tall retaining walls***

Under Table 18.12.060(B) in the Landscaping Chapter of the IMC, Type 1 Landscape is required for parking areas adjacent to streets, outdoor storage, trash enclosures, mechanical/electrical equipment, retaining walls and blank walls abutting rights-of-way. A Type 1 Landscape is meant to provide a dense sight barrier to significantly separate uses, with plant materials consisting of large evergreen and deciduous material to provide 90% sight obscuring vegetation within 3 years. As an alternative, screening can consist of 70% evergreen trees backed by a 100% sight-obscuring fence softened or accented with landscaping. Additional landscape requirements for fences, hedges, trash enclosures and mechanical equipment are covered under IMC 18.12.130.

Mechanical equipment. Per IMC 18.12.130.D, all mechanical equipment areas, except at the access areas for the utility boxes, shall be screened from view of the public right-of-way through the use of hedges or fencing on all sides. Screening shall use a Type 1 landscape with a minimum width of five (5) feet. Ground-mounted mechanical equipment locations, size and types have not been provided to the City for review. Alternative screening for mechanical equipment is acceptable, as prescribed in IMC 18.07.135. See additional staff discussion on mechanical equipment screening in the subsection with the same title. While no mechanical equipment is shown in the preliminary plans, staff anticipates there may be either ground or roof set mechanical equipment that may require screening.

*Trash enclosures.* IMC 18.12.130.C requires trash enclosures to be screened using a 100% sight-obscuring fence or wall and a Type 1 Landscape. The chain link fence proposed is not compatible with the building design and another material that is compatible shall be approved with the Building Permit. The proposed waste enclosure screen will need to be modified to incorporate a three (3) to five (5) landscape strip that includes seventy (70) percent evergreen trees and utilizes a fence that is compatible with the building design. Additional staff analysis of parking area and waste enclosure site design are found in Exhibit 5 Appendix 2, Design Criteria Checklist.

In accordance with IMC 18.12.160(B), in order to insure that all plant materials used in landscapes shall be maintained in a healthy and growing condition, a cash deposit equivalent to fifty (50) percent of the value of the landscaped plant material, cost of labor, irrigation and materials shall be posted with the City prior to receipt of a temporary or final Certificate of Occupancy. The cash deposit will be returned to the applicant in three (3) years if the plants remain in a healthy growing condition and have achieved full coverage. The Development Services Director/Manager may accept other suitable security as permitted in Chapter 18.04.

***Tree Retention***

A detailed tree survey, including summaries of trees to be retained and trees to be removed on the entire property, are included Exhibit 5. IMC 18.12.1385 prescribes the tree retention rate for various types of land uses. The retention rate for MF-M, Multifamily- Medium, is being used per IMC 18.07.360, District standards table. The trees to be retained on site are primarily at the northeast and eastern side of the property and currently screen the site from the Highlands Dr. No tree removal is allowed in the critical area or its buffer.

Below is the tree count summary provided by the applicant. The clearing of trees will result in a tree retention rate of twenty-seven (27) percent. Per IMC 18.12.1390.A.2, the project is not required to provide replacement trees because its retention rate does not fall below the minimum twenty-five (25) percent required.

**Table 4. Tree Retention Summary**

Total caliper inches of all significant trees within the Developable Site Area*	2766
Tree Retention Required in Caliper Inches	679
Retention Rate Required	25%
Retention Rate Proposed	27%

\*Trees are required to be retained in Critical Areas and their buffers and are not counted towards the tree retention requirements.

**Conditions of Approval:** See Construction Conditions 12 through 16, 19 and 27 through 31 in Exhibit 6.

**H. SIGNS**

Findings of Fact: As stated above, the applicants will submit for a Sign Permit, with detailed sign plans, at a later time. An approved sign permit is required prior to installation of any type of signs that is regulated under IMC 18.11.

**Conditions of Approval:** See Construction Conditions 17 and 18 in Exhibit 6.

**I. FENCES**

Findings of Fact: IMC 18.07.120.C. *Preferred Materials* identifies wood, brick, stucco or wrought iron as preferred materials for fences (and other vertical accessory elements) when they are visible from a public right-of-way or neighboring property. Chain link is not compatible with the building design and another fence type must be provided to ensure design compatibility. This pertains to the fences located at the parking lot edge, the trash enclosure and the courtyard. Hedges, planted so they provide an effective barrier, can also substitute for fencing.

**Conditions of Approval:** See Construction Conditions 13 and 14 in Exhibit 6.

## **J. OUTDOOR LIGHTING:**

Findings of Fact: Outdoor lighting is governed by IMC 18.07.107. A lighting plan is required for this project. Lighting associated with signage is addressed by Chapter 18.11 IMC, Signs.

The applicant submitted a Lighting Plan for review On June 23, 2015. The lighting plan included the photometric measurements for the entire site and a general representation of the light fixtures to be used. The lighting levels currently proposed shall be modified to: limit lighting in the parking lot to 5 footcandles, except at the beginning boundary of the critical area buffer where the limit is 0.3 footcandles, and increase lighting for common areas, including the walkway and courtyard, to 0.3 footcandles.

**Conditions of Approval:** See Construction Conditions 9 through 11 and 25 in Exhibit 6.

## **K. DUMPSTER ENCLOSURE/RECYCLING:**

Findings of Fact:

A waste and recycling enclosure will be provided on the southeastern corner of the building. The applicant has not yet submitted the forms for Solid Waste Service and Collection Standards for approval by CleanScapes, with specific details.

With the exception of the fence type proposed, the dumpster/recycling enclosure appears to satisfy the City's development criteria and will need to be approved by CleanScapes, Inc. prior to issuance of the Building Permit. The applicant should also work with the City's Resource Conservation Office to determine the most appropriate and beneficial management of garbage, recycling and food waste.

**Conditions of Approval:** See Construction Conditions 12 and 13 in Exhibit 6.

## **L. MECHANICAL EQUIPMENT SCREENING**

Findings of Fact: Mechanical equipment including, but not limited to, HVAC, electrical transformer vaults, and satellite dishes must be significantly screened. Screening of ground-mounted equipment shall be through appropriate fencing, landscaping, or a combination of the two (2). The screening shall be effective in both winter and summer. Rooftop equipment shall be screened in a manner and material that is architecturally compatible with the building. Examples of appropriate screening include, but are not limited to, lattices, parapet walls or rooftop plantings.

Earlier discussion of landscape screening under the Landscaping and Tree Retention subsection of this Staff Report address screening of ground-mounted mechanical and electrical utility equipment. IMC 18.12.130.D requires Type 1 landscape for screening of ground-mounted mechanical and

electrical equipment. While no mechanical equipment is shown in these preliminary plans, any proposed mechanical equipment will be required to be screened as required by the code.

**Conditions of Approval:** See Construction Conditions 22 and 23.

### **RECOMMENDATION:**

Based on the submitted application and plans, the Administration recommends that the Development Commission move to:

- A. Approve the Site Development Permit for SDP15-00001: *Fieldstone Memory Care*, Exhibits 1 through 7, and the conditions of approval provided below.
- B. Direct the Development Services Department to prepare Findings of Fact which affirm the Development Commission's approval of the Site Development Permit for *Fieldstone Memory Care*, application SDP15-00001. The Staff Report dated August 27, 2015 will serve as the Findings of Fact.

### **STAFF RECOMMENDED CONDITIONS OF APPROVAL**

Due to the schematic level of information provided in the SDP application, some aspects of the submittal will receive design as well as technical review at the time of Site Work or Building Permit, rather than dividing the review between land use and construction permits. As a result, a Pre-Submittal Meeting with City Staff is required of the Applicant. This meeting will be used to complete the land use level of design review prior to full submittal of an application for utility or building construction, and to ensure that all necessary requirements for a complete Building Permit and Site Work Permit are prepared by the Applicant.

Nothing in this set of Recommended Conditions of Approval shall be interpreted to excuse the applicant from meeting all of the requirements of the City of Issaquah Comprehensive Plan, the Issaquah Land Use Code, the International Building Code, the City's Street Standards and other regulatory instruments used by the City to ensure public welfare, health and safety.

#### **I. SEPA**

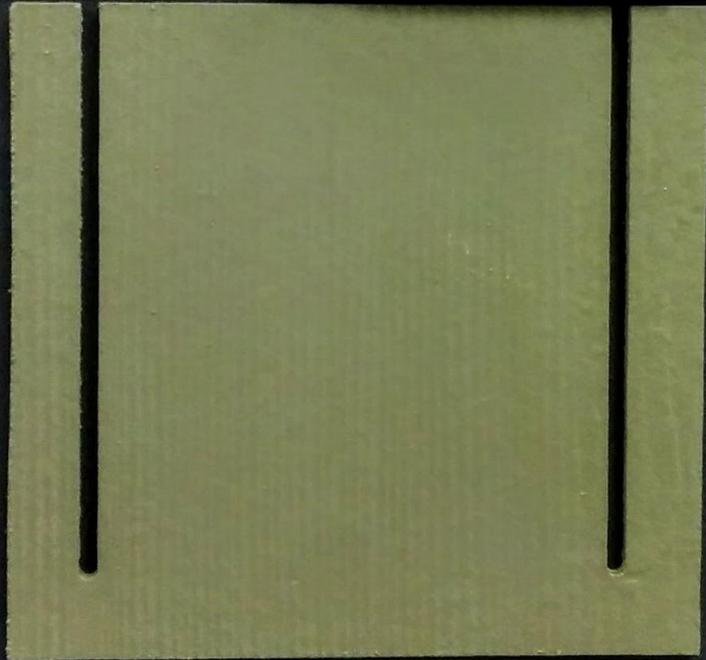
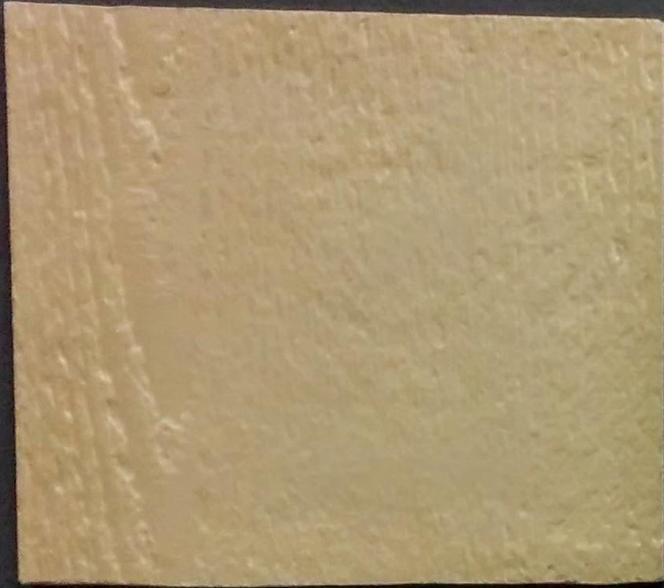
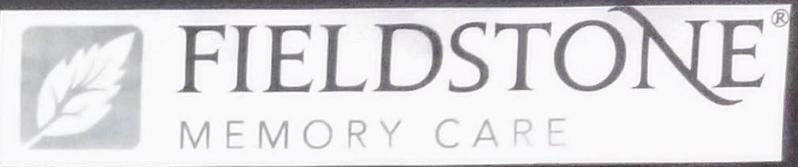
1. *The site access shall maintain the City's adopted level of service (LOS) standard "D." The City will evaluate if an alternate driveway location is necessary or if channelization improvements on Issaquah-Fall City Road are needed to maintain the LOS and safe access operations.*

#### **II. TRAFFIC**

2. *Twenty (20) trips will be logged into the City's trip bank. The concurrency fee shall be application fee (20 new PM Peak Hour Trips x \$52.50 = \$1,050) shall be paid prior to the submittal of construction permits.*

#### **III. USE AND ZONING DISTRICT STANDARDS:**

3. *A boundary line adjustment shall be recorded prior to the issuance of a Building Permit.*





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MAR 23 2015

**SEPA ENVIRONMENTAL CHECKLIST**  
**UPDATED 2014**

City of Issaquah

***Purpose of checklist:***

Governmental agencies use this checklist to help determine whether the environmental impacts of your proposal are significant. This information is also helpful to determine if available avoidance, minimization or compensatory mitigation measures will address the probable significant impacts or if an environmental impact statement will be prepared to further analyze the proposal.

***Instructions for applicants:*** [\[help\]](#)

This environmental checklist asks you to describe some basic information about your proposal. Please answer each question accurately and carefully, to the best of your knowledge. You may need to consult with an agency specialist or private consultant for some questions. You may use "not applicable" or "does not apply" only when you can explain why it does not apply and not when the answer is unknown. You may also attach or incorporate by reference additional studies reports. Complete and accurate answers to these questions often avoid delays with the SEPA process as well as later in the decision-making process.

The checklist questions apply to all parts of your proposal, even if you plan to do them over a period of time or on different parcels of land. Attach any additional information that will help describe your proposal or its environmental effects. The agency to which you submit this checklist may ask you to explain your answers or provide additional information reasonably related to determining if there may be significant adverse impact.

***Instructions for Lead Agencies:***

Please adjust the format of this template as needed. Additional information may be necessary to evaluate the existing environment, all interrelated aspects of the proposal and an analysis of adverse impacts. The checklist is considered the first but not necessarily the only source of information needed to make an adequate threshold determination. Once a threshold determination is made, the lead agency is responsible for the completeness and accuracy of the checklist and other supporting documents.

***Use of checklist for nonproject proposals:*** [\[help\]](#)

For nonproject proposals (such as ordinances, regulations, plans and programs), complete the applicable parts of sections A and B plus the SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS (part D). Please completely answer all questions that apply and note that the words "project," "applicant," and "property or site" should be read as "proposal," "proponent," and "affected geographic area," respectively. The lead agency may exclude (for non-projects) questions in Part B - Environmental Elements –that do not contribute meaningfully to the analysis of the proposal.

**A. Background** [\[help\]](#)

1. Name of proposed project, if applicable: [\[help\]](#)

*Issaquah Memory Care*

2. Name of applicant: [\[help\]](#)

*Cascadia Development, LLC*

3. Address and phone number of applicant and contact person: [\[help\]](#)

<b>Applicant:</b> Cascadia Development, LLC 4120 Englewood Avenue Yakima, WA 98908 Contact: Justin Younker	<b>Contact Person:</b> Barghausen Consulting Engineers, Inc. 18215 - 72nd Avenue South Kent, WA 98032 Contact: Don Dawes
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4. Date checklist prepared: [\[help\]](#)

*March 2015*

5. Agency requesting checklist: [\[help\]](#)

*City of Issaquah*

6. Proposed timing or schedule (including phasing, if applicable): [\[help\]](#)

*Construction 2015-2016*

7. Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain. [\[help\]](#)

*No future expansions are planned at this time.*

8. List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal. [\[help\]](#)

*The following environmental documents have been prepared:*

- *Boundary and Topographic Survey prepared by Barghausen Consulting Engineers, Inc., dated December 31, 2013*
- *Wetland Delineation prepared by Altmann Oliver Associates, LLC, dated April 28, 2014*
- *State of Washington Archaeological Site Inventory Form for Maclean Site dated December 30, 2013*
- *State of Washington Archaeological Site Inventory Form for Issaquah-Fall City Road Segment dated January 17, 2014*
- *Cultural Resources Assessment prepared by SWCA Environmental Consultants, dated January 30, 2014*
- *Tree Survey Site Plan prepared by Poston Architects dated May 28, 2014*
- *Preliminary Engineering Plans prepared by Barghausen Consulting Engineers, Inc., dated March 20, 2015*
- *Preliminary Detention and Water Quality Analysis prepared by Barghausen Consulting Engineers, Inc., dated October 15, 2014*
- *Sammamish Plateau Water & Sewer District Developer Extension Agreement Letter dated April 16, 2014*
- *Geotechnical Engineering Report (pending)*

9. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain. [\[help\]](#)

No permits or applications are pending for the property.

10. List any government approvals or permits that will be needed for your proposal, if known. [\[help\]](#)

*The following approvals or permits will be required for the proposal:*

- *Sammamish Plateau Water & Sewer District Developer Extension Agreement*
- *Design Review by City of Issaquah*
- *SEPA Determination by City of Issaquah*
- *Administrative Adjustment of Standards (AAS) by City of Issaquah*
- *Site Development Permit (SDP) by City of Issaquah*
- *Commercial Construction Permit by City of Issaquah*
- *Traffic Concurrency by City of Issaquah*
- *Washington State Department of Archaeology and Historic Preservation Permit*
- *NPDES Permit from Washington Department of Ecology*
- *Early Start Grading Permit (if applicable) by City of Issaquah*
- *Right-of-Way Use Permit by City of Issaquah*
- *Boundary Line Adjustment by City of Issaquah*

11. Give brief, complete description of your proposal, including the proposed uses and the size of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this page. (Lead agencies may modify this form to include additional specific information on project description.) [\[help\]](#)

*Cascadia Development, LLC (Cascadia) intends to develop an Alzheimer's and memory care community consisting of a 45,000 square foot single story building housing 60 units. Site construction will include development of 47 parking spaces, vehicular maneuvering areas, frontage improvements, utility installations, and landscaping.*

12. Location of the proposal. Give sufficient information for a person to understand the precise location of your proposed project, including a street address, if any, and section, township, and range, if known. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map, if reasonably available. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit applications related to this checklist. [\[help\]](#)

*The site is vacant and located at the intersection of S.E. Issaquah-Fall City Road and Highlands Drive N.E. in Issaquah, Washington. The site includes Parcel Nos. 222406-9002 and -9149. See vicinity map attached.*

## **B. Environmental Elements [\[help\]](#)**

### **1. Earth**

- a. General description of the site [\[help\]](#)  
(circle one): Flat, rolling, hilly, steep slopes, mountainous, other \_\_\_\_\_

- b. What is the steepest slope on the site (approximate percent slope)? [\[help\]](#)

*Portions of the site contain 50 percent slopes from prior grading for public roads. No steep slopes exist in the development area.*

- c. What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any agricultural land of long-term commercial significance and whether the proposal results in removing any of these soils. [\[help\]](#)

*According to the NCRS Soil Maps, the site contains Everett gravelly sandy loam (EvC and EvD) soils (see attached).*

- d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe. [\[help\]](#)

*The applicant is not aware of any unstable slopes on the site.*

- e. Describe the purpose, type, total area, and approximate quantities and total affected area of any filling, excavation, and grading proposed. Indicate source of fill. [\[help\]](#)

*Approximately 10,000 cubic yards of grading will occur on the site. Earthwork will be balanced as much as possible. Exported material (if applicable) will be taken to an approved facility. Imported fill will come from an approved source.*

- f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe. [\[help\]](#)

*If temporary erosion and sedimentation controls are not installed prior to construction, erosion could result from grading activities.*

- g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt or buildings)? [\[help\]](#)

*Approximately 26 percent of the site will be covered with impervious surfaces.*

- h. Proposed measures to reduce or control erosion, or other impacts to the earth, if any: [\[help\]](#)

*TESC BMPs should be employed prior to any construction activities. The owner and contractor shall be responsible for compliance and reporting per City regulations and the NPDES Permit that will be required for the project.*

## 2. Air

- a. What types of emissions to the air would result from the proposal during construction, operation, and maintenance when the project is completed? If any, generally describe and give approximate quantities if known. [\[help\]](#)

*During construction, fugitive dust from earth moving equipment could occur. Also, fumes from diesel construction equipment could be generated. These emissions would be*

*temporary and limited to the construction window only. After site development, no emissions are expected.*

- b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe. [\[help\]](#)

*Off-site emissions may include wood smoke from nearby residences as well as emissions from automobiles on nearby roads.*

- c. Proposed measures to reduce or control emissions or other impacts to air, if any: [\[help\]](#)

*If needed during construction, water trucks may be used to control fugitive dust. Street sweepers will be used as needed to control tracking soils off-site during construction. TESC measures, including a construction access road will remove some dirt and dust from construction vehicles leaving the site.*

### 3. Water

- a. Surface Water: [\[help\]](#)

- 1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into. [\[help\]](#)

*The site contains an isolated Category II wetland that is contained within a topographic depression. It does not appear that surface waters leave the site. The site is in the East Lake Sammamish and Issaquah Creek Drainage Basin in the Sammamish River watershed; which lies in Cedar River-Sammamish WRIA 8.*

- 2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans. [\[help\]](#)

*Project activities including construction of the building, parking lot, stormwater facilitates, grading, tree removal, utilities, and retaining wall(s).*

- 3) Estimate the amount of fill and dredge material that would be placed in or removed from surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material. [\[help\]](#)

*No fill or dredge is proposed in the wetland area.*

- 4) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known. [\[help\]](#)

*The project does not propose to withdraw or divert any surface waters. Stormwater from the development project will be treated, detained, and then released to the north side of the wetland buffer. The site drains into the wetland in the southern half of the property.*

- 5) Does the proposal lie within a 100-year floodplain? If so, note location on the site plan. [\[help\]](#)

According to FEMA FIRM No. 53033C0692G the site does not lie within the 100 year flood plain.

- 6) Does the proposal involve any discharges of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge. [\[help\]](#)

*No wastes will be discharged to surface waters. Stormwater from the site will be discharged on site after treatment and detention. The project will be connected to the public sanitary sewer system provided by Sammamish Plateau Water and Sewer District.*

b. Ground Water:

- 1) Will groundwater be withdrawn from a well for drinking water or other purposes? If so, give a general description of the well, proposed uses and approximate quantities withdrawn from the well. Will water be discharged to groundwater? Give general description, purpose, and approximate quantities if known. [\[help\]](#)

*No groundwater withdrawals are proposed. The project will be connected to the public water supply provided by Sammamish Plateau Water and Sewer District.*

- 2) Describe waste material that will be discharged into the ground from septic tanks or other sources, if any (for example: Domestic sewage; industrial, containing the following chemicals. . . ; agricultural; etc.). Describe the general size of the system, the number of such systems, the number of houses to be served (if applicable), or the number of animals or humans the system(s) are expected to serve. [\[help\]](#)

*This item does not apply as no waste material will be discharged.*

c. Water runoff (including stormwater):

- 1) Describe the source of runoff (including storm water) and method of collection and disposal, if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe.

*The project generated stormwater runoff will come from new impervious surfaces, including buildings, sidewalks, parking areas, and street improvements. Stormwater will be collected in a series of catch basins and pipes and routed to the on-site detention and water quality system. After detention and treatment, stormwater will be released to the natural location so it will go into the wetland onsite.*

- 2) Could waste materials enter ground or surface waters? If so, generally describe.

*The water quality treatment of project generated stormwater includes a sand filter vault that will treat pollutants prior to discharge.*

- 3) Does the proposal alter or otherwise affect drainage patterns in the vicinity of the site? If so, describe.

*The project does not propose to alter drainage patterns. The site currently drains to the wetland in the southern half of the site. In the developed condition, the proposal releases treated stormwater to the edge of the wetland buffer to maintain existing drainage flow patterns.*

- d. Proposed measures to reduce or control surface, ground, and runoff water, and drainage pattern impacts, if any:

*The project proposes a detention vault followed by a sand filter vault for stormwater detention and water quality.*

**4. Plants [\[help\]](#)**

- a. Check the types of vegetation found on the site: [\[help\]](#)

- deciduous tree: alder, maple, aspen, other
- evergreen tree: fir, cedar, pine, other
- shrubs
- grass
- pasture
- crop or grain
- Orchards, vineyards or other permanent crops.
- wet soil plants: cattail, buttercup, bullrush, skunk cabbage, other
- water plants: water lily, eelgrass, milfoil, other
- other types of vegetation

- b. What kind and amount of vegetation will be removed or altered? [\[help\]](#)

*Trees and other vegetation will be removed for construction of the site improvements, building, parking lot, and stormwater facilities.*

- c. List threatened and endangered species known to be on or near the site. [\[help\]](#)

*The applicant is not aware of any threatened or endangered plant species on or near the site.*

- d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any: [\[help\]](#)

*The project will retain trees on the site to the extent feasible for the project's footprint of site development. A mix of native and ornamental landscaping will be planted with the project.*

- e. List all noxious weeds and invasive species known to be on or near the site.

This item is unknown.

**5. Animals**

- a. List any birds and other animals which have been observed on or near the site or are known to be on or near the site. Examples include: [\[help\]](#)

- birds: hawk, heron, eagle, songbirds, other: \_\_\_\_\_
- mammals: deer, bear, elk, beaver, other: \_\_\_\_\_
- fish: bass, salmon, trout, herring, shellfish, other \_\_\_\_\_

- b. List any threatened and endangered species known to be on or near the site. [\[help\]](#)

- c. Is the site part of a migration route? If so, explain. [\[help\]](#)

*The site may fall within the Pacific Flyway for migratory birds.*

- d. Proposed measures to preserve or enhance wildlife, if any: [\[help\]](#)

*Tree retention and the remaining protected wetland will preserve wildlife habitat that may exist on the site.*

- e. List any invasive animal species known to be on or near the site.

*The applicant is not aware of any invasive animal species on the site.*

## 6. Energy and natural resources

- a. What kinds of energy (electric, natural gas, oil, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc. [\[help\]](#)

*The project will use electricity and natural gas for its energy needs. Electricity will be used for lighting, heating, and other equipment. Natural gas will be used for heating and other equipment. Generators are likely to be available for backup electricity.*

- b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe. [\[help\]](#)

*The project's building height complies with zoning limits and will not interfere with neighboring properties potential use of solar power.*

- c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts, if any: [\[help\]](#)

*The project will comply with Washington State Energy Code.*

## 7. Environmental health

- a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste, that could occur as a result of this proposal? If so, describe. [\[help\]](#)

*The project does not pose an environmental risk for toxic chemicals or waste.*

- 1) Describe any known or possible contamination at the site from present or past uses.

*The applicant is not aware of any prior contamination of the site. The site is vacant.*

- 2) Describe existing hazardous chemicals/conditions that might affect project development and design. This includes underground hazardous liquid and gas transmission pipelines located within the project area and in the vicinity.

*The project is not at risk from existing hazardous chemicals or transmission pipelines.*

- 3) Describe any toxic or hazardous chemicals that might be stored, used, or produced during the project's development or construction, or at any time during the operating life of the project.

*The project is a memory care facility that may dispense limited medications to clients. Medications are not toxic or hazardous.*

- 4) Describe special emergency services that might be required.

*No special emergency services are anticipated.*

- 5) Proposed measures to reduce or control environmental health hazards, if any:

*The proposed memory care facility will be equipped with the state and federally mandated safety equipment typical for the use.*

**b. Noise**

- 1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)? [\[help\]](#)

*Existing noise in the area is from adjacent public roads. Road noise is not expected to impact the project.*

- 2) What types and levels of noise would be created by or associated with the project on a short-term or a long-term basis (for example: traffic, construction, operation, other)? Indicate what hours noise would come from the site. [\[help\]](#)

*In the short term, noise will be generated by construction activities and equipment. In the long term, the project's noise may be from equipment such as generators, heat pumps, etc. Noise impacts are expected to be minimal.*

- 3) Proposed measures to reduce or control noise impacts, if any: [\[help\]](#)

*No noise reduction measures are expected to be necessary.*

**8. Land and shoreline use**

- a. What is the current use of the site and adjacent properties? Will the proposal affect current land uses on nearby or adjacent properties? If so, describe. [\[help\]](#)

*The site is vacant and surrounded by public roads to the west, north, and east. To the south is a wetland. West of the site is Summerhill Village Condominium. East of the site are single family homes.*

- b. Has the project site been used as working farmlands or working forest lands? If so, describe. How much agricultural or forest land of long-term commercial significance will be converted to other uses as a result of the proposal, if any? If resource lands have not been designated, how many acres in farmland or forest land tax status will be converted to nonfarm or nonforest use? [\[help\]](#)

*The applicant is not aware of prior use of the site for farming or farmland.*

- 1) Will the proposal affect or be affected by surrounding working farm or forest land normal business operations, such as oversize equipment access, the application of pesticides, tilling, and harvesting? If so, how:

*The project will not impact any farming or forest land operations as these do not exist in the area.*

- c. Describe any structures on the site. [\[help\]](#)

*The site contains no structures.*

- d. Will any structures be demolished? If so, what? [\[help\]](#)

*No structures will be demolished.*

- e. What is the current zoning classification of the site? [\[help\]](#)

*The site is split zoned - the north half is MF-M and the south half is SF-SL.*

- f. What is the current comprehensive plan designation of the site? [\[help\]](#)

*Low density residential (south) and Multifamily Residential (north).*

- g. If applicable, what is the current shoreline master program designation of the site? [\[help\]](#)

*The site is not in any shoreline jurisdiction; therefore, this does not apply.*

- h. Has any part of the site been classified as a critical area by the city or county? If so, specify. [\[help\]](#)

*The site contains a Category II wetland.*

- i. Approximately how many people would reside or work in the completed project? [\[help\]](#)

*Approximately 50 people will work on the site on shifts.*

- j. Approximately how many people would the completed project displace? [\[help\]](#)

*No people will be displaced by the project; the site is vacant and undeveloped.*

- k. Proposed measures to avoid or reduce displacement impacts, if any: [\[help\]](#)

*This item does not apply.*

- l. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any: [\[help\]](#)

*The proposed use is consistent with the zoning requirements as it a permitted use. The project is designed to comply with setbacks, parking standards, landscaping, etc.*

- m. Proposed measures to ensure the proposal is compatible with nearby agricultural and forest lands of long-term commercial significance, if any:

*Tree retention will be employed to the extent feasible.*

## 9. Housing

- a. Approximately how many units would be provided, if any? Indicate whether high, middle, or low-income housing. [\[help\]](#)

*The project is a memory care facility with 68 beds.*

- b. Approximately how many units, if any, would be eliminated? Indicate whether high, middle, or low-income housing. [\[help\]](#)

*This item does not apply as this is not a housing project.*

- c. Proposed measures to reduce or control housing impacts, if any: [\[help\]](#)

*This item does not apply as this is not a housing project.*

## 10. Aesthetics

- a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed? [\[help\]](#)

*The tallest portion of the building will be 34 feet tall.*

- b. What views in the immediate vicinity would be altered or obstructed? [\[help\]](#)

*No views will be obstructed by the project.*

- c. Proposed measures to reduce or control aesthetic impacts, if any: [\[help\]](#)

*The design of the facility uses attractive architecture, materials, and colors.*

## 11. Light and glare

- a. What type of light or glare will the proposal produce? What time of day would it mainly occur? [\[help\]](#)

*The project could produce light during evening hours from parking lot lighting and building lighting.*

- b. Could light or glare from the finished project be a safety hazard or interfere with views? [\[help\]](#)

*Light from the project will not create a safety hazard.*

- c. What existing off-site sources of light or glare may affect your proposal? [\[help\]](#)

*Any existing off-site lighting from street lights or surrounding uses are not expected to affect the project.*

- d. Proposed measures to reduce or control light and glare impacts, if any:

If needed, parking lot lights will be equipped with cutoffs to minimize light leaving the site.

## 12. Recreation

- a. What designated and informal recreational opportunities are in the immediate vicinity? [\[help\]](#)

*Regionally, there are enumerable recreation opportunities. None exists on the site.*

- b. Would the proposed project displace any existing recreational uses? If so, describe. [\[help\]](#)

*The project will not displace or impact any existing recreational opportunities.*

- c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any: [\[help\]](#)

*The project will not reduce or use recreation opportunities.*

## 13. Historic and cultural preservation

- a. Are there any buildings, structures, or sites, located on or near the site that are over 45 years old listed in or eligible for listing in national, state, or local preservation registers located on or near the site? If so, specifically describe. [\[help\]](#)

*Yes, cultural resources have been identified on the site.*

- b. Are there any landmarks, features, or other evidence of Indian or historic use or occupation? This may include human burials or old cemeteries. Are there any material evidence, artifacts, or areas of cultural importance on or near the site? Please list any professional studies conducted at the site to identify such resources. [\[help\]](#)

*Yes. There have been some survey and testing for cultural resources.*

- c. Describe the methods used to assess the potential impacts to cultural and historic resources on or near the project site. Examples include consultation with tribes and the department of archeology and historic preservation, archaeological surveys, historic maps, GIS data, etc. [\[help\]](#)

*The applicant is coordinating mitigation with the Snoqualmie Tribe and Department of Archaeology and Historic Preservation (DAHP).*

- d. Proposed measures to avoid, minimize, or compensate for loss, changes to, and disturbance to resources. Please include plans for the above and any permits that may be required.

*The project will minimize grading activities to the project area. A DAHP permit may be required.*

## 14. Transportation

- a. Identify public streets and highways serving the site or affected geographic area and describe proposed access to the existing street system. Show on site plans, if any. [\[help\]](#)

*The site is served by SE Issaquah-Fall City Road and Highlands Drive NE. Access is solely from SE Issaquah-Fall City Road.*

- b. Is the site or affected geographic area currently served by public transit? If so, generally describe. If not, what is the approximate distance to the nearest transit stop? [\[help\]](#)

*The site is not served by transit within 1/4 mile.*

- c. How many additional parking spaces would the completed project or non-project proposal have? How many would the project or proposal eliminate? [\[help\]](#)

*The project provides 47 parking stalls and one loading space.*

- d. Will the proposal require any new or improvements to existing roads, streets, pedestrian, bicycle or state transportation facilities, not including driveways? If so, generally describe (indicate whether public or private). [\[help\]](#)

*The proposal will add curb, gutter, and sidewalk to SE Issaquah-Fall City Road along its frontage.*

- e. Will the project or proposal use (or occur in the immediate vicinity of) water, rail, or air transportation? If so, generally describe. [\[help\]](#)

*The project does not occur in the vicinity of or use water, rail, or air transportation.*

- f. How many vehicular trips per day would be generated by the completed project or proposal? If known, indicate when peak volumes would occur and what percentage of the volume would be trucks (such as commercial and nonpassenger vehicles). What data or transportation models were used to make these estimates? [\[help\]](#)

*Traffic is expected to be minimal as the residents will not drive. There are approximately 15 employees per shift with three shifts.*

- g. Will the proposal interfere with, affect or be affected by the movement of agricultural and forest products on roads or streets in the area? If so, generally describe.

*The surrounding area is not agricultural or forest product related; therefore, this item is not expected to apply.*

- h. Proposed measures to reduce or control transportation impacts, if any: [\[help\]](#)

*No transportation impacts will result from the proposal.*

## 15. Public services

- a. Would the project result in an increased need for public services (for example: fire protection, police protection, public transit, health care, schools, other)? If so, generally describe. [\[help\]](#)

*An incremental increased need for public services (health care, fire, and police) will be generated by the proposal.*

- b. Proposed measures to reduce or control direct impacts on public services, if any. [help]

*The project will pay required impact fees to offset its impact on public services.*

#### 16. Utilities

- a. Circle utilities currently available at the site: [help]

electricity, natural gas, water, refuse service, telephone, sanitary sewer, septic system, other \_\_\_\_\_

*The site is vacant but all utilities are available in the adjacent rights-of-way.*

- b. Describe the utilities that are proposed for the project, the utility providing the service, and the general construction activities on the site or in the immediate vicinity which might be needed. [help]

*The project will install utilities (water, sewer, and stormwater) including pipes 8-inches and larger in diameter on- and off-site. Any impacts to critical areas will be mitigated as required. Utility trenching and construction may be needed in the public road system.*

#### C. Signature [HELP]

The above answers are true and complete to the best of my knowledge. I understand that the lead agency is relying on them to make its decision.

Signature: \_\_\_\_\_

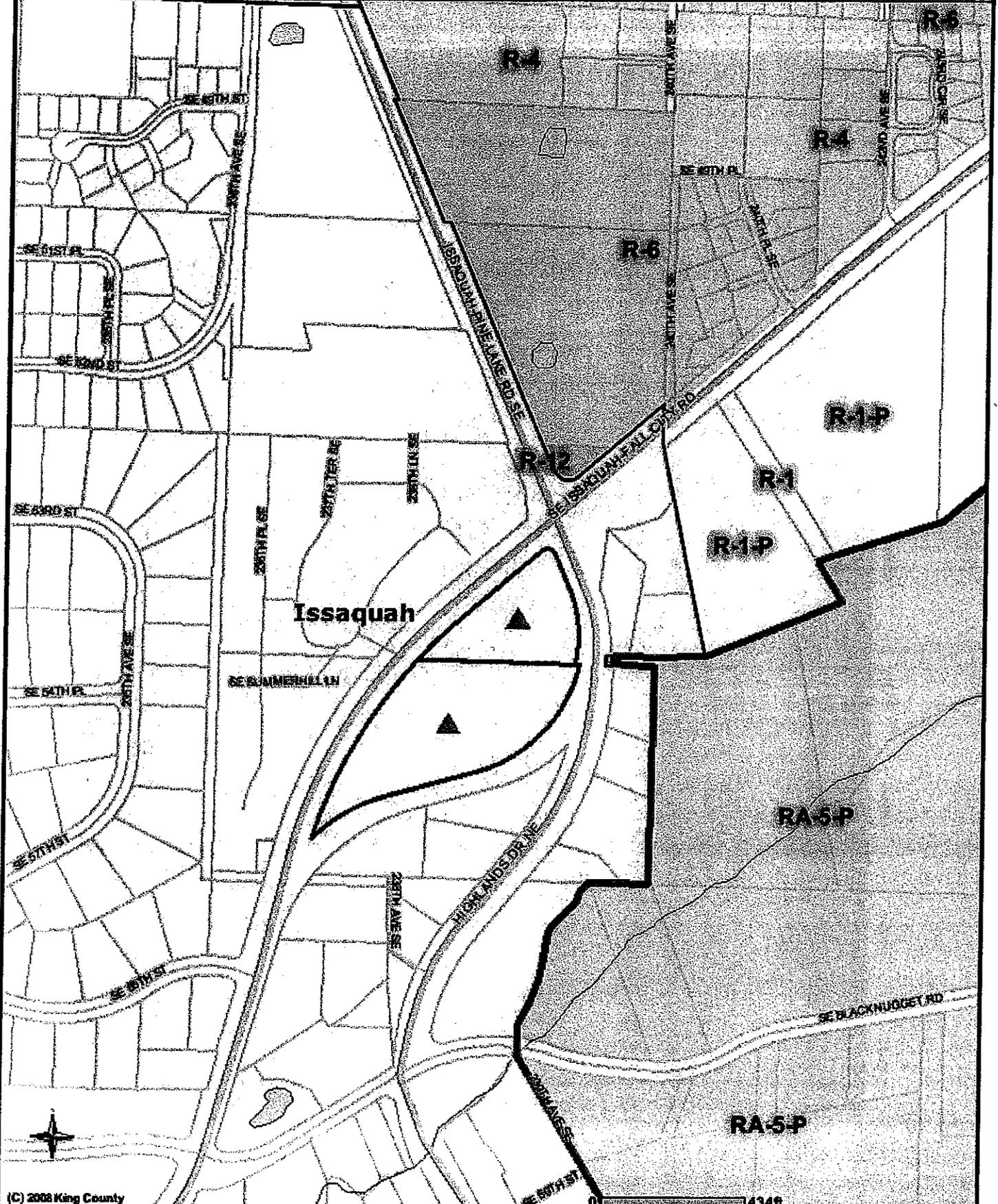


Name of signee \_\_\_\_\_ Ivana Halvorsen \_\_\_\_\_

Position and Agency/Organization Senior Planner, Barghausen Consulting Engineers, Inc.

Date Submitted: \_\_\_\_\_ March 23, 2015 \_\_\_\_\_

# Proposed Memory Care Facility



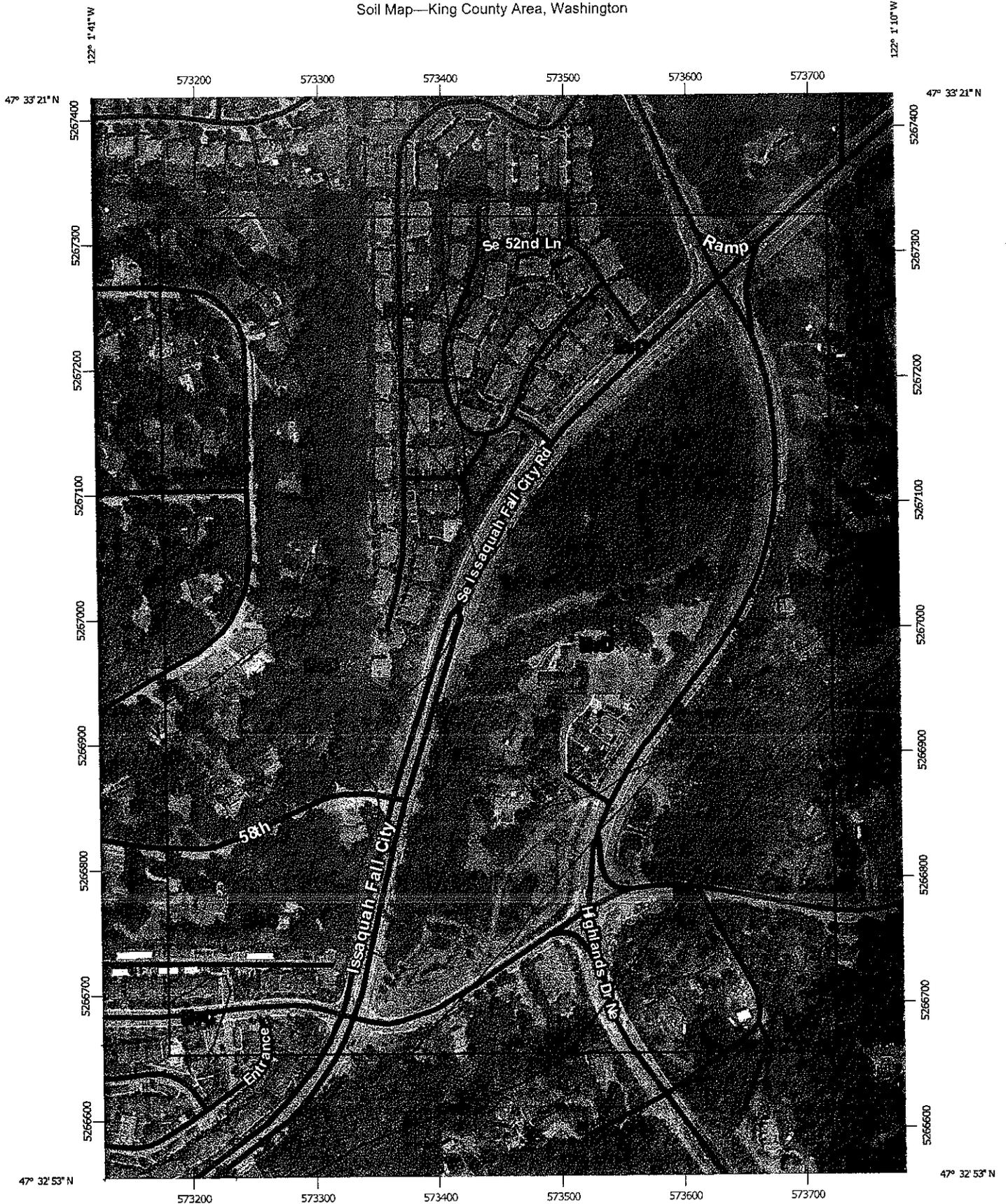
(C) 2008 King County

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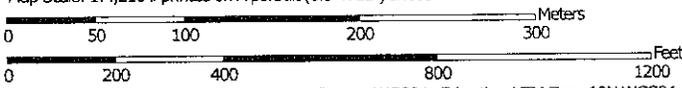
Date: 3/12/2014 Source: King County IMAP - Property Information (<http://www.metrokc.gov/GIS/IMAP>)



Soil Map—King County Area, Washington



Map Scale: 1:4,210 if printed on A portrait (8.5" x 11") sheet.



Map projection: Web Mercator Corner coordinates: WGS84 Edge tics: UTM Zone 10N WGS84



Natural Resources Conservation Service

Web Soil Survey National Cooperative Soil Survey

### MAP LEGEND

- |  |  |   |  |
|--|--|---|--|
| <b>Area of Interest (AOI)</b>  |  |  Spoil Area            |  |
|  Area of Interest (AOI) |  |  Stony Spot            |  |
| <b>Soils</b>   |  |  Very Stony Spot       |  |
|  Soil Map Unit Polygons |  |  Wet Spot              |  |
|  Soil Map Unit Lines    |  |  Other                 |  |
|  Soil Map Unit Points   |  |  Special Line Features |  |
| <b>Special Point Features</b>  |  | <b>Water Features</b>   |  |
|  Blowout                |  |  Streams and Canals    |  |
|  Borrow Pit             |  | <b>Transportation</b>   |  |
|  Clay Spot              |  |  Rails                 |  |
|  Closed Depression      |  |  Interstate Highways   |  |
|  Gravel Pit             |  |  US Routes             |  |
|  Gravelly Spot          |  |  Major Roads           |  |
|  Landfill               |  |  Local Roads           |  |
|  Lava Flow              |  | <b>Background</b>   |  |
|  Marsh or swamp         |  |  Aerial Photography    |  |
|  Mine or Quarry         |  |   |  |
|  Miscellaneous Water    |  |   |  |
|  Perennial Water        |  |   |  |
|  Rock Outcrop           |  |   |  |
|  Saline Spot            |  |   |  |
|  Sandy Spot            |  |   |  |
|  Severely Eroded Spot |  |   |  |
|  Sinkhole             |  |   |  |
|  Slide or Slip        |  |   |  |
|  Sodic Spot           |  |   |  |

### MAP INFORMATION

The soil surveys that comprise your AOI were mapped at 1:24,000.

**Warning:** Soil Map may not be valid at this scale.  
 Enlargement of maps beyond the scale of mapping can cause misunderstanding of the detail of mapping and accuracy of soil line placement. The maps do not show the small areas of contrasting soils that could have been shown at a more detailed scale.

Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service  
 Web Soil Survey URL: <http://websoilsurvey.nrcs.usda.gov>  
 Coordinate System: Web Mercator (EPSG:3857)

Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more accurate calculations of distance or area are required.

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: King County Area, Washington  
 Survey Area Data: Version 10, Sep 30, 2014

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Jul 8, 2014—Jul 15, 2014

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

## Map Unit Legend

King County Area, Washington (WA633)			
Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
BeC	Beausite gravelly sandy loam, 6 to 15 percent slopes	23.4	25.9%
BeD	Beausite gravelly sandy loam, 15 to 30 percent slopes	15.0	16.6%
Bh	Bellingham silt loam	15.3	17.0%
EvC	Everett gravelly sandy loam, 5 to 15 percent slopes	16.2	18.0%
EvD	Everett gravelly sandy loam, 15 to 30 percent slopes	19.5	21.6%
NeC	Neilton very gravelly loamy sand, 2 to 15 percent slopes	0.7	0.8%
<b>Totals for Area of Interest</b>		<b>90.1</b>	<b>100.0%</b>

## King County Area, Washington

### EvC—Everett gravelly sandy loam, 5 to 15 percent slopes

#### Map Unit Setting

*National map unit symbol:* 1hmt3

*Mean annual precipitation:* 30 to 45 inches

*Mean annual air temperature:* 50 degrees F

*Frost-free period:* 180 days

*Farmland classification:* Farmland of statewide importance

#### Map Unit Composition

*Everett and similar soils:* 100 percent

*Estimates are based on observations, descriptions, and transects of the mapunit.*

#### Description of Everett

##### Setting

*Landform:* Terraces

*Parent material:* Glacial outwash with a component of volcanic ash in the upper part

##### Typical profile

*H1 - 0 to 17 inches:* gravelly ashy sandy loam

*H2 - 17 to 32 inches:* very gravelly sandy loam

*H3 - 32 to 60 inches:* very gravelly coarse sand

##### Properties and qualities

*Slope:* 5 to 15 percent

*Depth to restrictive feature:* More than 80 inches

*Natural drainage class:* Somewhat excessively drained

*Capacity of the most limiting layer to transmit water (Ksat):* High (1.98 to 5.95 in/hr)

*Depth to water table:* More than 80 inches

*Frequency of flooding:* None

*Frequency of ponding:* None

*Available water storage in profile:* Low (about 5.0 inches)

##### Interpretive groups

*Land capability classification (irrigated):* None specified

*Land capability classification (nonirrigated):* 3e

*Hydrologic Soil Group:* A

*Other vegetative classification:* Droughty Soils (G002XN402WA)

## Data Source Information

Soil Survey Area: King County Area, Washington

Survey Area Data: Version 10, Sep 30, 2014

## King County Area, Washington

### EvD—Everett gravelly sandy loam, 15 to 30 percent slopes

#### Map Unit Setting

*National map unit symbol:* 1hmt4  
*Mean annual precipitation:* 30 to 45 inches  
*Mean annual air temperature:* 50 degrees F  
*Frost-free period:* 180 days  
*Farmland classification:* Farmland of statewide importance

#### Map Unit Composition

*Everett and similar soils:* 100 percent  
*Estimates are based on observations, descriptions, and transects of the mapunit.*

#### Description of Everett

##### Setting

*Landform:* Terraces  
*Parent material:* Glacial outwash with a component of volcanic ash in the upper part

##### Typical profile

*H1 - 0 to 17 inches:* gravelly ashy sandy loam  
*H2 - 17 to 32 inches:* very gravelly sandy loam  
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*Capacity of the most limiting layer to transmit water (Ksat):* High (1.98 to 5.95 in/hr)  
*Depth to water table:* More than 80 inches  
*Frequency of flooding:* None  
*Frequency of ponding:* None  
*Available water storage in profile:* Low (about 5.0 inches)

##### Interpretive groups

*Land capability classification (irrigated):* None specified  
*Land capability classification (nonirrigated):* 4e  
*Hydrologic Soil Group:* A  
*Other vegetative classification:* Droughty Soils (G002XN402WA)

## Data Source Information

Soil Survey Area: King County Area, Washington  
Survey Area Data: Version 10, Sep 30, 2014







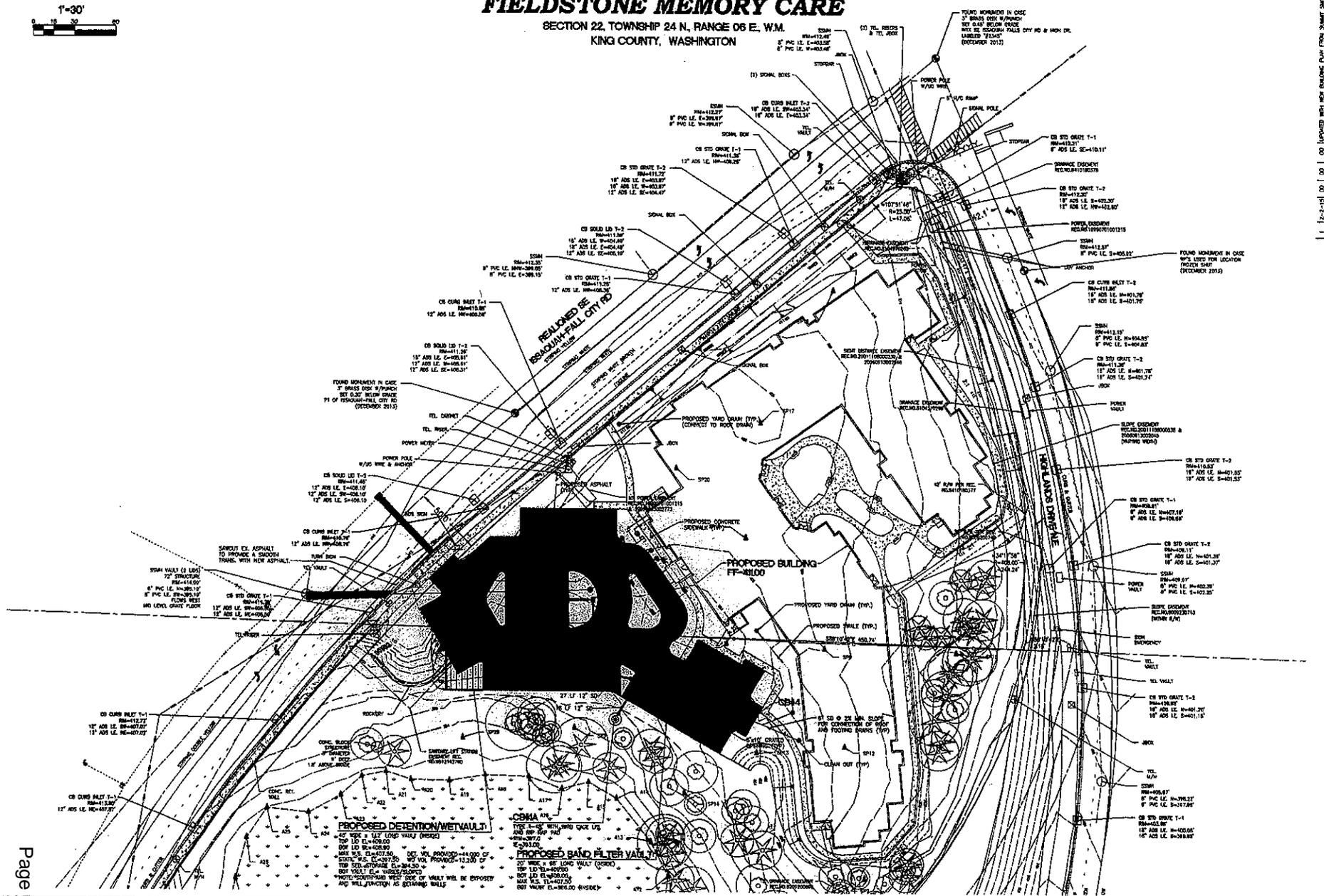




# PRELIMINARY ROADWAY AND STORM DRAINAGE PLAN

## FOR FIELDSTONE MEMORY CARE

SECTION 22, TOWNSHIP 24 N, RANGE 06 E, W.M.  
KING COUNTY, WASHINGTON



**PROPOSED DETENTION/WETVAULT**  
 TYPE 1 - 12' WIDE, 10' DEEP  
 12' WIDE x 12' LONG VASIT (INSIDE)  
 TOP LID EL=408.00  
 SIDEWALL EL=408.00  
 BOTTOM EL=408.00  
 12' WIDE x 12' LONG VASIT (INSIDE)  
 TOP LID EL=408.00  
 SIDEWALL EL=408.00  
 BOTTOM EL=408.00  
 12' WIDE x 12' LONG VASIT (INSIDE)  
 TOP LID EL=408.00  
 SIDEWALL EL=408.00  
 BOTTOM EL=408.00

**PROPOSED BAND FILTER VAULT**  
 TYPE 1 - 12' WIDE, 10' DEEP  
 12' WIDE x 12' LONG VASIT (INSIDE)  
 TOP LID EL=408.00  
 SIDEWALL EL=408.00  
 BOTTOM EL=408.00  
 12' WIDE x 12' LONG VASIT (INSIDE)  
 TOP LID EL=408.00  
 SIDEWALL EL=408.00  
 BOTTOM EL=408.00

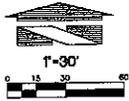
<p>1651</p> <p>18215 7900 AVENUE SOUTH        SEASIDE, WA 98148        (206) 251-8222 FAX        (206) 251-8782 FAX        ONE ENGINEERING, LAND PLANNING        SURVEYING, ENVIRONMENTAL</p>	<p>18215 7900 AVENUE SOUTH        SEASIDE, WA 98148        (206) 251-8222 FAX        (206) 251-8782 FAX        ONE ENGINEERING, LAND PLANNING        SURVEYING, ENVIRONMENTAL</p>	<p>18215 7900 AVENUE SOUTH        SEASIDE, WA 98148        (206) 251-8222 FAX        (206) 251-8782 FAX        ONE ENGINEERING, LAND PLANNING        SURVEYING, ENVIRONMENTAL</p>	<p>18215 7900 AVENUE SOUTH        SEASIDE, WA 98148        (206) 251-8222 FAX        (206) 251-8782 FAX        ONE ENGINEERING, LAND PLANNING        SURVEYING, ENVIRONMENTAL</p>
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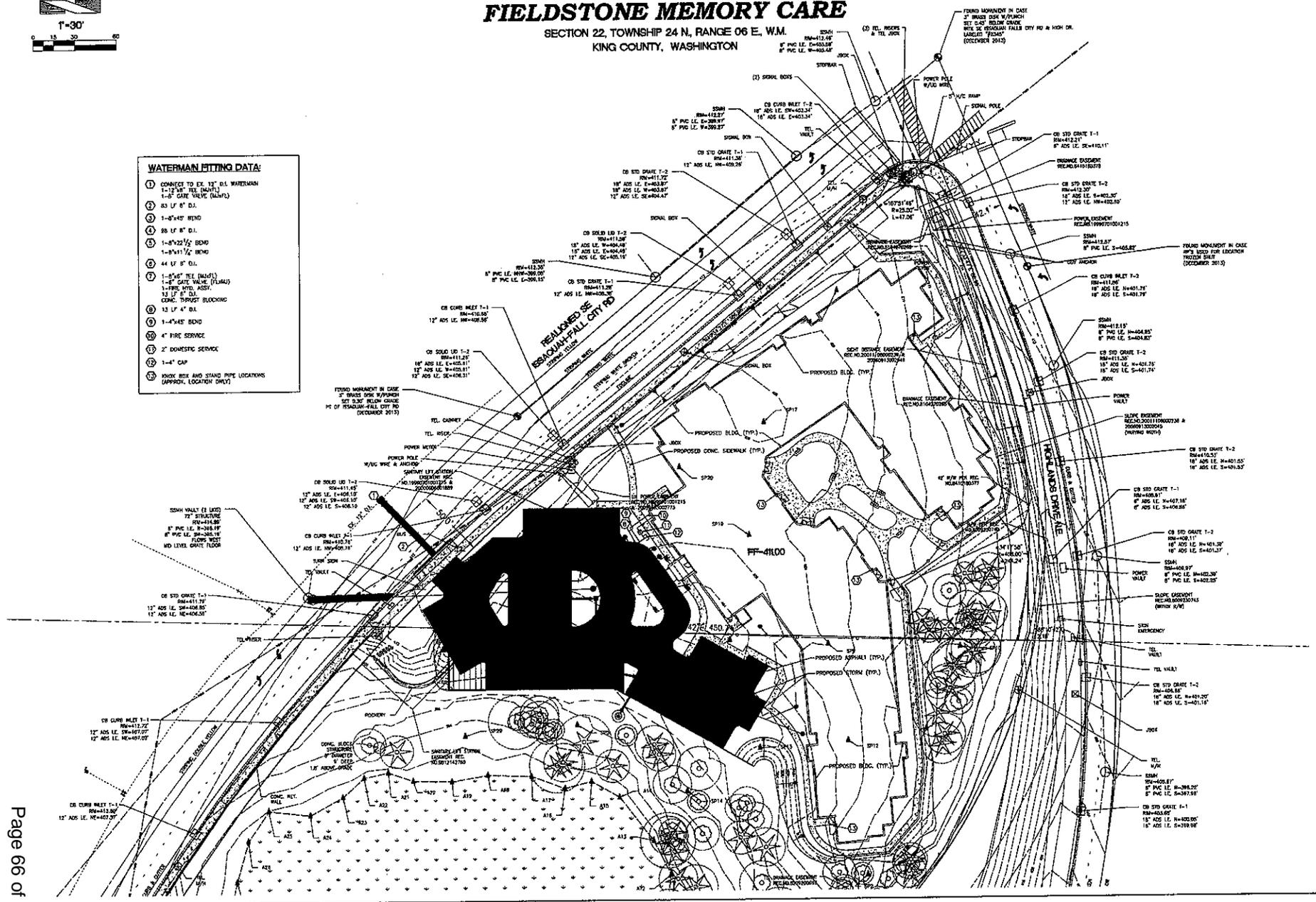
# PRELIMINARY WATER PLAN FOR **FIELDSTONE MEMORY CARE**

SECTION 22, TOWNSHIP 24 N, RANGE 06 E, W.M.  
KING COUNTY, WASHINGTON



### WATERMAN FITTING DATA:

- 1 CONNECT TO EX. 12" D.I. WATERMAN  
1-1/2" 90° TEL. (VALVE)
- 2 1-2" GATE VALVE (VALVE)
- 3 83 LF 8" D.I.
- 4 1-8" 45° BEND
- 5 88 LF 8" D.I.
- 6 1-8" 22 1/2° BEND
- 7 1-8" 11 1/2° BEND
- 8 44 LF 8" D.I.
- 9 1-8" 90° TEL. (VALVE)
- 10 1-2" GATE VALVE (FLANK)
- 11 1-1/2" 90° TEL. (VALVE)
- 12 13 LF 8" D.I.
- 13 13 LF 8" D.I.
- 14 13 LF 8" D.I.
- 15 1-4" 45° BEND
- 16 4" FIRE SERVICE
- 17 2" DOMESTIC SERVICE
- 18 1-4" CAP
- 19 FLOOR BOX AND STAND PIPE LOCATIONS  
(APPROX. LOCATION ONLY)



PRELIMINARY WATER PLAN  
FOR  
**FIELDSTONE MEMORY CARE**

Summit Smith Healthcare Facilities  
6737 West Washington Street, Suite 3245  
Milwaukee, WI 53214  
Contact: Andy Palec

DATE: 10/25/14

SCALE: 1" = 30'

DESIGNED BY: JCS

CHECKED BY: JCS

APPROVED BY: JCS

19215 72ND AVENUE SOUTH  
SUITE 202  
420/251-8222 FAX  
420/251-8788 FAX  
CIVIL ENGINEERING, LAND PLANNING  
SURVEYING, ENVIRONMENTAL SERVICES

**BAGHAUSEN** & COMPANY  
ENGINEERS

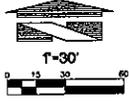
JOB NUMBER: 16161

SHEET: 1

10/25/14



**PRELIMINARY TREE RETENTION PLAN**  
 FOR  
**FIELDSTONE MEMORY CARE**  
 SECTION 22, TOWNSHIP 24 N., RANGE 06 E., W.M.  
 KING COUNTY, WASHINGTON



**SIGNIFICANT TREE LEGEND**

- ★ CONIFER TO REMAIN
- DECIDUOUS TO REMAIN
- TREE PROTECTION AREA (TPA TYP)
- ✱ CONIFER TO BE REMOVED
- DECIDUOUS TO BE REMOVED

**SOURCE OF SIGNIFICANT TREES**

FIELD SURVEYED BY:  
 BARGHAUSEN CONSULTING ENGINEERS, INC. IN DECEMBER 2013

POINT #	DATE (DBP)	SIZE (DBH)	SPECIES	POINT #	DATE (DBP)	SIZE (DBH)	SPECIES	POINT #	DATE (DBP)	SIZE (DBH)	SPECIES	POINT #	DATE (DBP)	SIZE (DBH)	SPECIES
300000	11"	MAPLE	NO	300001	12"	CHERRY	NO	300002	12"	FR	NO	300003	12"	FR	NO
300004	8"	MAPLE	NO	300005	14"	CHERRY	NO	300006	8"	MAPLE	NO	300007	12"	MAPLE	NO
300008	10"	MAPLE	NO	300009	8"	CHERRY	NO	300010	8"	MAPLE	NO	300011	12"	MAPLE	NO
300012	8"	CHERRY	NO	300013	10"	MAPLE	NO	300014	10"	FR	NO	300015	12"	MAPLE	NO
300018	8"	MAPLE	NO	300019	10"	MAPLE	NO	300020	12"	FR	NO	300021	12"	FR	NO
300024	8"	MAPLE	NO	300025	10"	FR	NO	300026	12"	FR	NO	300027	12"	FR	NO
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30061															

**CITY OF ISSAQUAH  
MITIGATED DETERMINATION OF NONSIGNIFICANCE (MDNS)**

**Description of Proposal:** Construct an assisted living memory care facility consisting of a one-story 45,000 SF building with 60 units on a 6.5 acre site. The proposed development includes 46 surface parking spaces, landscaping, utility installation and street frontage improvements. There is a 1.2 acre Category II wetland located on the south portion of the site and a 75-foot wetland buffer is required. The proposal includes wetland buffer averaging; reducing the buffer by 3,026 SF to a minimum buffer width of 60 feet, and adding 3,393 SF of buffer replacement area. The site would be accessed from a driveway off Issaquah-Fall City Road, approximately 300 feet southwest from the intersection with Highlands Drive NE.

**Proponent:** Cascadia Development, LLC  
4120 Englewood Ave.  
Yakima, WA 98908  
Attn: Justin Younker

**Permit Number:** SDP15-00001

**Location of Proposal:** South corner of SE Issaquah-Fall City Rd. and Highlands Dr. NE.

**Lead Agency:** City of Issaquah

**Determination:** The lead agency for this proposal has determined that it does not have a probable significant adverse impact on the environment. An environmental impact statement is not required under RCW 43.21C.030(2)(c). This decision was made after review of a completed environmental checklist and other information on file with the lead agency. This information is available to the public on request.

**Comment/Appeal Period:** This MDNS is issued under WAC 197-11-340(2) and 197-11-680(3)(a)vii. There is a 21-day combined comment/appeal period for this determination, between **August 20, 2015 and September 10, 2015**. Anyone wishing to comment may submit written comments to the Responsible Official. The Responsible Official will reconsider the determination based on timely comments. Any person aggrieved by this determination may appeal by filing a Notice of Appeal with the City of Issaquah Permit Center. Appellants should prepare specific factual objections. Copies of the environmental determination and other project application materials are available from the Issaquah Development Services Department, 1775 12th Avenue NW.

Appeals of this SEPA determination must be consolidated with appeal of the underlying permit, per IMC 18.04.250.

**Notes:**

- 1) This threshold determination is based on review of the construction plans received June 26, 2015; Wetland Report (Altmann Oliver Associates) dated April 28, 2014; Preliminary Detention and Water Quality Analysis (Barghausen Engineers) dated October 15, 2014; Traffic Impact Analysis (TENW) dated June 24, 2015; environmental checklist dated and received March 23, 2015; and other documents in the file.
- 2) Issuance of this threshold determination does not constitute approval of the permit. The proposal will be reviewed for compliance with all applicable City of Issaquah codes, which regulate development activities, including the Land Use Code, Critical Area Regulations, Building Codes, Clearing and Grading Ordinance, and Surface Water Design Manual.

## Findings:

1. Critical Areas – There is a 1.2 acre Category II wetland located on the south portion of the site. The wetland is an isolated topographic depression and includes a mix of palustrine forested, scrub-shrub, and emergent plant communities. Category II wetlands with 16 habitat points require a 75-foot wetland buffer plus a 15-foot building setback from the buffer. The proposal avoids direct wetland impacts. Wetland buffer averaging is proposed; reducing the buffer by 3,026 SF to a minimum buffer width of 60 feet, and adding 3,393 SF of buffer replacement area. The proposed buffer averaging is consistent with the City’s Critical Areas Regulations; limiting buffer reductions/buffer averaging to 25% of the standard buffer width and providing an equal buffer replacement area. The north part of the wetland buffer adjacent to the development area and the buffer replacement area is presently forested. The south wetland buffer area has fewer trees and is more suitable for wetland buffer enhancement. The applicant shall enhance the wetland buffer at a 1:1 ratio, an area equal to the 3,026 SF buffer reduction area.

Final wetland buffer enhancement plans are required for approval by the Issaquah Development Services Department (DSD) prior to issuing construction permits. Final plans shall include a planting plan and a 5-year monitoring/maintenance plan with performance standards for monitoring success of the enhancement planting. The plans shall meet standards of the King County Critical Areas Mitigation Guidelines for the planting density and monitoring performance standards.

2. Cultural Resources – There is an archaeological site present on the site that is protected under State law (RCW 27.53). There has been an Archaeological Excavation Permit, issued by the Washington State Department of Archeology and Historic Preservation (DAHP), for archaeological testing and data recovery. A permit from the Washington State Department of Archeology and Historic Preservation (DAHP) shall be obtained under RCW 27.53 for archaeological monitoring of construction, prior to issuance of construction permits. A professional archaeologist shall be onsite during ground disturbance. The applicant shall consult with affected Tribes on appropriate content and media for historic public education and/or art element to be developed and installed on the project site.
3. Traffic – A Traffic Impact Study (TENW, June 24, 2015) was provided to estimate traffic trip generation from the project, evaluate the site access, and to address traffic impacts and mitigation related to the development proposal. The study concludes the proposal would generate 186 weekday daily trips, with 12 trips occurring during the weekday AM peak hour, and 20 trips during the PM peak hour. The trip generation estimate is based on the methodology included in the institute of Transportation Engineers (ITE) *Trip Generation Manual*.

The City completed a system-wide transportation concurrency assessment; modeling future planned growth and the road improvements necessary to maintain the City’s adopted level of service (LOS) standards. Transportation impact fees were adopted to fund the road improvements (Ordinance #2733, effective February 2, 2015). Under the City’s new concurrency standards, individual development applications are not required to address their traffic impacts on the City’s local street system, provided a proposal is consistent with the growth assumptions previously evaluated in the traffic concurrency model and the applicant pay traffic impact fees to fund the identified road improvements. The subject proposal is consistent with the growth assumptions in the traffic concurrency model. Therefore, the proposed development can withdraw trips from the “trip bank” that was calculated for concurrency and can mitigate their traffic impacts by payment of the traffic impact fee. The traffic impact fee will be used by the City to fund transportation improvements identified in the concurrency model and on the City’s Transportation Improvement Program (TIP).

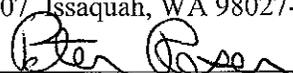
However, the concurrency assessment doesn't address traffic operations and safety at the project site driveway access or at non-concurrency intersections. The site would be accessed from a driveway off Issaquah-Fall City Road, approximately 300 feet southwest from the intersection with Highlands Drive NE. The traffic report evaluated the site access for level of service (LOS) and queuing, site entering and stopping distance, and turn lane analysis. The analysis assumed turn movements from the access drive would be restricted to right-in/right-out turns only.

The traffic study concludes the site driveway would operate at LOS A in the AM peak and LOS C in the PM peak. Intersection sight distance and stopping sight distance would meet applicable standards. The traffic study concludes no site access improvements, (i.e. inbound right-turn pocket or outbound acceleration lane) on Issaquah-Fall City Road are needed. The site access shall maintain the City's adopted level of service (LOS) standard "D." The City will evaluate if an alternate driveway location is necessary or if channelization improvements on Issaquah-Fall City Road are needed to maintain the LOS and safe access operations.

4. Bicycle and Pedestrian Facilities – The *Nexus Study for Bicycle and Pedestrian Facilities Mitigation Fees* (Henderson Young & Company, December 10, 2014) was adopted by the City Council, Ordinance #2733, effective February 2, 2015. The study quantifies the direct impact of new development on the current system of bicycle and pedestrian facilities and the additional demands from future growth to maintain the adopted level of service. The report uses trip generation rates based on the different land use types to quantify the impacts of new development. It also identifies 16 specific bicycle and pedestrian projects that are needed to support the City's level of service standard. Payment of mitigation fees as determined in the study may satisfy a development's requirement to mitigate their project impacts on the level of service standard. If the developer doesn't voluntarily use the methodology and mitigation fees as determined in the report, the developer may choose other methods to quantify and mitigate their impact including conducting a study of its impacts and identifying alternate means of mitigating impacts to achieve the adopted standards. The mitigation fee for assisted living facilities is presently \$120.72/bed. The mitigation fee will be assessed with issuance of building permits and the actual cost of the mitigation fee will be the adopted fee in effect at the time of permit issuance. Applicant objections to the voluntary payment should be made during the SEPA comment period.
5. Public Services - The proposal would have a potential impact on public services, including police and general government buildings. IMC Chapter 3.74, Methods to Mitigate Development Impacts, provides alternatives to mitigate for direct impacts of proposed development. The City may approve a voluntary payment in lieu of other mitigation. Rate studies for police facilities and general government buildings are included in IMC 18.10.260 as the City's SEPA policy base. The rate studies present the methodology and formulas for determining the amount of the mitigation fee commensurate with the proposed land use and project impacts. The current mitigation fee is \$.04932/SF for general government and \$.13562/SF for the police mitigation fee. The mitigation fee will be assessed with issuance of building permits and the actual cost of the mitigation fee will be the adopted fee in effect at the time of permit issuance. Applicant objections to the voluntary payment should be made during the SEPA comment period.

**Mitigation Measures:** The Mitigated Determination of Nonsignificance is based on the checklist received March 23, 2015 and supplemental information in the application. The following SEPA mitigation measures shall be deemed conditions of the approval of the licensing decision pursuant to Chapter 18.10 of the Issaquah Land Use Code. All conditions are based on policies adopted by reference in the Land Use Code.

1. The applicant shall enhance the wetland buffer at a 1:1 ratio, an area equal to the 3,026 SF buffer reduction area. Final wetland buffer enhancement plans are required for approval by the Issaquah Development Services Department (DSD) prior to issuing construction permits. Final plans shall include a planting plan and a 5-year monitoring/maintenance plan with performance standards for monitoring success of the enhancement planting. The plans shall meet standards of the King County Critical Areas Mitigation Guidelines for the planting density and monitoring performance standards.
2. A permit from the Washington State Department of Archeology and Historic Preservation (DAHP) shall be obtained under RCW 27.53 for archaeological monitoring of construction, prior to issuance of construction permits. A professional archaeologist shall be onsite during ground disturbance. The applicant shall consult with affected Tribes on appropriate content and media for historic public education and/or art element to be developed and installed on the project site.
3. The site access shall maintain the City's adopted level of service (LOS) standard "D." The City will evaluate if an alternate driveway location is necessary or if channelization improvements on Issaquah-Fall City Road are needed to maintain the LOS and safe access operations.
4. The applicant shall mitigate for potential impacts on public services and bicycle and pedestrian facilities. The City may approve a voluntary payment in lieu of other mitigation. The current mitigation fee is \$.04932/SF for general government, \$0.13562/SF for the police mitigation fee, and \$120.72/bed for the bicycle/pedestrian facilities mitigation fee. The mitigation fees will be assessed with issuance of building permits and the actual fee amount will be the adopted fee in effect at the time of permit issuance. Applicant objections to the voluntary payment should be made during the SEPA comment period. The applicant should pay the voluntary contribution prior to issuance of building permits.

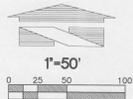
**Responsible Official:** Peter Rosen  
**Position/Title:** Environmental Planner  
**Address/Phone:** P.O. Box 1307 Issaquah, WA 98027-1307 (425) 837-3094  
**Date:** 8/20/2015 **Signature:**  \_\_\_\_\_

cc: Washington State Department of Ecology  
Muckleshoot Indian Tribe  
U.S. Army Corps of Engineers  
Washington State Department of Fish and Wildlife  
Washington State Department of Archeology and Historic Preservation (DAHP)  
Issaquah Development Services Department  
Issaquah Public Works Engineering and Parks and Recreation Departments

**SURVEY INFORMATION**

**LEGAL DESCRIPTION: (MACLEAN PARCEL)**  
 THAT PORTION OF THE NORTH HALF OF THE SOUTHWEST QUARTER OF THE NORTHEAST QUARTER OF SECTION 22, TOWNSHIP 24 NORTH, RANGE 6 EAST, WILLAMETTE MERIDIAN, IN KING COUNTY, WASHINGTON, LYING WESTERLY OF THE WESTERLY LINE OF ISSAQUAH-FALL CITY ROAD AS CONVEYED UNDER RECORDING NUMBER 900941411, SAID CENTERLINE BEGINNING AT THE SOUTHWEST CORNER OF SECTION 22, TOWNSHIP 24 NORTH, RANGE 6 EAST, WILLAMETTE MERIDIAN, RECORDS OF KING COUNTY.

EXCEPT ANY PORTION THEREOF LYING WESTERLY OF THE EASTERLY MARGIN OF A KING COUNTY ROAD BEING A STRIP OF LAND 100 FEET IN WIDTH WITH THE CENTER BEING THE CENTERLINE OF ISSAQUAH-FALL CITY ROAD AS ESTABLISHED BY KING COUNTY SURVEY NO. 22-2406113 AND BY (DEED) RECORDING NUMBER 900941411, SAID CENTERLINE BEGINNING AT THE SOUTHWEST CORNER OF SECTION 22, TOWNSHIP 24 NORTH, RANGE 6 EAST, WILLAMETTE MERIDIAN, RECORDS OF KING COUNTY.



THENCE SOUTH 88°24'57" EAST, A DISTANCE OF 478.30 FEET ALONG THE SOUTH LINE OF SAID SECTION 22;  
 THENCE NORTH 54°34'34" EAST, A DISTANCE OF 2,389.54 FEET TO THE POINT OF BEGINNING AND THE BEGINNING OF 720.00 FOOT RADIUS TANGENT CURVE TO THE LEFT;  
 THENCE ALONG THE ARC OF SAID 720.00 FOOT RADIUS CURVE THROUGH A CENTRAL ANGLE OF 38°43'08" AN ARC DISTANCE OF 486.56 FEET;  
 THENCE NORTH 15°21'28" EAST, A DISTANCE OF 869.58 FEET TO THE BEGINNING OF A 1,536.00 FOOT RADIUS TANGENT CURVE TO THE RIGHT;  
 THENCE ALONG THE ARC OF SAID 1,536.00 FOOT RADIUS CURVE THROUGH A CENTRAL ANGLE OF 33°43'18" AN ARC DISTANCE OF 904.02 FEET;  
 THENCE NORTH 49°34'44" EAST, A DISTANCE OF 401.62 FEET TO THE INTERSECTION OF ISSAQUAH-PINE LAKE ROAD AND ISSAQUAH-FALL CITY ROAD AND THE TERMINUS OF THIS CENTERLINE DESCRIPTION;  
 AND THAT PORTION LYING SOUTHEASTERLY OF THE ARC OF A CIRCLE HAVING A RADIUS OF 25 FEET BEING TANGENT TO A LINE 50 FEET NORTHERLY AND PARALLEL TO SAID CENTERLINE OF ISSAQUAH-FALL CITY ROAD AND TANGENT TO A LINE 42 FEET EASTERLY OF AND PARALLEL TO THE CENTERLINE OF ISSAQUAH-PINE LAKE ROAD;  
 AND THAT PORTION LYING NORTHEASTERLY OF THE ARC OF A CIRCLE HAVING A RADIUS OF 25 FEET BEING TANGENT TO A LINE 50 FEET SOUTHERLY AND PARALLEL TO SAID CENTERLINE OF ISSAQUAH-FALL CITY ROAD AND TANGENT TO A LINE 42 FEET EASTERLY OF AND PARALLEL TO THE CENTERLINE OF ISSAQUAH-PINE LAKE ROAD.

**(PINSHEL PARCEL)**  
 THAT PORTION OF THE SOUTH HALF OF THE SOUTHWEST QUARTER OF THE NORTHEAST QUARTER OF SECTION 22, TOWNSHIP 24 NORTH, RANGE 6 EAST, WILLAMETTE MERIDIAN, IN KING COUNTY, WASHINGTON, LYING WESTERLY OF THE WESTERLY LINE OF ISSAQUAH-FALL CITY ROAD AS CONVEYED TO KING COUNTY BY INSTRUMENTS RECORDED UNDER RECORDING NUMBERS 2721716 AND 820920097;  
 EXCEPT THE FOLLOWING DESCRIBED TRACT;  
 COMMENCING AT THE SOUTHWEST CORNER OF SAID SUBDIVISION;  
 THENCE SOUTH 88°19'16" EAST ALONG THE SOUTH LINE OF SAID SUBDIVISION 132.85 FEET TO THE TRUE POINT OF BEGINNING;  
 THENCE NORTH 01°36'43" EAST PARALLEL TO THE WEST LINE OF SAID SUBDIVISION 96.42 FEET;  
 THENCE SOUTH 88°30'31" EAST 159.33 FEET TO THE WESTERLY MARGIN OF THE ISSAQUAH-FALL CITY ROAD;  
 THENCE SOUTHWESTERLY ALONG SAID MARGIN TO THE SOUTH LINE OF SAID SUBDIVISION;  
 THENCE NORTH 88°19'16" WEST ALONG SAID LINE 83.00 FEET TO THE TRUE POINT OF BEGINNING; AND EXCEPT ANY PORTION THEREOF LYING WESTERLY OF THE EASTERLY MARGIN OF A ROAD, AS CONVEYED TO KING COUNTY BY DEED RECORDED UNDER RECORDING NUMBERS 900941411, BEING A STRIP OF LAND 100 FEET IN WIDTH, 50 FEET WIDE ON EACH SIDE OF THE FOLLOWING DESCRIBED CENTERLINE;  
 COMMENCING AT THE SOUTHWEST CORNER OF SECTION 22, TOWNSHIP 24 NORTH, RANGE 6 EAST, WILLAMETTE MERIDIAN, IN KING COUNTY, WASHINGTON;  
 THENCE SOUTH 88°24'57" EAST, A DISTANCE OF 478.30 FEET ALONG THE SOUTH LINE OF SAID SECTION 22;  
 THENCE NORTH 54°34'34" EAST, A DISTANCE OF 2,389.54 FEET TO THE POINT OF BEGINNING AND THE BEGINNING OF 720.00 FOOT RADIUS TANGENT CURVE TO THE LEFT;  
 THENCE ALONG THE ARC OF SAID 720.00 FOOT RADIUS CURVE THROUGH A CENTRAL ANGLE OF 38°43'08" AN ARC DISTANCE OF 486.56 FEET;  
 THENCE NORTH 15°21'28" EAST, A DISTANCE OF 869.58 FEET TO THE BEGINNING OF A 1,536.00 FOOT RADIUS TANGENT CURVE TO THE RIGHT;  
 THENCE ALONG THE ARC OF SAID 1,536.00 FOOT RADIUS CURVE THROUGH A CENTRAL ANGLE OF 33°43'18" AN ARC DISTANCE OF 904.02 FEET;  
 THENCE NORTH 49°34'44" EAST, A DISTANCE OF 401.62 FEET TO THE INTERSECTION OF ISSAQUAH-PINE LAKE ROAD AND ISSAQUAH-FALL CITY ROAD AND THE TERMINUS OF THIS CENTERLINE DESCRIPTION.

**BASES OF BEARINGS: (NAD 83/91)**  
 HORIZONTAL DATUM FOR THIS SURVEY IS NAD 83/91 PER KING COUNTY, KING COUNTY CONTROL POINT DESIGNATION NO. 2286 WAS HELD FOR POSITION AND A LINE BETWEEN SAID POINT DESIGNATION NO. 2286 AND POINT DESIGNATION NO. 2278 WAS HELD FOR ROTATION, BEING 54318.47°.

**VERTICAL DATUM: VERTICAL DATUM FOR THIS SURVEY IS NAVD83, KING COUNTY CONTROL POINT DESIGNATION NO. 2286 WAS HELD FOR ELEVATION, BEING 3915.1 (TWIGB)**

**DATES OF SURVEY:**  
 FIELD SURVEY BY BARGHAUSEN CONSULTING ENGINEERS, INC. CONDUCTED IN DECEMBER, 2013.  
 ALL MONUMENTS SHOWN AS FOUND WERE VISITED AT THAT TIME.

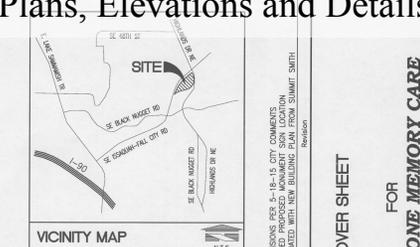
**DEVIATION REQUEST:**

WITH THE PROPOSED DEVELOPMENT OF THE SITE, THE DEVELOPER IS REQUESTING A DEVIATION FROM THE REQUIRED CITY FRONTAGE IMPROVEMENTS ALONG ISSAQUAH-FALL CITY ROAD BETWEEN THE SOUTH LIMITS OF FRONTAGE AND THE NORTH END OF THE EXISTING CONCRETE RETAINING WALL (UNDEVELOPED PORTION OF THE SITE).  
 IT IS OUR UNDERSTANDING THAT THE ROAD WIDTHS AND TRAVEL LINES MEET THE CURRENT CITY STANDARDS AND THAT THE ONLY IMPROVEMENT THAT WOULD BE REQUIRED IS TO REMOVE THE EXISTING SIDEWALK AND INSTALL A NEW SIDEWALK SEPARATED FROM THE TRAVEL LANE BY A PLANTER STRIP.  
 IT IS OUR OPINION THAT THE EXISTING SIDEWALK MEETS THE CRITERIA REQUIRED BY THE CITY TO GRANT APPROVAL.  
 1. THE SIDEWALK IN ITS CURRENT EXISTING LOCATION ACHIEVES THE INTENDED RESULT.  
 2. THE EXISTING SIDEWALK ADDRESSES PUBLIC SAFETY AND OPERATION BY PROVIDING A SAFE PEDESTRIAN ROUTE SEPARATED BY A VERTICAL CURB.  
 3. THE DEVIATION REQUEST WILL NOT ADVERSELY IMPACT HOW WELL THE NEARBY PUBLIC FACILITIES CAN BE MAINTAINED.

**COVER SHEET**  
 FOR  
**FIELDSTONE MEMORY CARE**  
 SECTION 22, TOWNSHIP 24 N., RANGE 06 E., W.M.  
 KING COUNTY, WASHINGTON

**SHEET INDEX**

- 1.0 COVER SHEET
- 2.0 PRELIMINARY SITE AND CIRCULATION PLAN
- 3.0 PRELIMINARY GRADING PLAN
- 4.0 PRELIMINARY ROADWAY AND STORM DRAINAGE PLAN
- W1 PRELIMINARY WATER PLAN
- W1 PRELIMINARY SEWER PLAN
- 1 OF 1 BOUNDARY AND TOPOGRAPHIC SURVEY
- L1 PRELIMINARY LANDSCAPE PLAN AND LEGEND
- L2 PRELIMINARY LANDSCAPE NOTES AND DETAILS
- TR1 TREE RETENTION PLAN
- TR2 TREE RETENTION PLAN



**OWNER:**  
 MACLEAN FAMILY LLC AND PASCHAL FAMILY TRUST  
 4923 ISSAQUAH-PINE LAKE ROAD SE  
 ISSAQUAH, WA 98029  
 (425) 302-3300

**DEVELOPER:**  
 CASCADE DEVELOPMENT, LLC  
 4120 ENGLEBROOK AVENUE  
 TACOMA, WA 98008  
 (206) 460-5642  
 CONTACT: JUSTIN YOUNKER

**ENGINEER/PLANNER/SURVEYOR:**  
 BARGHAUSEN CONSULTING ENGINEERS, INC.  
 18215 72ND AVE. SOUTH  
 KENT, WA 98032  
 (425) 251-6222  
 FAX (425) 251-6762  
 CONTACT: DON DAWES/WANA HALVORSEN/BRIAN GILLODY

**ARCHITECT:**  
 SUMMIT SMITH HEALTHCARE FACILITIES  
 6737 WEST WASHINGTON STREET, SUITE 3245  
 MILWAUKEE, WI 53214  
 (414) 453-1110  
 CONTACT: ANDY PALEC

**UTILITIES/SERVICES:**

**WATER:** SAMMAMISH PLATEAU WATER AND SEWER DISTRICT  
 1515-228TH AVENUE S.E.  
 SAMMAMISH, WA 98075  
 (425) 392-6256

**SEWER:** SAMMAMISH PLATEAU WATER AND SEWER DISTRICT  
 1515-228TH AVENUE S.E.  
 SAMMAMISH, WA 98075  
 (425) 392-6256

**POWER:** PUGET SOUND ENERGY - CUSTOMER CONSTRUCTION SERVICES  
 3130 S. 38TH STREET  
 TACOMA, WA 98409  
 (888) 321-7779

**GAS:** PUGET SOUND ENERGY - CUSTOMER CONSTRUCTION SERVICES  
 3130 S. 38TH STREET  
 TACOMA, WA 98409  
 (888) 321-7779

**TELEPHONE:** JENNIFFER JOHNSON CENTURY LINK  
 1550 NEWPORT WAY N.W.  
 ISSAQUAH, WA 98027  
 (206) 346-6553  
 Jennifer.gorman@centurylink.com

**CABLE:** JIM BROOKE, ENGINEERING CONSTRUCTION COORDINATOR  
 410 VALLEY AVE. N.W., SUITE 12 - BLDG. C  
 RYDALBURG, WA 98371  
 OFFICE: (253) 298-7535 CELL: (253) 261-2895  
 jim\_brooke@cable.comcast.com

**FIRE:** EASTSIDE FIRE AND RESCUE  
 178 NEWPORT WAY N.W.  
 ISSAQUAH, WA 98027  
 (425) 313-5000

**EROSION CONTROL SUPERVISOR:**  
 DON DAWES  
 BARGHAUSEN CONSULTING ENGINEERS, INC.  
 18215 72ND AVE. SOUTH  
 KENT, WA 98032  
 (425) 251-6222

**PROJECT INFORMATION:**

1. TAX PARCELS #s 2721716, 8209200697
2. ZONING: NORTH PARCEL = MF-M SOUTH PARCEL = SFSL
3. TOTAL SITE AREA: 4282,717 S.F. (16,490 AC)
4. DEVELOPABLE SITE AREA: 1,618,848 S.F. (43,72 AC)
5. TOTAL NUMBER OF UNITS: 60
6. PROPOSED IMPERVIOUS AREA = 473,663 S.F.
7. PROPOSED PERVIOUS AREA = 4209,054 S.F.
8. REQUIRED MIN. SETBACKS: FRONT STREET: 15 FEET
9. PARKING REQUIRED = 45
10. PARKING PROVIDED = 46 (INCLUDES 2 ACCESSIBLE STALLS)
11. FIRE FLOW RATE: 130
12. REQUIRED PERMITS:  
 CITY OF ISSAQUAH RIGHT-OF-WAY USE PERMIT  
 CITY OF ISSAQUAH BUILDING PERMIT  
 CITY OF ISSAQUAH GRADING PERMIT

**LEGEND**

**EXISTING:**

- LIQUOR LAUNDRY (LLM)
- POWER POLE
- PUMPER VAULT
- PUMPER METER
- JUNCTION BOX (AS NOTED)
- TELEPHONE
- TELEPHONE MANHOLE
- TELEPHONE INLET
- CATCH BASIN (CB)
- STORM MANHOLE (SMH)
- SEWER MANHOLE (SMH)
- SEWER
- WATER VALVE (WV)
- WATER MANHOLE
- WATER METER
- BLOW-OFF / AIRC
- FIRE HYDRANT (FH) / CONNECTION (FC)
- SOIL
- SIGNAL POLE
- SURVEY MONUMENT (AS NOTED)
- OUR ANCHOR
- CONCRETE
- ASPHALT
- EDGE OF PAVEMENT
- CHAIN LINK FENCE
- WOOD FENCE
- BARRIERS WIRE FENCE
- HIGHWIRE FENCE
- WATER LINE
- OVERHEAD/UNDERGROUND TEL. LINE
- OVERHEAD/UNDERGROUND PWR. LINE
- STORM LINE
- SEWER LINE
- EX TREES TO BE REMOVED
- EX TREES TO BE REMAIN
- EX WETLAND MARKER
- DIRECTION OF VEHICLE TRAVEL

**PROPOSED:**

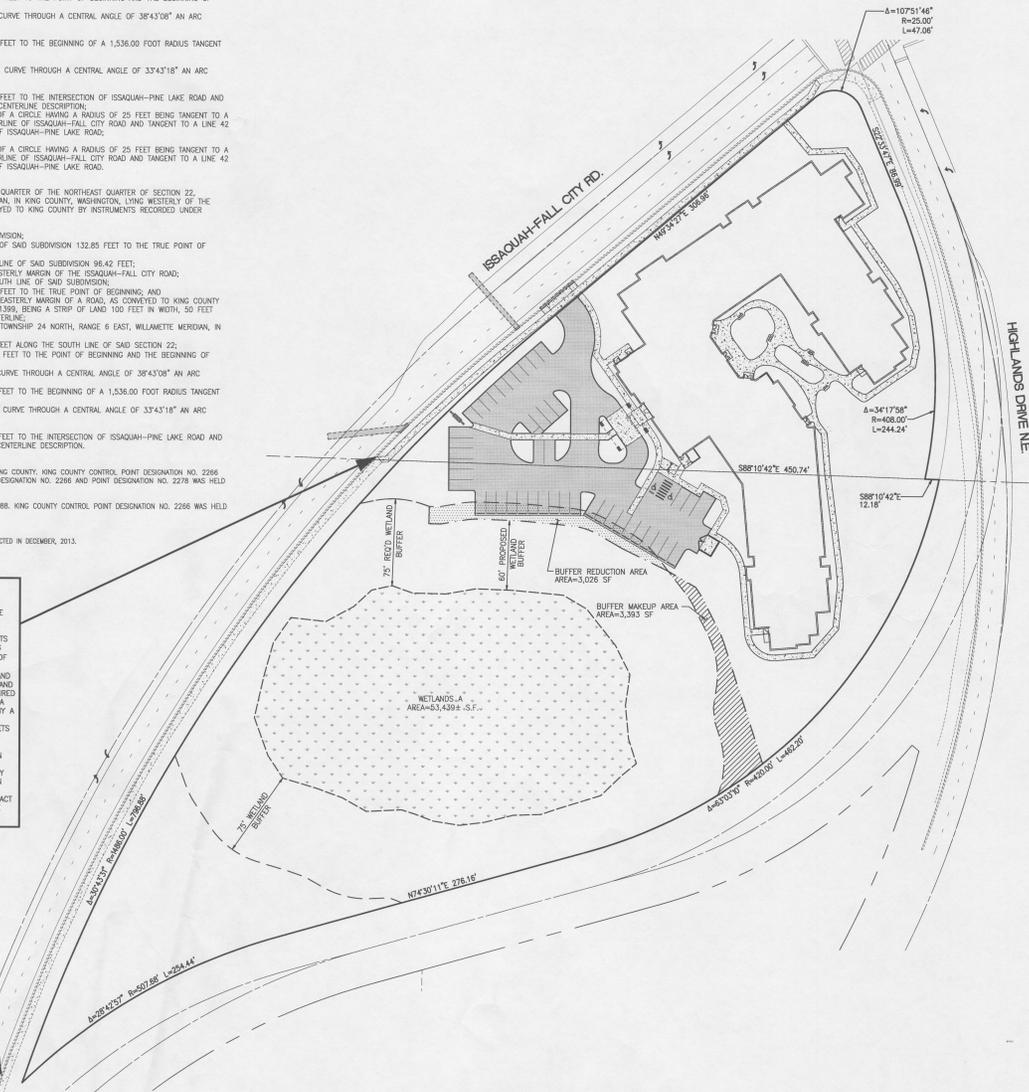
- PROPOSED ASPHALT
- PROPOSED CONCRETE
- PROPOSED CONTOURS
- PROPOSED TYPE 1 CATCH BASIN
- PROPOSED TYPE 2 CATCH BASIN
- PROPOSED STORM LINE
- PROPOSED 6" STORM LINE WITH C.O.
- PROPOSED YARD DRAIN
- PROPOSED SWALE
- PROPOSED WATER
- PROPOSED SEWER MANHOLE
- PROPOSED 6" SEWER LINE WITH C.O.

**Scale:**  
 Horizontal 1"=50'  
 Vertical N/A

**Designated CEE:**  
 Drawn: CEE  
 Checked: CEE  
 Approved: J.D.  
 Date: 3/13/19

**Job Number:** 16151  
**Sheet:** 10 of 4

**RECEIVED JUN 26 2015 CITY OF ISSAQUAH**



**COVER SHEET**  
 FOR  
**FIELDSTONE MEMORY CARE**  
 SECTION 22, TOWNSHIP 24 N., RANGE 06 E., W.M.  
 KING COUNTY, WASHINGTON  
**SDR15-0001**  
 For: **SUMMIT SMITH HEALTHCARE FACILITIES**  
 6737 WEST WASHINGTON STREET, SUITE 3245  
 MILWAUKEE, WI 53214  
 CONTACT: ANDY PALEC  
 Date: 6/25/15  
 Scale: 1" = 1'











**SURVEY INFORMATION**

**LEGAL DESCRIPTION (UNCLAS PARCEL)**

THAT PORTION OF THE NORTH HALF OF THE SOUTHWEST QUARTER OF THE NORTHEAST QUARTER OF SECTION 22, TOWNSHIP 24 NORTH, RANGE 6 EAST, WILLAMETTE MERIDIAN, IN KING COUNTY, WASHINGTON, LYING WESTERLY OF THE WESTERLY LINE OF ISSAQUAH-FALL CITY ROAD AS CONVEYED TO KING COUNTY BY INSTRUMENTS RECORDED UNDER RECORDING NUMBERS 2721716, 8009230742, AND 8410180377;

EXCEPT ANY PORTION THEREOF LYING WESTERLY OF THE EASTERLY MARGIN OF A KING COUNTY ROAD BEING A STRIP OF LAND 100 FEET IN WIDTH WITH THE CENTER BEING THE CENTERLINE OF ISSAQUAH-FALL CITY ROAD AS ESTABLISHED BY A KING COUNTY SURVEY NO. 22-24-N-13 AND BEING RECORDED UNDER RECORDING NUMBER 980941411, SAID CENTERLINE BEGINNING AT THE SOUTHWEST CORNER OF SECTION 22, TOWNSHIP 24 NORTH, RANGE 6 EAST, WILLAMETTE MERIDIAN, RECORDS OF KING COUNTY;

THENCE SOUTH 82°45'57" EAST A DISTANCE OF 476.30 FEET ALONG THE SOUTH LINE OF SAID SECTION 22;

THENCE NORTH 54°34'34" EAST A DISTANCE OF 2,389.54 FEET TO THE POINT OF BEGINNING AND THE BEGINNING OF 720.00 FOOT RADIUS TANGENT CURVE TO THE LEFT;

THENCE ALONG THE ARC OF SAID 720.00 FOOT RADIUS CURVE THROUGH A CENTRAL ANGLE OF 38°43'08" AN ARC DISTANCE OF 486.56 FEET;

THENCE NORTH 19°51'28" EAST, A DISTANCE OF 869.68 FEET TO THE BEGINNING OF A 1,536.00 FOOT RADIUS TANGENT CURVE TO THE RIGHT;

THENCE ALONG THE ARC OF SAID 1,536.00 FOOT RADIUS CURVE THROUGH A CENTRAL ANGLE OF 37°42'18" AN ARC DISTANCE OF 904.02 FEET;

THENCE NORTH 49°34'44" EAST, A DISTANCE OF 401.62 FEET TO THE INTERSECTION OF ISSAQUAH-PINE LAKE ROAD AND ISSAQUAH-FALL CITY ROAD AND THE TERMINUS OF THIS CENTERLINE DESCRIPTION;

THAT PORTION LYING SOUTHEASTERLY OF THE ARC OF A CIRCLE HAVING A RADIUS OF 25 FEET BEING TANGENT TO A LINE 50 FEET NORTHERLY AND PARALLEL TO SAID CENTERLINE OF ISSAQUAH-FALL CITY ROAD AND TANGENT TO A LINE 42 FEET EASTERLY OF AND PARALLEL TO THE CENTERLINE OF ISSAQUAH-PINE LAKE ROAD;

AND THAT PORTION LYING NORTHEASTERLY OF THE ARC OF A CIRCLE HAVING A RADIUS OF 25 FEET BEING TANGENT TO A LINE 50 FEET SOUTHERLY AND PARALLEL TO SAID CENTERLINE OF ISSAQUAH-FALL CITY ROAD AND TANGENT TO A LINE 42 FEET EASTERLY OF AND PARALLEL TO THE CENTERLINE OF ISSAQUAH-PINE LAKE ROAD.

**(PARCEL PARCELS)**

THAT PORTION OF THE SOUTH HALF OF THE SOUTHWEST QUARTER OF THE NORTHEAST QUARTER OF SECTION 22, TOWNSHIP 24 NORTH, RANGE 6 EAST, WILLAMETTE MERIDIAN, IN KING COUNTY, WASHINGTON, LYING WESTERLY OF THE WESTERLY LINE OF ISSAQUAH-FALL CITY ROAD AS CONVEYED TO KING COUNTY BY INSTRUMENTS RECORDED UNDER RECORDING NUMBERS 2721716 AND 8009230742;

EXCEPT THE FOLLOWING DESCRIBED TRACT;

COMMENCING AT THE SOUTHWEST CORNER OF SAID SUBDIVISION;

THENCE SOUTH 89°19'18" EAST ALONG THE SOUTH LINE OF SAID SUBDIVISION 132.85 FEET TO THE TRUE POINT OF BEGINNING;

THENCE NORTH 01°36'43" EAST PARALLEL TO THE WEST LINE OF SAID SUBDIVISION 96.42 FEET;

THENCE NORTH 82°53'11" EAST 150.33 FEET TO THE WESTERLY MARGIN OF THE ISSAQUAH-FALL CITY ROAD;

THENCE SOUTHERLY ALONG SAID MARGIN TO THE SOUTH LINE OF SAID SUBDIVISION;

THENCE NORTH 89°19'18" WEST ALONG SAID LINE 83.00 FEET TO THE TRUE POINT OF BEGINNING AND;

EXCEPT ANY PORTION THEREOF LYING WESTERLY OF THE EASTERLY MARGIN OF A ROAD, AS CONVEYED TO KING COUNTY BY BEING RECORDED UNDER RECORDING NUMBER 980201399, BEING A STRIP OF LAND 100 FEET IN WIDTH, 50 FEET WIDE ON EACH SIDE OF THE FOREGOING DESCRIBED CENTERLINE;

BEGINNING AT THE SOUTHWEST CORNER OF SECTION 22, TOWNSHIP 24 NORTH, RANGE 6 EAST, WILLAMETTE MERIDIAN, IN KING COUNTY, WASHINGTON;

THENCE SOUTH 82°45'57" EAST A DISTANCE OF 476.30 FEET ALONG THE SOUTH LINE OF SAID SECTION 22;

THENCE NORTH 54°34'34" EAST A DISTANCE OF 2,389.54 FEET TO THE POINT OF BEGINNING AND THE BEGINNING OF 720.00 FOOT RADIUS TANGENT CURVE TO THE LEFT;

THENCE ALONG THE ARC OF SAID 720.00 FOOT RADIUS CURVE THROUGH A CENTRAL ANGLE OF 38°43'08" AN ARC DISTANCE OF 486.56 FEET;

THENCE NORTH 19°51'28" EAST, A DISTANCE OF 869.68 FEET TO THE BEGINNING OF A 1,536.00 FOOT RADIUS TANGENT CURVE TO THE RIGHT;

THENCE ALONG THE ARC OF SAID 1,536.00 FOOT RADIUS CURVE THROUGH A CENTRAL ANGLE OF 37°42'18" AN ARC DISTANCE OF 904.02 FEET;

THENCE NORTH 49°34'44" EAST, A DISTANCE OF 401.62 FEET TO THE INTERSECTION OF ISSAQUAH-PINE LAKE ROAD AND ISSAQUAH-FALL CITY ROAD AND THE TERMINUS OF THIS CENTERLINE DESCRIPTION.

**BASIS OF BEARING: (NAD 83/11)**

HORIZONTAL DATUM FOR THIS SURVEY IS NAD 83/11 PER KING COUNTY, KING COUNTY CONTROL POINT DESIGNATION NO. 2266 WAS HELD FOR POSITION AND A LINE BETWEEN SAID POINT DESIGNATION NO. 2266 AND POINT DESIGNATION NO. 2278 WAS HELD FOR ROTATION, BEING 347.81 METERS.

VERTICAL DATUM: HORIZONTAL DATUM FOR THIS SURVEY IS NAVD83, KING COUNTY CONTROL POINT DESIGNATION NO. 2266 WAS HELD FOR ELEVATION, BEING 397.51 (NAVD83).

**DATES OF SURVEY:**

FIELD SURVEY BY BRUNSON CONSULTING ENGINEERS, INC. CONDUCTED IN DECEMBER, 2013. ALL MONUMENTS SHOWN AS FOUND WERE VISITED AT THAT TIME.

**TAX ACCOUNT NUMBERS:**

GROSS LAND AREA: CROSS LAND AREA=282,717.1 S.F. (6,490.2 ACRES)

**BUILDING AREA:**

NO BUILDINGS ON SITE

**REFERENCE SURVEYS:**

1. K.C.S.P. PLN05-00030, REC. NO. 2008011900004

**PROCEDURE / NARRATIVE:**

A FIELD TRAVELER TRIP A SYSTEM POSITIONED FOCUS 30 AND SPECTRA PRECISION RANGER SUPPLEMENTED WITH FIELD NOTES WERE PERFORMED, ESTABLISHING THE ANGULAR AND DISTANCE RELATIONSHIPS BETWEEN THE MONUMENTS, PROPERTY LINES AND IMPROVEMENTS. THE RESULTING DATA WERE USED TO DETERMINE THE STANDARDS FOR LAND BOUNDARY SURVEYS AS SET FORTH IN WAC 332-133-030.

**SURVEYOR'S NOTES:**

1. ALL DISTANCES SHOWN HEREIN ARE IN SURVEY FEET.

**SPECIAL EXCEPTIONS (UNCLAS PARCEL)**

1. TELEPHONE EASEMENT AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 717687, (NOT PLOTTABLE) BLANKET IN NATURE.

2. POWER EASEMENT AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 4407251, (PLOTTED HEREON).

3. SLOPE EASEMENT AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 8009230743, (PLOTTED HEREON).

4. GRASSLAND EASEMENT AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 810427028, (PLOTTED HEREON).

5. GRASSLAND FACILITY EASEMENT AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 8410180378, (PLOTTED HEREON).

6. SEWER EASEMENT AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 9811231594, (PLOTTED HEREON).

7. TELEPHONE EASEMENT AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 199910700015, (NOT PLOTTABLE) PORTION IS OFF-SITE HOWEVER THE BLANKET PORTION AFFECTS THE UNDERLYING PROPERTY.

8. SEWER EASEMENT AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 200110000230 AND ASSIGNMENT RECORDED UNDER RECORDING NO. 2006091302645, (PLOTTED HEREON).

9. SEWER EASEMENT AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 200006091899, (PLOTTED HEREON).

10. SLOPE EASEMENT AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 200110000230 AND ASSIGNMENT RECORDED UNDER RECORDING NO. 2006091302645, (PLOTTED HEREON).

11. SLOPE EASEMENT AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 2002040302277, (PLOTTED HEREON) (NOT PLOTTABLE).

12. SEWER AGREEMENT AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 9702818071, (NOT PLOTTABLE).

13. NOTICE OF CASH CREDIT FOR FUTURE PAYMENTS OF WATER AND SEWER GENERAL FACILITY CHARGES, INCLUDING THE TERMS AND PROVISIONS THEREOF, RECORDED UNDER RECORDING NO. 2004032000196, (NOT PLOTTABLE).

14. TELEPHONE AGREEMENT AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 20010504000419, (NOT PLOTTABLE) BLANKET PORTIONS AFFECT UNDERLYING PARCELS.

15. RIGHT TO MAKE NECESSARY SLOPES FOR CUTS OR FILLS UPON PROPERTY, RECORDED UNDER RECORDING NO. 8009230742, (NOT PLOTTABLE) BLANKET IN NATURE.

16. RIGHT TO MAKE NECESSARY SLOPES DESCRIBED AS GRANITE IN DEED, RECORDED UNDER RECORDING NO. 8410180377, (NOT PLOTTABLE) BLANKET IN NATURE.

17. TERMS AND CONDITIONS OF NOTICE OF CHANGES BY WATER, SEWER, AND/OR STORM AND SURFACE WATER UTILITIES, RECORDED UNDER RECORDING NUMBER 930701817, 200401020006, 2004120100040 AND 2008012800770, (NOT PLOTTABLE).

**BOUNDARY & TOPOGRAPHIC SURVEY**

**SURVEY INFORMATION CONT.**

**SPECIAL EXCEPTIONS (UNCLAS PARCEL)**

1. TELEPHONE EASEMENT AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 4407251, (NOT PLOTTABLE) BLANKET IN NATURE.

2. POWER EASEMENT AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 8009230743, (PLOTTED HEREON).

3. SLOPE EASEMENT AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 8001070329, (PLOTTED HEREON).

4. SEWER EASEMENT AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 9809230743, (PLOTTED HEREON).

5. SEWER EASEMENT AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 98124276, (PLOTTED HEREON).

6. RECREATIONAL EASEMENT AND MAINTENANCE AGREEMENT, AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 199912100082, (NOT PLOTTABLE) BLANKET FOR PROPERTY NORTH OF REALIGNS ISSAQUAH - FALL CITY ROAD.

7. EASEMENT AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 200006091899, (PLOTTED HEREON).

8. EASEMENT AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 20011024002074 AND ASSIGNMENT RECORDED UNDER RECORDING NO. 2006091302645, (NOT PLOTTABLE).

9. EASEMENT AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 971106039, (NOT PLOTTABLE).

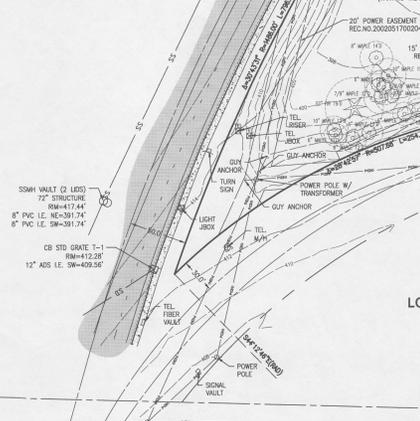
10. AGREEMENT AND THE TERMS AND CONDITIONS THEREOF, RECORDED UNDER RECORDING NO. 20011024002074 AND ASSIGNMENT RECORDED UNDER RECORDING NO. 2006091302645, (NOT PLOTTABLE).

11. RIGHT TO MAKE NECESSARY SLOPES FOR CUTS OR FILLS UPON PROPERTY HEREIN DESCRIBED AS GRANITE IN DEED, RECORDED UNDER RECORDING NO. 8009200691, (NOT PLOTTABLE).

12. TERMS AND CONDITIONS OF NOTICE OF CHANGES BY WATER, SEWER, AND/OR STORM AND SURFACE WATER UTILITIES, RECORDED UNDER RECORDING NUMBERS 930701817, 200110000230, 200401020006, 2004120100040 AND 2008012800770, (NOT PLOTTABLE).



**LEGEND**

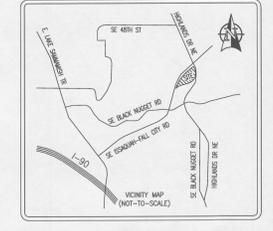


LOT A

LOT B  
K.C.S.P. PLN05-00030, REC. NO. 2008011900004

LOT C

**SURVEYOR'S CERTIFICATE:**  
THIS IS TO CERTIFY THAT THIS MAP OR PLAN AND THE SURVEY UPON WHICH IT IS BASED WERE MADE BY ME OR UNDER MY SUPERVISION AND CORRECTLY REFLECTS THE CONDITIONS OF THE SITE AS OF THE DATE OF THE FIELD SURVEY.  
  
Brian D. Gillooly, P.L.S. DATE 12/31/13  
BRUNSON CONSULTING ENGINEERS, INC. 46313  
WASHINGTON REGISTRATION NO. 46313



Job Number: 16151  
Sheet: 1 of 1  
Date: 12/31/13  
Scale: 1"=50'  
For: CASCADIA DEVELOPMENT, LLC  
Designed by: [Signature]  
Drawn by: [Signature]  
Checked by: [Signature]  
Date: 12/19/13  
18215 72ND AVENUE SOUTH  
KENT, WA 98032  
(425)251-8222  
(425)251-8795 FAX  
ONE ENGINEERING, LAND SURVEYING,  
SURVEYING, ENVIRONMENTAL SERVICES  
BACHHAUSEN | D | OHI SURVEYING  
| CONSULTING ENGINEERS  
Title: BOUNDARY AND TOPOGRAPHIC SURVEY  
PTN OF THE SW1/4 OF THE NE1/4 OF SEC. 22,  
TWP. 24 N., R6E 6 EAST, W. M.  
CITY OF ISSAQUAH KING COUNTY STATE OF WASHINGTON

# PRELIMINARY LANDSCAPE PLANTING PLAN AND LEGEND

FOR

## FIELDSTONE MEMORY CARE

SECTION 22, TOWNSHIP 24 N., RANGE 06 E., W.M.  
KING COUNTY, WASHINGTON

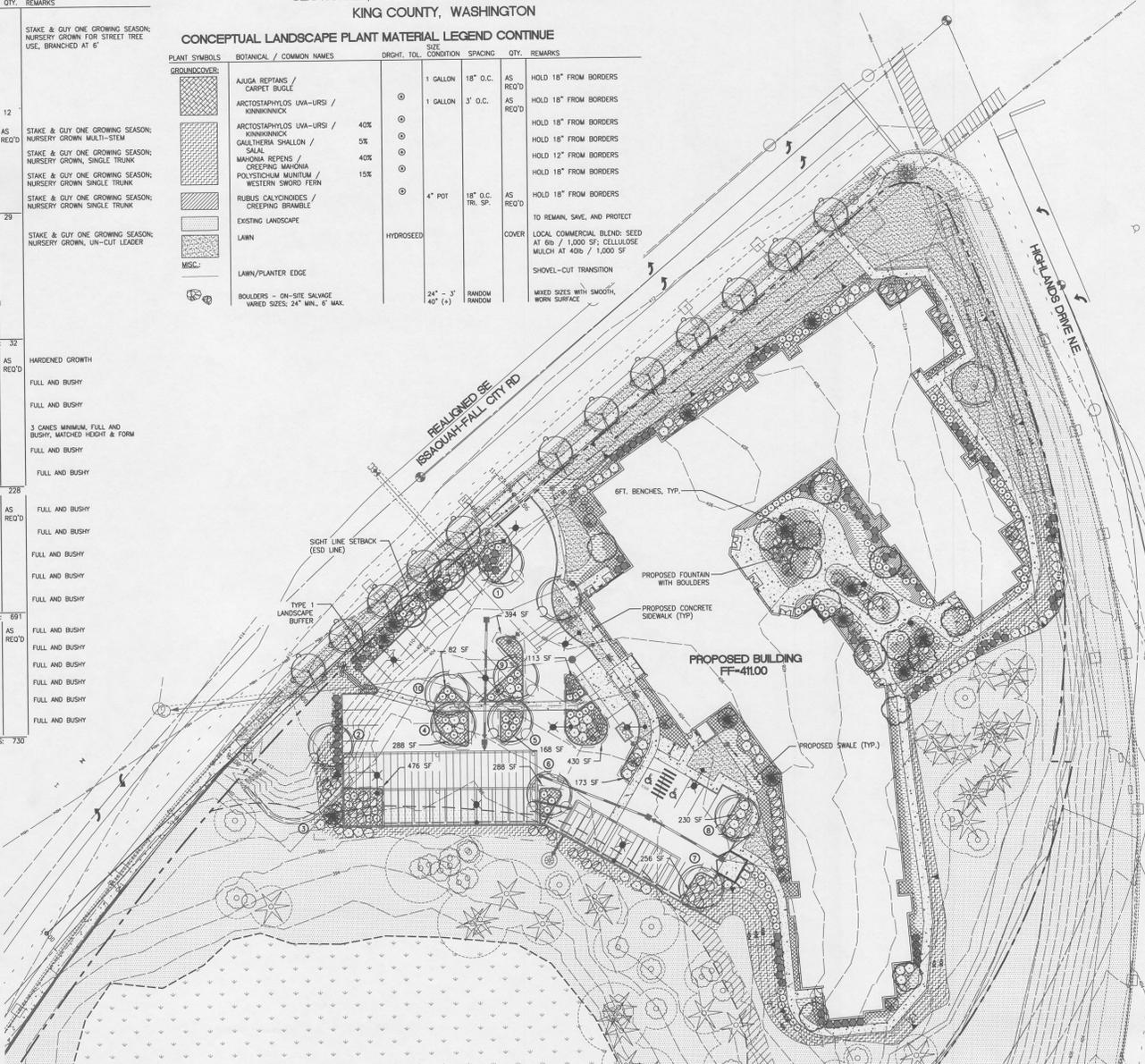
### CONCEPTUAL LANDSCAPE PLANT MATERIAL LEGEND

NOTE: PLANT SYMBOLS REPRESENT ANY ONE PLANT LOCATED WITHIN THE PLANT CATEGORY IT IS LISTED ADJACENT TOO.

PLANT SYMBOLS	BOTANICAL / COMMON NAMES	NATIVE OR DRGHT. TOL.	SIZE CONDITION	SPACING	QTY.	REMARKS
<b>STREET TREES:</b>	CRATAEGUS PHAENOCORYMIS / WASHINGTON HAWTHORN		2" CAL. B & B	AS SHOWN		STAKE & CLY ONE GROWING SEASON; NURSERY GROWN FOR STREET TREE USE, BRANCHED AT 6'
	FAGUS SYLVATICA 'FASTIGIATA' / FASTIGIATE BEECH					
	QUERCUS TRICANTHOS 'SHADEMASTER' / SHADEMASTER HONEYLOCUST					
	NYSSA SYLVATICA / TUPELO					
	<b>TOTAL STREET TREES:</b>				12	
<b>DECIDUOUS TREES:</b>	AMELANCHIER X GRANDIFLORA 'AUTUMN BRILLANCE' / AUTUMN BRILLANCE DOGWOOD		1.5" CAL. B & B	AS SHOWN AS REQ'D		STAKE & CLY ONE GROWING SEASON; NURSERY GROWN MULTI-STEM
	CERCIDIPHYLLUM JAPONICUM 'ROTFUCHS' / RED FOX KAISERA TREE		1.5" CAL. B & B			STAKE & CLY ONE GROWING SEASON; NURSERY GROWN, SINGLE TRUNK
	LIQUIDAMBAR STYRACIFLUA 'ROTUNDIFOLIA' / ROTUNDIFOLIA SWEETGUM		2" CAL. B & B			STAKE & CLY ONE GROWING SEASON; NURSERY GROWN SINGLE TRUNK
	STEWARTIA PSEUDOCAMELLIA / JAPANESE SNOWBELL		1.5" CAL. B & B			STAKE & CLY ONE GROWING SEASON; NURSERY GROWN SINGLE TRUNK
	<b>TOTAL DECIDUOUS TREES:</b>				29	
<b>CONIFEROUS TREES:</b>	PICEA PUNGENS 'HOOPSII' / HOOPSII BLUE SPRUCE		6"-8" HT. MINIMUM	AS SHOWN		STAKE & CLY ONE GROWING SEASON; NURSERY GROWN, UN-CUT LEADER
	PNUS NIGRA 'ARNDOLD'S SENTINEL' / ARNDOLD'S SENTINEL PINE		6"-8" HT. MINIMUM	AS SHOWN		
	PSEUDOTSUGA MENZIESII / DOUGLAS FIR		6"-8" HT. MINIMUM	AS SHOWN		
	TSUGA HETEROPHYLLA / WESTERN HEMLOCK		6"-8" HT. MINIMUM	AS SHOWN		
	XANTHOXYPIHIS NODIKATENSIS 'PENDULA' / NODIKA CYPRESS		6"-8" HT. MINIMUM	AS SHOWN		
	<b>TOTAL CONIFEROUS TREES:</b>				32	
<b>TALL SHRUBS:</b>	ARBUZUS UNEDO 'OKTOBERFEST' / OKTOBERFEST STRAWBERRY TREE		24" - 30" 5 GAL. MIN.	AS REQ'D		HARDENED GROWTH
	CISTUS HYBRIDUS / WHITE ROCKROSE		21" - 24" 3 GAL. MIN.			FULL AND BUSHY
	ESCALONIA 'NEWPORT DWARF' / NEWPORT DWARF ESCALONIA		21" - 24" 3 GAL. MIN.			FULL AND BUSHY
	MADONIA AQUIFOLIUM / OREGON OLEA		24" - 30" 5 GAL. MIN.			3 CANES MINIMUM, FULL AND BUSHY, MATCHED HEIGHT & FORM
	MYRICA CALIFORNICA / OREGON MYRTLE		24" - 30" 5 GAL. MIN.			FULL AND BUSHY
	SYMPHORICARPOS ALBUS / COMMON SNOWBERRY		15" - 18" 2 GAL. MIN.			FULL AND BUSHY
	<b>TOTAL TALL SHRUBS:</b>				228	
<b>LOW SHRUBS:</b>	LAVANDULA ANGUSTIFOLIA 'MUNSTEAD' / MUNSTEAD LAVENDER		1 GALLON 2" O.C.	AS REQ'D		FULL AND BUSHY
	PENNETSETUM ALOPECUROIDES 'HAMELI' / HAMELI FOUNTAIN GRASS		1 GALLON 2" O.C.			FULL AND BUSHY
	PNUS MUGO 'SLOWMOUND' / SLOWMOUND MUDDO PINE		15" - 18" 2 GAL. MIN.			FULL AND BUSHY
	SPIRAEA HIPPOCISTIS 'SNOWMOUND' / SNOWMOUND SPIRAEA		15" - 18" 2 GAL. MIN.			FULL AND BUSHY
	VACCINIUM OVALE / EVERGREEN HUCKLEBERRY		18" - 21" 2 GAL. MIN.			FULL AND BUSHY
	<b>APPROXIMATE TOTAL LOW SHRUBS:</b>				691	
<b>PERENNIALS:</b>	ARCTOTIS HYBRIDS / AFRICAN DAISY		1 GALLON 24" - 36" O.C.	AS REQ'D		FULL AND BUSHY
	CAMPANULA SPP. / BELLFLOWER					FULL AND BUSHY
	HEMEROCALLIS HYBRIDS / YELLOW DAYLILY					FULL AND BUSHY
	HOSTA HYBRIDS / PLANTAIN LILY					FULL AND BUSHY
	KNIPHOFIA LVARIA / RED-HOT POKER					FULL AND BUSHY
	PENNETSETUM ALOPECUROIDES / FOUNTAIN GRASS					FULL AND BUSHY
	<b>APPROXIMATE TOTAL PERENNIALS:</b>				730	

### CONCEPTUAL LANDSCAPE PLANT MATERIAL LEGEND CONTINUE

PLANT SYMBOLS	BOTANICAL / COMMON NAMES	DRGHT. TOL.	SIZE CONDITION	SPACING	QTY.	REMARKS
<b>GROUNDCOVER:</b>	AJUGA REPERTANS / CARPET BUGLE		1 GALLON	18" O.C.	AS REQ'D	HOLD 18" FROM BORDERS
	ARCTOSTAPHYLOS UVA-URSI / KINNICKINICK		1 GALLON	3' O.C.	AS REQ'D	HOLD 18" FROM BORDERS
	ARCTOSTAPHYLOS UVA-URSI / KINNICKINICK		40%			HOLD 18" FROM BORDERS
	CALTHEDIA SHALLOM / SALAL		5%			HOLD 18" FROM BORDERS
	MANISHA REPERTANS / CREEPING MANISHA		40%			HOLD 12" FROM BORDERS
	POLYSTICHUM MUNITZIM / WESTERN SWIRDO FERN		15%			HOLD 18" FROM BORDERS
	RUBUS CALYCOIDES / CREEPING BRAMBLE		4" POT	18" O.C. TR. SP.	AS REQ'D	HOLD 18" FROM BORDERS
	EXISTING LANDSCAPE					TO REMAIN, SAVE, AND PROTECT
	LAWN	HYDROSEED				COVER LOCAL COMMERCIAL BLEND; SEED AT 60% / 1,000 SF; CELLULOSE MULCH AT 40% / 1,000 SF
	LAWN/PLANTER EDGE	MGC:				SHOVEL-CUT TRANSITION
	BOULDERS - ON-SITE SALVAGE		24" - 3' 40" (+)	RANDOM		MIXED SIZES WITH SMOOTH, WORK SURFACE

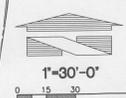


### LANDSCAPE PARKING LOT PLANTING CALCULATIONS

PARKING LOT REQUIREMENT	QUANTITY
TOTAL REQUIRED PARKING LOT TREES: (1) TREE FOR EVERY 6 PARKING STALLS	7.83 OR 8 TREES
47 PARKING STALLS = 47/6 = 7.83	10 TREES
TOTAL PROVIDED PARKING LOT TREES =	
TOTAL REQUIRED PARKING LOT SQUARE FOOTAGE: 25 SF PER STALL	1,175 SF TOTAL
PARKING STALLS = 47 X 25 =	2,888 SF
PROVIDED PARKING LOT SQUARE FOOTAGE =	

### GENERAL LANDSCAPE PLANTING NOTES

- CONCEPTUAL LANDSCAPE PLANTING PLAN LEGEND LISTS/PROVIDES AN EXAMPLE OF VARIOUS VEGETATION LIKELY TO BE USED WITHIN FUTURE LANDSCAPE PLANTING CONSTRUCTION DOCUMENT SET. PLANT SYMBOLS REPRESENT ANY ONE LISTED PLANT WITHIN ADJACENT PLANT CATEGORY. ADDITIONAL VEGETATION SHALL BE ADDED TO THE LEGEND AND PLANTING DESIGN PER LANDSCAPE ARCHITECT'S DISCRETION.
- REFER TO SHEET L2 FOR ADDITIONAL PLANTING NOTES, DETAILS AND SOIL PREPARATION.
- IRRIGATION CONSTRUCTION DOCUMENTS SHALL BE CREATED DURING FINAL CONSTRUCTION DOCUMENT REVIEW, WHEN SITE DESIGN WILL BE FORMALLY ESTABLISHED.



**PRELIMINARY LANDSCAPE PLAN AND LEGEND FOR FIELDSTONE MEMORY CARE**  
 DATE: 06/25/15  
 DRAWN BY: [Signature]  
 CHECKED BY: [Signature]  
 APPROVED BY: [Signature]  
 DATE: 06/25/15  
 SCALE: 1"=30'-0"  
 SHEET: 16151  
 OF: 2  
 PROJECT: 18215 72ND AVENUE SOUTH, REG-22, KING COUNTY, WA 98148  
 PHONE: (206) 251-8222, FAX: (206) 251-8782  
 BARGHAUSEN CONSULTING ENGINEERS  
 STATE OF WASHINGTON REGISTERED LANDSCAPE ARCHITECT  
 NAME: [Signature]  
 LICENSE NO: 1014  
 EXPIRES: 09/02/16  
 CONTACT: ANDY PALEC



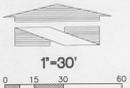
# PRELIMINARY TREE RETENTION PLAN

FOR

## FIELDSTONE MEMORY CARE

SECTION 22, TOWNSHIP 24 N., RANGE 06 E., W.M.  
KING COUNTY, WASHINGTON

FOUND MONUMENT IN CASE  
OF BRASS DISK W/INCH  
SET 0.45' BELOW GRADE  
FOR 5" ISSAQUAH FALLS CITY RD & HIGH DR.  
LABELED "2345"  
(DECEMBER 2015)



### SIGNIFICANT TREE RETENTION/ MITIGATION REQUIREMENT:

CANOPY COVERAGE CALCULATION	
TOTAL SITE AREA	282,717 S.F. (6.49 AC)
DEVELOPABLE SITE AREA	161,848 S.F. (3.72 AC)
TOTAL CALIPER (DBH) OF ALL SIGNIFICANT TREES IN DEVELOPABLE SITE AREA	2,786'
RETENTION REQUIRED: (25% OF THE TOTAL CALIPER (DBH) OF ALL SIGNIFICANT TREES IN DEVELOPABLE SITE AREA	2,786' x .25 = 697'
TOTAL CALIPER (DBH) OF ALL SIGNIFICANT TREES NOT REMOVED IN DEVELOPABLE SITE AREA	759'

### SIGNIFICANT TREE LEGEND

- ★ CONFIR TO REMAIN
- DECIDUOUS TO REMAIN
- TREE PROTECTION AREA (TPA TYP)
- ★ CONFIR TO BE REMOVED
- DECIDUOUS TO BE REMOVED

### SOURCE OF SIGNIFICANT TREES

FIELD SURVEYED BY:  
BARGHAUSEN CONSULTING ENGINEERS, INC. IN DECEMBER 2013

TREE RETENTION TABLE											
POINT #	SIZE (DBH)	TYPE	REMOVED	POINT #	SIZE (DBH)	TYPE	REMOVED	POINT #	SIZE (DBH)	TYPE	REMOVED
350012	34"	FIR	YES	350316	12"	FIR	YES	350013	6"	CHERRY	YES
350013	28"	FIR	YES	350317	8"	FIR	YES	350014	6"	CHERRY	YES
350015	22"	FIR	YES	350318	14"	FIR	YES	350016	20"	FIR	NO
350016	20"	FIR	YES	350319	28"	FIR	NO	350017	12"	FIR	NO
350020	8"	FIR	YES	350320	100"	MAPLE	NO	350018	12"	MAPLE	NO
350021	28"	FIR	YES	350321	12"	MAPLE	NO	350019	6"	CHERRY	NO
350022	28"	FIR	YES	350322	8"	ALDER	NO	350020	14"	MAPLE	YES
350028	15"	FIR	YES	350325	14"	MAPLE	YES	350021	8"	CHERRY	YES
350029	18"	FIR	YES	350326	8"	MAPLE	YES	350022	8"	CEDRAR	YES
350029	20"	FIR	YES	350327	6"	FIR	YES	350023	12"	FIR	YES
350030	12"	FIR	YES	350328	6"	CEDRAR	YES	350024	27"	FIR	YES
350031	20"	FIR	YES	350329	27"	FIR	YES	350025	18"	FIR	YES
350032	18"	FIR	YES	350331	8"	MAPLE	YES	350026	22"	FIR	YES
350033	22"	FIR	YES	350332	22"	FIR	YES	350027	24"	FIR	YES
350034	20"	FIR	YES	350333	14"	MAPLE	YES	350028	18"	FIR	YES
350035	22"	FIR	YES	350334	30"	FIR	YES	350029	24"	FIR	YES
350036	12"	FIR	YES	350335	12"	FIR	YES	350030	24"	FIR	YES
350037	24"	FIR	YES	350336	22"	FIR	YES	350031	12"	CHERRY	YES
350038	18"	FIR	YES	350337	22"	FIR	YES	350032	12"	CHERRY	YES
350039	24"	FIR	YES	350338	24"	FIR	YES	350033	12"	CHERRY	YES
350040	12"	CHERRY	YES	350339	8"	MAPLE	YES	350034	12"	MAPLE	YES
350041	12"	MAPLE	YES	350340	17"	FIR	YES	350035	15"	FIR	YES
350042	15"	FIR	YES	350341	15"	MAPLE	YES	350036	15"	FIR	YES
350043	8"	CHERRY	YES	350343	17"	FIR	YES	350037	15"	FIR	YES
350045	8"	MAPLE	YES	350344	16"	FIR	YES	350038	18"	FIR	YES
350046	20"	FIR	YES	350345	15"	FIR	YES	350039	18"	FIR	YES
350047	18"	FIR	YES	350346	12"	MAPLE	YES	350040	27"	FIR	YES
350048	18"	FIR	YES	350347	24"	MAPLE	YES	350041	27"	FIR	YES
350049	27"	FIR	YES	350348	8"	CHERRY	YES	350042	24"	FIR	YES
350051	24"	FIR	YES	350350	12"	MAPLE	YES	350043	8"	ALDER	YES
350052	8"	FIR	YES	350351	8"	ALDER	YES	350044	28"	FIR	YES
350053	24"	FIR	YES	350352	8"	ALDER	YES	350045	22"	FIR	YES
350054	28"	FIR	YES	350353	20"	FIR	YES	350046	22"	FIR	YES
350055	22"	FIR	YES	350354	20"	FIR	YES	350047	18"	FIR	YES
350056	18"	FIR	YES	350355	7"	MAPLE	YES	350048	18"	FIR	YES
350057	18"	FIR	YES	350356	7"	MAPLE	YES	350049	28"	FIR	YES
350058	18"	FIR	YES	350357	20"	FIR	YES	350050	28"	FIR	YES
350059	18"	FIR	YES	350358	20"	FIR	YES	350051	18"	FIR	YES
350060	18"	FIR	YES	350359	20"	FIR	YES	350052	18"	FIR	YES
350061	18"	FIR	YES	350360	18"	FIR	YES	350053	18"	FIR	YES
350062	18"	FIR	YES	350361	18"	FIR	YES	350054	18"	FIR	YES
350063	18"	FIR	YES	350362	18"	FIR	YES	350055	18"	FIR	YES
350064	18"	FIR	YES	350363	18"	FIR	YES	350056	18"	FIR	YES
350065	18"	FIR	YES	350364	18"	FIR	YES	350057	18"	FIR	YES
350066	18"	FIR	YES	350365	18"	FIR	YES	350058	18"	FIR	YES
350067	18"	FIR	YES	350366	18"	FIR	YES	350059	18"	FIR	YES
350068	18"	FIR	YES	350367	18"	FIR	YES	350060	18"	FIR	YES
350069	18"	FIR	YES	350368	18"	FIR	YES	350061	18"	FIR	YES
350070	18"	FIR	YES	350369	18"	FIR	YES	350062	18"	FIR	YES
350071	18"	FIR	YES	350370	18"	FIR	YES	350063	18"	FIR	YES
350072	18"	FIR	YES	350371	18"	FIR	YES	350064	18"	FIR	YES
350073	18"	FIR	YES	350372	18"	FIR	YES	350065	18"	FIR	YES
350074	18"	FIR	YES	350373	18"	FIR	YES	350066	18"	FIR	YES
350075	18"	FIR	YES	350374	20"	FIR	YES	350067	18"	FIR	YES
350076	18"	FIR	YES	350375	20"	FIR	YES	350068	18"	FIR	YES
350077	18"	FIR	YES	350376	20"	FIR	YES	350069	18"	FIR	YES
350078	18"	FIR	YES	350377	20"	FIR	YES	350070	18"	FIR	YES
350079	18"	FIR	YES	350378	20"	FIR	YES	350071	18"	FIR	YES
350080	18"	FIR	YES	350379	20"	FIR	YES	350072	18"	FIR	YES
350081	18"	FIR	YES	350380	18"	FIR	YES	350073	18"	FIR	YES
350082	18"	FIR	YES	350381	18"	FIR	YES	350074	18"	FIR	YES
350083	18"	FIR	YES	350382	18"	FIR	YES	350075	18"	FIR	YES
350084	18"	FIR	YES	350383	18"	FIR	YES	350076	18"	FIR	YES
350085	18"	FIR	YES	350384	10"	ALDER	NO	350077	18"	FIR	YES
350086	18"	FIR	YES	350385	10"	ALDER	NO	350078	18"	FIR	YES
350087	18"	FIR	YES	350386	10"	ALDER	NO	350079	18"	FIR	YES
350088	18"	FIR	YES	350387	10"	ALDER	NO	350080	18"	FIR	YES
350089	18"	FIR	YES	350388	10"	ALDER	NO	350081	18"	FIR	YES
350090	18"	FIR	YES	350389	10"	ALDER	NO	350082	18"	FIR	YES
350091	18"	FIR	YES	350390	10"	ALDER	NO	350083	18"	FIR	YES
350092	18"	FIR	YES	350391	10"	ALDER	NO	350084	18"	FIR	YES
350093	18"	FIR	YES	350392	10"	ALDER	NO	350085	18"	FIR	YES
350094	18"	FIR	YES	350393	10"	ALDER	NO	350086	18"	FIR	YES
350095	18"	FIR	YES	350394	10"	ALDER	NO	350087	18"	FIR	YES
350096	18"	FIR	YES	350395	10"	ALDER	NO	350088	18"	FIR	YES
350097	18"	FIR	YES	350396	10"	ALDER	NO	350089	18"	FIR	YES
350098	18"	FIR	YES	350397	10"	ALDER	NO	350090	18"	FIR	YES
350099	18"	FIR	YES	350398	10"	ALDER	NO	350091	18"	FIR	YES
350100	18"	FIR	YES	350399	10"	ALDER	NO	350092	18"	FIR	YES
350101	18"	FIR	YES	350400	10"	ALDER	NO	350093	18"	FIR	YES
350102	18"	FIR	YES	350401	10"	ALDER	NO	350094	18"	FIR	YES
350103	18"	FIR	YES	350402	10"	ALDER	NO	350095	18"	FIR	YES
350104	18"	FIR	YES	350403	10"	ALDER	NO	350096	18"	FIR	YES
350105	18"	FIR	YES	350404	10"	ALDER	NO	350097	18"	FIR	YES
350106	18"	FIR	YES	350405	10"	ALDER	NO	350098	18"	FIR	YES
350107	18"	FIR	YES	350406	10"	ALDER	NO	350099	18"	FIR	YES
350108	18"	FIR	YES	350407	10"	ALDER	NO	350100	18"	FIR	YES
350109	18"	FIR	YES	350408	10"	ALDER	NO	350101	18"	FIR	YES
350110	18"	FIR	YES	350409	10"	ALDER	NO	350102	18"	FIR	YES
350111	18"	FIR	YES	350410	10"	ALDER	NO	350103	18"	FIR	YES
350112	18"	FIR	YES	350411	10"	ALDER	NO	350104	18"	FIR	YES
350113	18"	FIR	YES	350412	10"	ALDER	NO	350105	18"	FIR	YES
350114	18"	FIR	YES	350413	10"	ALDER	NO	350106	18"	FIR	YES
350115	18"	FIR	YES	350414	10"	ALDER	NO	350107	18"	FIR	YES
350116	18"	FIR	YES	350415	10"	ALDER	NO	350108	18"	FIR	YES
350117	18"	FIR	YES	350416	10"	ALDER	NO	350109	18"	FIR	YES
350118	18"	FIR	YES	350417	10"	ALDER	NO	350110	18"	FIR	YES
350119	18"	FIR	YES	350418	10"	ALDER	NO	350111	18"	FIR	YES
350120	18"	FIR	YES	350419	10"	ALDER	NO	350112	18"	FIR	YES
350121	18"	FIR	YES	350420	10"	ALDER	NO	350113	18"	FIR	YES
350122	18"	FIR	YES	350421	10"	ALDER	NO	350114	18"	FIR	YES
350123	18"	FIR	YES	350422	10"	ALDER	NO	350115	18"	FIR	YES
350124	18"	FIR	YES	350423	10"	ALDER	NO	350116	18"	FIR	YES
350125	18"	FIR	YES	350424	10"	ALDER	NO	350117	18"	FIR	YES
350126	18"	FIR	YES	350425	10"	ALDER	NO	350118	18"	FIR	YES
350127	18"	FIR	YES	350426	10"	ALDER	NO	350119	18"	FIR	YES
350128	18"	FIR	YES	350427	10"	ALDER	NO	350120	18"	FIR	YES
350129	18"	FIR	YES	350428	10"	ALDER	NO	350121	18"	FIR	YES
350130	18"	FIR	YES	350429	10"	ALDER	NO	350122	18"	FIR	YES
350131	18"	FIR	YES	350430	10"	ALDER	NO	350123	18"	FIR	YES
350132	18"	FIR	YES	350431	10"	ALDER	NO	350124	18"	FIR	YES
350133	18"	FIR	YES	350432	10"	ALDER	NO	350125	18"	FIR	YES
350134	18"	FIR	YES	350433	10"	ALDER	NO	350126	18"	FIR	YES
350135	18"	FIR	YES	350434	10"	ALDER	NO	350127	18"	FIR	YES
350136	18"	FIR	YES	350435	10"	ALDER	NO	350128	18"	FIR	YES
350137	18"	FIR	YES	350436	10"	ALDER	NO	350129	18"	FIR	YES
350138	18"	FIR	YES	350437	10"	ALDER	NO	350130	18"	FIR	YES
350139											



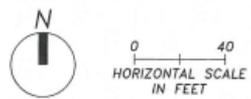




JUN 26 2015  
CITY OF ISSAQUAH

CALCULATION SUMMARY		TARGET	ACTUAL
CALCULATION AREA	DESIGN CRITERIA		
	AVG. MAINTAINED FOOT CANDLES		
PARKING LOT	2.50	15.00:1	13.40:1
	2.68		

\* LIGHT SPILLOVER SHALL BE LIMITED TO 0.3 FOOTCANDLES AT THE BEGINNING BOUNDARY OF CRITICAL AREA BUFFER.



DATE:  
06/19/2015

**TENW**  
 Transportation Engineering NorthWest  
 Transportation Planning | Design | Traffic Impact & Operations  
 11422 18 8th Street, Suite 200, Bellevue, WA 98004 | Office (425) 882-6747  
 Project Contact: Stan DeWitt  
 Phone: (425) 220-1232

FIELDSTONE MEMORY CARE  
ISSAQUAH, WA

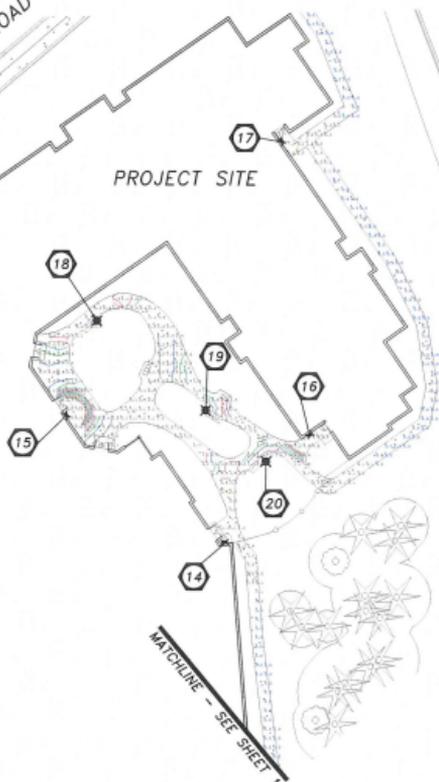
ILLUMINATION CALCULATIONS

SHEET	1
OF	2

ISSAQUAH - FALL CITY ROAD

PROJECT SITE

HIGHLANDS DRIVE NE



CALCULATION SUMMARY		TARGET / ACTUAL	
CALCULATION AREA	DESIGN CRITERIA		
	AVG. MAINTAINED FOOT CANDLES	UNIFORMITY RATIO (AVG. FC / MIN. FC)	
COURTYARD	1.25	15.00:1	13.80:1
	1.38		



0 40  
HORIZONTAL SCALE  
IN FEET

DATE:  
06/19/2015

**TENW**

Transportation Engineering Northwest

Transportation Planning | Design | Traffic Impact & Operations  
11420 16 8th Street, Suite 202, Bellevue, WA 98004 | Office (425) 889-6747  
Project Contact: Glenn Dullmead  
Phone: 425-254-0282

FIELDSTONE MEMORY CARE  
ISSAQUAH, WA

SHEET  
2

ILLUMINATION CALCULATIONS

OF  
2

# ISSAQUAH MEMORY CARE

ISSAQUAH, WASHINGTON

## OWNER

CASCADIA DEVELOPMENT LLC  
410 SINGLEWOOD AVENUE  
YAKIMA, WA 98903  
CONTACT: JUSTIN TOUNKER  
TEL: (509) 424-8000  
FAX: (509) 469-9510

## PREPARER

SUNNIT SMITH HEALTHCARE FACILITIES  
6737 WASHINGTON STREET  
SUITE 3145  
MILWAUKEE, WI 53214  
CONTACT: ANDY PALEC  
APALEC@SUNNITSMITH.COM  
TEL: (614) 453-0110  
FAX: (614) 453-0110

## PROJECT PROFILE

USE OF BUILDING: R-1 ASSISTED LIVING FACILITIES  
AS LICENSED BY WASHINGTON STATE  
UNDER CHAPTER 508-78  
WAC (IBC 308.4.1 AMENDMENTS)

RESIDENTS:	UNITS	QUANTITY	RESIDENTS
		S1	SINGLA UNITS
	B	COMPANION UNITS	10
	TOTAL	60	60

### BUILDING PROFILE:

- SINGLE STORY, NO BASEMENT, SBCA: RENTHOUS.
- CONSTRUCTION: V-A IIR PROTECTED, SPRINKLERED IBC
- GROSS AREA: 45,519 S.F.
- MAXIMUM HEIGHT: 34 FEET

## DRAWING INDEX

- C COVER SHEET
- 1 FLOOR PLAN
- 1 ROOF PLAN
- 3 ELEVATIONS
- 4 ELEVATIONS
- 5 VIEW

SDP15-00001

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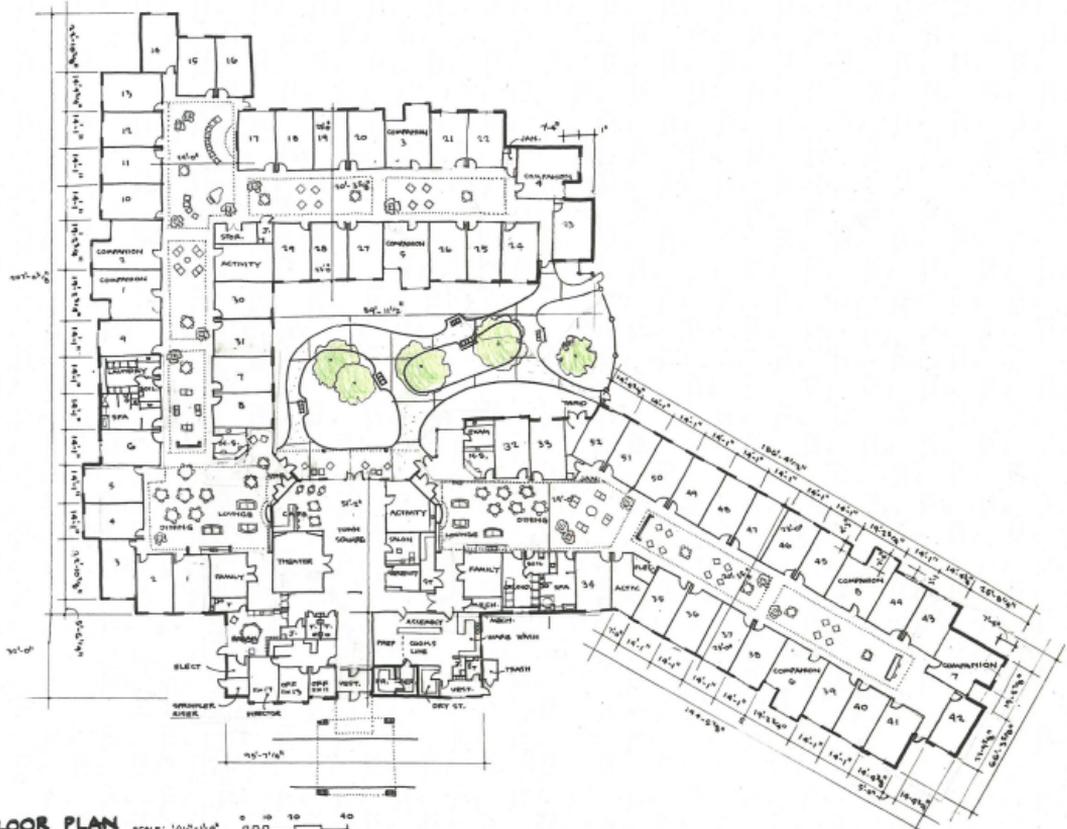
MAR 23 2015

City of Issaquah

COVER SHEET

30515

SHEET C



**FLOOR PLAN** SCALE: 1/4"=1'-0" 0 10 20 40

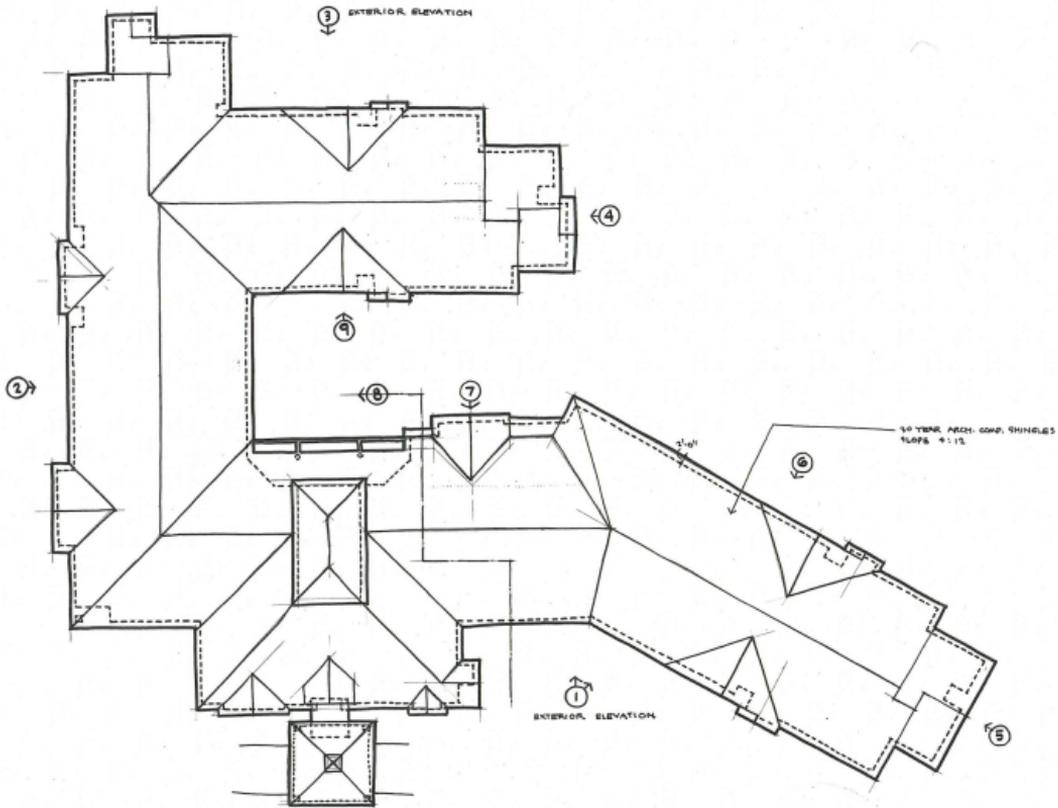


**ISSAQUAH MEMORY CARE**

**ISSAQUAH, WA.** OWNER: CASCADIA DEVELOPMENT, LLC YAKIMA, WA 030519

PREPARED BY: SUMMIT SMITH HEALTHCARE FACILITY (855-613-7) WASHINGTON STREET SUITE 3545, MUKAMBEK, WA 98146  
 CONTACT: ANDY PALEC. ap@smc @summit-smith.com TEL: 414-453-0110 FAX: 414-453-0112

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 City of Issaquah



ROOF PLAN

SCALE: 1/4" = 1'-0"



ISSAQUAH MEMORY CARE

ISSAQUAH, WA.

OWNER: CASCADIA DEVELOPMENT, LLC YAKIMA, WA 030515

PREPARED BY: SUNSHINE SMITH HEALTHCARE FACILITIES - 6137 WASHINGTON STREET, SUITE 3148, PALLMADER, WA 99114  
 CONTACT: ANDY PHELPS - ap@sc@sunshinewh.com TEL: 416-453-0110 FAX: 416-453-0117

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 MAR 23 2015  
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SHEET 2  
 OF 5



1 ELEVATION  
LEFT



1 ELEVATION  
RIGHT



2 ELEVATION  
LEFT



2 ELEVATION  
RIGHT



5 ELEVATION

EXTERIOR  
ELEVATIONS

SCALE: 1/8" = 1'-0"

0 5 10 20  
FEET

RECEIVED

ISSAQUAH MEMORY CARE

ISSAQUAH, WA.

OWNER: CASCADIA DEVELOPMENT, LLC YAKIMA, WA

050515 SHEET 3

OF 5

PREPARED BY: SWINNEY HEALTHCARE FACILITIES • 6733 MARSHINGTON STREET, SUITE 3340, MILWAUKEE, WI 53114  
CONTACT: ANDY PAUL • apaul@swinneyhealth.com • TEL: 414-451-0110 • FAX: 414-451-0112

MAR 23 2015

City of Issaquah



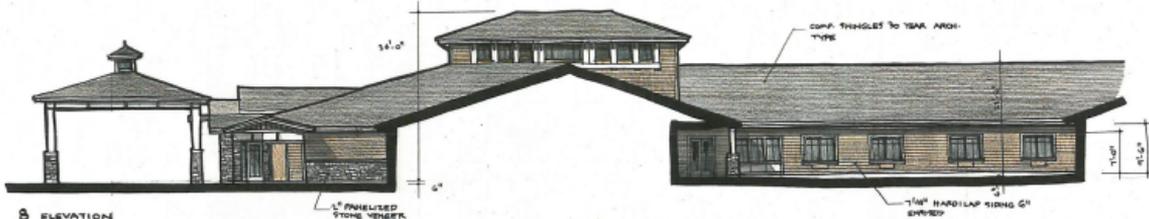
3 ELEVATION



4 ELEVATION



7 ELEVATION



8 ELEVATION



9 ELEVATION

ISSAQUAH MEMORY CARE

SCALE: 1/8" = 1'-0"  
 0 5 10 20

EXTERIOR  
 ELEVATIONS

ISSAQUAH, WA.

OWNER: CASCADIA DEVELOPMENT, LLC YAKIMA, WA 030515

PREPARED: SUMMIT SMITH HEALTHCARE FACILITIES • 6737 WASHINGTON STREET, SUITE 3145, MINNAPARK, WA 98114

CONTACT: ANDY PILEC: apilec@summit-smith.com • TEL: 414-451-0110 • FAX: 414-453-0173

SHEET 4  
 OF 9

RECEIVED

MAR 23 2015

City of Issaquah



ISSAQUAH MEMORY CARE

ISSAQUAH, WA. OWNER: CASCADIA DEVELOPMENT LLC YAKIMA, WA 989015

PREPARED BY: SUMMIT SOUTH HEATHCARE FACILITIES - 6731 WASHINGTON STREET, SUITE 5140, MEMPHIS, TN 38114  
CONTACT: ANDY PALCO - 60380@summitsouth.com - TEL: 414-455-0110 - FAX: 414-455-0113

VIEW FROM WEST



SHEET 5  
OF 9

RECEIVED

MAR 20 2015

City of Issaquah

**Design Criteria Checklist**

Instructions: This checklist is used to review a project proposal's consistency with the City's design criteria for Level 0, 1, 2, 3, 4, 5 and 6 Review. This checklist information, provided by the Planning Director/Manager (Level 0, 1 and 2) or the Development Commission (Level 3, 5 and 6) or the Hearing Examiner (Level 4) to the applicant is intended to be clear direction on the conditions necessary for design approval of the project's finalized application. It is understood that a Level 0, 1, 2 and 3 Review shall receive more detailed and specific direction provided by the Planning Director/Manager (Level 0, 1 and 2) or Development Commission (Level 3) regarding design criteria than a Level 5 or 6 Review due to the larger size and scope of a Level 5 Review.

1. **Conditions of Approval.** If the design criteria has not been addressed to the satisfaction of the Planning Director/Manager (Level 0, 1 and 2) or Development Commission (Level 3 and 5) or Hearing Examiner (Level 4), clear written direction shall be identified as to how the project can meet the specific design criteria in the column titled "Conditions of Approval."
2. **Acceptable.** If the design criteria has been addressed on the current site plan to the satisfaction of the Planning Director/Manager (Level 0, 1 and 2) or Development Commission (Level 3, 5 and 6) or Hearing Examiner (Level 4), the date of that site plan shall be listed in the column "Acceptable" so that there is clear direction to the applicant for the final application.
3. **N/A** = "Not Applicable to this project."

Additional Criteria: Adopted photographs and a color system (Ord. 1983) exemplify and illustrate the written design criteria within this checklist and shall be considered in interpreting and applying the written criteria. Copies of these photographs and color system shall be kept on file at the City Planning Department and are available for public inspection during regular business hours. The photographs and the color system are part of this checklist for interpreting and applying the written criteria.

SDP15-00001 Fieldstone Memory Care

APPENDIX 2, DESIGN STANDARDS	Conditions of Approval	Acceptable	N/A
<b>A. Site Layout and Overall Design Concepts</b>			
<p>1. <b>Building Location:</b> Building locations and their orientation to one another provide for pedestrian/people areas such as courtyards, plazas, pocket parks, etc.</p> <p><i>Staff comments: The building is oriented to the corner of the intersection and retains the existing wetland on site. Additionally, where possible, trees are being retained on the property adjacent to the trail on the east side of the property.</i></p>		3-23-15	
<p>2. <b>Energy Efficient Design:</b> The project is oriented to receive maximum winter sun benefit and uses architectural features and/or landscaping to screen summer sun.</p> <p><i>Staff comments: The courtyard is south facing and is ideally oriented to take advantage of the winter sun.</i></p>		3-23-15	

SDP15-00001 Fieldstone Memory Care

APPENDIX 2, DESIGN STANDARDS	Conditions of Approval	Acceptable	N/A
<p>3. <b>Functional Site Design:</b> Design and layout of the buildings, parking areas, pedestrian areas, landscape and open areas are conducive to the existing topography and existing features of the site. Parking areas are designed so that they function well with the overall site design; for instance, parking areas provide safe and efficient nonmotorized movement, and traffic flow is predictable within the designated parking areas and driveways.</p>	<ol style="list-style-type: none"> <li>1. Non-motorized movement within the parking area shall include the provision of a continuous walkway from on at least one side of parking lot aisles that do not contain angle parking per 18.07.080. Currently there are not enough walkways proposed to meet this requirement.</li> <li>2. Walkways shall be physically separated from vehicular areas by grade, landscaping strips, berms, barriers, curbs or similar means providing pedestrian visibility and consistent with ADA access.</li> <li>3. Additionally, curbs located within the emergency vehicle clear zone shall be rolled, instead of vertical.</li> <li>4. The loading space shall incorporate a maneuvering space of not less than fifty-two (52) feet in length shall be provided adjacent to the loading dock; this maneuvering space shall not include any area designated for off-street parking storage or trash dumpsters.</li> </ol>	<p>6-26-15</p>	

SDP15-00001 Fieldstone Memory Care

APPENDIX 2, DESIGN STANDARDS	Conditions of Approval	Acceptable	N/A	
4.	<b>Lighting:</b>			
	<p>a. Lighting standards and fixtures are of a design and size compatible with the general character of the building and adjacent areas, including other lighting standards/fixtures. Design compatibility includes the following lighting standard/fixture characteristics: architectural style, standard/fixture color, light color, decoration, material, placement, texture and shape.</p>	<ol style="list-style-type: none"> <li>1. Lighting standards and fixtures shall comply with Appendix 2 of the Issaquah Municipal Code, section 4.</li> <li>2. The placement of some lights within the emergency vehicle clear zone may need to be relocated or the height of the pole and fixture shall be 14 feet in height to maintain clearance from emergency vehicles.</li> </ol>		
	<p>b. Lighting complies with IMC <a href="#">18.07.107</a>, Outdoor lighting.</p>	<ol style="list-style-type: none"> <li>1. The lighting levels currently proposed do not fully meet the code requirements. The lighting within the parking area is limited to 5 foot candles, except at the beginning boundary of the critical area buffer is limited to 0.3 footcandles. The minimum lighting levels for common areas, including the courtyard and walkway is 0.3 footcandles. The lighting levels within the parking and common areas shall meet the code requirements.</li> </ol>		

SDP15-00001 Fieldstone Memory Care

APPENDIX 2, DESIGN STANDARDS	Conditions of Approval	Acceptable	N/A
	<p>2. The lighting levels proposed adjacent to the critical area buffer appear to meet the code requirements.</p> <p>3. All lighting will be reviewed for compliance with building permit submittal.</p>		
<p>5. <b>Natural Setting – Views:</b> The relationship of the natural setting of the valley and surrounding mountains is used to enhance the overall design and layout of the plan in the following ways:</p>			
<p>a. <b>Hillside Design:</b> Structures built on hillsides are designed so that they blend into the hillside to minimize their visible impact to surrounding areas. The ridgeline of the hillside is not broken by any structures, lighting standards/fixtures, or loss of vegetative cover. Methods to integrate the structure into the hillside include: height control; colors that are muted instead of brilliant or bright colors; maintenance of existing trees to the greatest extent possible; and/or other appropriate methods.</p>			

SDP15-00001 Fieldstone Memory Care

APPENDIX 2, DESIGN STANDARDS	Conditions of Approval	Acceptable	N/A
<p>b. <b>Primary Views:</b> Public views of Mount Rainier, Cougar, Squak and Tiger Mountains are not blocked; for example, the view of Mount Rainier from Rainier Blvd. and the railroad ROW pathway should remain unobstructed.</p> <p><i>Staff comments: Trees that are being retained on the ease edge of the property are mature and will block views. The building as proposed is one-story and will not impact views from adjacent properties. Due to the critical area on site, all vegetation and trees within the critical area will be protected and will not change the existing conditions.</i></p>		3-23-15	
<p>6. <b>Existing Vegetation/Topography Features:</b> Existing vegetation, topography and other features of the site are preserved and integrated into the overall site design. Suitable existing vegetation shall be preserved, and measures to assure its preservation shall be provided.</p>			

SDP15-00001 Fieldstone Memory Care

APPENDIX 2, DESIGN STANDARDS	Conditions of Approval	Acceptable	N/A
<p><b>Staff comments:</b> A preliminary tree preservation plan was part of the project information reviewed. Tree retention requirements apply only to the developable site area, or the area on site that does not include a critical area or its buffer. All vegetation in the critical area and buffer are required to be retained and protected. The proposal exceeds the tree retention requirements.</p>			
<p>7. <b>Historical/Cultural Landmarks:</b> Historical and cultural landmarks, and Issaquah Treasures (as adopted by Resolution 93-15) are preserved and integrated into the overall site design.</p>			
<p><b>B. Landscape Design and Use of Plant Materials</b></p>			
<p>1. <b>Design Elements:</b> Architectural screens, fountains, and pavings of wood, brick, stone, gravel and/or other similar methods and materials are used in conjunction or combination with plant materials (or in place of plant materials where planting opportunities are limited).</p>	<p>1. The trash enclosure is required landscape screening on the north side of the proposed enclosure per the code. The walkway proposed adjacent to the enclosure will need to be shifted three (3) to five (5) feet to allow for a Type 1 screen in between the</p>		

SDP15-00001 Fieldstone Memory Care

APPENDIX 2, DESIGN STANDARDS	Conditions of Approval	Acceptable	N/A
	<p>enclosure and the pedestrian pathway.</p> <p>2. The type of landscape screen required is a Type 1 Dense Year-Round Sight Barrier and the proposed white rock rose will not sufficiently provide a Type 1 screen. A type 1 screen shall be a combination of approximately 70% evergreen trees backed by a 100% sight-obscuring fence softened or accented with landscaping.</p> <p>3. Chain link is not compatible with the site design or building architecture, so another fence style shall be chosen that complements the character of this development. A new fence material shall be chosen for the trash enclosure fence and fence at the edge of the parking lot.</p> <p>4. Courtyard fence shall be compatible with the site design and building architecture as well.</p>		
<p>2. <b>Design Unity:</b> Unity of design is achieved through repetition of certain plant varieties and other</p>		<p>3-23-15</p>	

SDP15-00001 Fieldstone Memory Care

APPENDIX 2, DESIGN STANDARDS	Conditions of Approval	Acceptable	N/A
<p>materials and by correlation with adjacent developments.</p> <p><b>Staff comments:</b> <i>The applicant is proposing a mix of native trees and vegetation that blend in with the retained tree stands. Additionally, the proposed flowering deciduous trees will provide seasonal color.</i></p>			
<p>3. <b>Enhanced Design:</b></p>			
<p>a. The landscape design of the site strengthens vistas and important focal points, provides for both solar exposure and shading where desirable, and retains significant existing vegetation.</p> <p><b>Staff comments:</b> <i>The landscape elements are primarily used to inject greenery and shade within the surface parking lots and courtyard and provide texture to the façades the building.</i></p>		3-23-15	
<p>b. Trees and shrubs are planted in parkways or</p>		3-23-15	

SDP15-00001 Fieldstone Memory Care

APPENDIX 2, DESIGN STANDARDS	Conditions of Approval	Acceptable	N/A
<p>paved areas where building sites limit plantings.</p>			
<p>c. Parking areas and traffic ways are enhanced with landscaped areas that contain trees and tree groupings (see also “Nonmotorized and Vehicular Areas – Design – Parking Areas”).</p>		3-23-15	
<p>4. <b>Usable Open Space Design:</b> The usable open space includes significant areas which have aesthetic value and/or value for recreational purposes and is easily accessible to the users of the development and to the general public (in cases where the open space has been dedicated), unless this guideline conflicts with the purpose and intent of the critical areas regulations.</p> <p><i><b>Staff comments:</b> An open air courtyard is proposed to be located in the center of the building and will be accessible to all residents and employees in the facility. The courtyard facilities have controlled access, secured by a fence.</i></p>		3-23-15	

SDP15-00001 Fieldstone Memory Care

APPENDIX 2, DESIGN STANDARDS	Conditions of Approval	Acceptable	N/A
5. <b>Plant Materials – Selection:</b>			
<p>a. <b>Appearance/Maintenance:</b> Plant materials are selected for their structure, texture, and color as well as their ultimate growth and ease of maintenance.</p> <p><i>Staff comments: The plant materials proposed have a compatible structure with site and building design with .</i></p>		3-23-15	
<p>b. <b>Noxious or Destructive:</b> Plant materials used for landscaping purposes are not destructive to sewer or water systems, sidewalks, building foundations or any other structure or utility. Noxious weeds and other plant materials including purple loosestrife and invasive species of ivy are not utilized in landscape planting plans.</p> <p><i>Staff comments: A detailed analysis of the landscape design and plant materials for</i></p>	<p>Plant materials and planting locations must be shown in conjunction with locations of utility lines as part of the Landscape Plans to be submitted with Construction drawings.</p> <p>Landscape Plans must show how proposed plant materials and planting locations comply with Sec.5b of Appendix 2, Design Criteria, of the Issaquah Municipal Code.</p>		

SDP15-00001 Fieldstone Memory Care

APPENDIX 2, DESIGN STANDARDS		Conditions of Approval	Acceptable	N/A
	<i>compliance with this requirement cannot be completed at the Master Site Plan review phase because the construction drawings for the utility lines have not been developed.</i>			
	<p>b. <b>Safety:</b> Alder trees, cottonwood trees or other trees that typically grow very quickly, have weak trunks and branches and are prone to falling are not proposed for planting in parking areas, next to buildings or other structures or in any pedestrian-oriented area. Tree selection and placement should not diminish required outdoor lighting illumination of the intended pedestrian areas and parking lots. Tree selection and placement may be used to screen lighting from adjacent properties or downgrade viewing.</p>	<p>Trees located near the entrance to the development shall be evaluated based on sight distance and may be required to be removed from areas impacting vehicle sightlines. Additionally, tree locations will be evaluated based lighting levels to ensure the location of the tree does not diminish the required outdoor lighting illumination of the intended pedestrian areas and parking lot.</p>		
<b>C. Design Harmony and Compatibility</b>				
1.	<b>Accessory Structures:</b> Street furniture, mailboxes, kiosks, lighting standards/fixtures, and accessory	Chain link is not compatible with the site design or building architecture, so another fence style		

SDP15-00001 Fieldstone Memory Care

APPENDIX 2, DESIGN STANDARDS	Conditions of Approval	Acceptable	N/A
<p>structures located on private property, public ways and other public properties are designed as part of the architectural concept of the building and landscape design.</p>	<p>shall be chosen that complements the character of this development. Also refer to B. Landscape Design and Use of Plant Materials, 1. Design Elements above in this checklist.</p>		
<p>2. <b>Building Materials/Components:</b></p>			
<p><b>Scale:</b> Building components, such as windows, doors, eaves, parapets, and signage have the same proportions, scale and relationship to one another. Building materials shall incorporate fire protection and emergency services access.</p> <p><b>Staff comments:</b> <i>Building components, including windows, doors, eaves and building modulation are in proportion to one another and incorporate fire protection and emergency service access.</i></p>		<p>3-23-15</p>	
<p><b>b. Durability/Maintenance:</b> Materials and finishes are selected for their durability and wear. Proper measures and devices are incorporated for protection against the elements, neglect, damage,</p>		<p>3-23-15</p>	

SDP15-00001 Fieldstone Memory Care

APPENDIX 2, DESIGN STANDARDS	Conditions of Approval	Acceptable	N/A
<p>and abuse. Configurations that tend to catch and accumulate debris, leaves, trash, and dirt should not be used.</p> <p><b>Staff comments:</b> <i>At this stage of concept design, no detailed information is available to determine whether there are areas or parts of the building that will accumulate debris, leaves, trash or dirt. However, the applicant has their own operational requirements that will ensure this.</i></p>			
<p>3. <b>Compatibility:</b> The proposed development is designed and oriented to be compatible with existing permitted land uses adjacent to the site and with the surroundings, both manmade and natural. Elements influencing compatibility include but are not limited to color, signage and lighting, size, scale, mass, and architectural style and design.</p> <p><b>Staff comments:</b> <i>The proposed building materials include natural colors and stone that reflect</i></p>		3-23-15	

SDP15-00001 Fieldstone Memory Care

APPENDIX 2, DESIGN STANDARDS	Conditions of Approval	Acceptable	N/A
<p><i>craftsman style commonly found throughout the region and will blend in with the retained trees on the east side of the property. The sprawling footprint of the building is meant to minimize its impact to the wetland and retained trees. The proposed single-story building is under the maximum height allowed. Lighting is compatible with building design.</i></p> <p><i>Adjacent to the site are a combination of one-story single family homes and two-story attached townhomes. The proposed building design is compatible with the surrounding development.</i></p>			
<p>4. <b>Design Components:</b></p>			
<p>a. Colors: Bright and/or brilliant colors are used only minimally for accent.</p> <p><b>Staff comments:</b> <i>A light forest green with off white accents, two neutral shades, are the predominant colors used for the exterior of the</i></p>		3-23-15	

SDP15-00001 Fieldstone Memory Care

APPENDIX 2, DESIGN STANDARDS	Conditions of Approval	Acceptable	N/A
<p><i>Stone veneer with various hues of brown and red color is used to add interest to the façade, is applied on full walls and half walls.</i></p>			
<p><b>b. Modulation:</b> Modulation has been incorporated in the overall design to reduce the bulk and mass of the building(s).</p> <p><b>Staff comments:</b> <i>The building is well-modulated, as shown in the perspective drawings. Modulation techniques used include recessing walls and using different materials to break up the facades into multiple horizontal planes. Windows are present at regular intervals as shown on the elevations.</i></p>		3-23-15	
<p><b>c. Facade:</b> Articulate the different parts of a building’s facade by use of color, arrangement of facade elements, or a change in materials.</p> <p><b>Staff comments:</b> <i>Most of the building façade is articulated using windows and 3 different textures and materials: stone, shingle siding, batten board,</i></p>		3-23-15	

SDP15-00001 Fieldstone Memory Care

APPENDIX 2, DESIGN STANDARDS	Conditions of Approval	Acceptable	N/A
<p><i>metal and fiberboard on every façade.</i></p>			
<p>d. <b>Ground Level:</b> Avoid blank walls at the ground level. Utilize windows, trellises, wall articulation, arcades, changes in materials, or other features. <b>Staff comments:</b> <i>Windows, wall articulation, differing materials and landscaping are used to avoid blank walls at the ground level.</i></p>		3-23-15	
<p>e. <b>Large Structures:</b> Large dominating structures should be broken up by creating horizontal emphasis through use of trim, adding windows or other ornamentation, use of colors, and landscape materials. <b>Staff comments:</b> <i>See staff comments for “Modulation” above also.</i></p>		3-23-15	
<p>f. <b>Corporate Style:</b> The use of standard “corporate” architectural style associated with chain-type business is strongly discouraged.</p>			
<p>5. <b>Signage:</b></p>	Signs shall be permitted separately.		

SDP15-00001 Fieldstone Memory Care

APPENDIX 2, DESIGN STANDARDS	Conditions of Approval	Acceptable	N/A
<p>a. <b>Architectural Element:</b> Every sign is designed as an integral architectural element of the building and site to which it principally relates; lighting of signage is compatible with the architectural character of building; and is compatible with signs on adjoining premises.</p>			
<p>b. <b>Graphic Elements:</b> Graphic elements are held to the minimum needed to convey the sign’s major message and are composed in proportion to the area of the sign face.</p>			
<p>c. <b>Materials:</b> The colors, materials, and lighting are held to the minimum needed to convey the sign’s major message and are composed in proportion to the area of the sign face.</p>			
<p>d. <b>Scale/Proportion:</b> Every sign is of compatible scale and proportion in design and visual relationship to buildings and surroundings.</p>			
<p>6. <b>Transition:</b></p>			

SDP15-00001 Fieldstone Memory Care

APPENDIX 2, DESIGN STANDARDS	Conditions of Approval	Acceptable	N/A
<p>a. The proposed development transitions well with adjoining, permitted land uses through architecture and landscaping in conformance with allowable setbacks.</p> <p><b>Staff comments:</b> See previous staff comments on "Compatibility."</p>		3-23-15	
<p>b. <b>Conflicting Architectural Styles:</b> In applicable cases, structures are made compatible with adjacent buildings of conflicting architectural styles by such means as screens and site breaks, or other suitable methods and materials.</p> <p><b>Staff comments:</b> <i>The proposed building style is compatible with adjacent buildings. See also previous staff comments on "Compatibility"</i></p>		3-23-15	
<p>7. <b>Projects with Multiple Structures:</b> Variable siting of individual buildings, heights of buildings, building modulation or other methods are used in order to prevent monotonous design.</p>			
<p><b>D. Nonmotorized and Vehicular Areas</b></p>			

SDP15-00001 Fieldstone Memory Care

APPENDIX 2, DESIGN STANDARDS		Conditions of Approval	Acceptable	N/A
1.	<p><b>Barrier-Free:</b> The location of the handicap access ramp is in close proximity to designated parking space(s).</p> <p><i>Staff comments: The International Building Code ADA requirements will ensure this standards is met.</i></p>		3-23-15	
2.	<p><b>Circulation/Trail Access:</b> Linkages for safe circulation for pedestrians and bicycles are provided within the site, and connect adjoining existing or proposed sidewalks and bicycle paths. Developments, including single family subdivisions, maintain trail access to existing and established trails through dedication of public easements.</p>	<p>A perimeter building walkway is proposed in addition to a walkway from the front entrance to the sidewalk located at the intersection of Issaquah Fall City Rd. and Highlands Dr. providing access to the King County trail along Highlands Dr. However, non-motorized movement within the parking area shall include the provision of a continuous walkway from on at least one side of parking lot aisles that do not contain angle parking per 18.07.080. Currently there are not enough walkways proposed to meet this requirement.</p>		
3.	<p><b>Design – Parking Areas:</b> Vehicle parking areas are</p>		3-23-15	

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APPENDIX 2, DESIGN STANDARDS	Conditions of Approval	Acceptable	N/A
<p>designed into the project in a manner that screens the majority of the parking area from both the public and the building occupants. Methods for limiting the visibility of the parking area to the surrounding area include: orienting parking areas away from building and pedestrian areas; placing the building adjacent to the main roadway, with parking behind the building; screening parking areas with intensive landscape barriers which provide solid screening during all seasons; using wooden fencing, berms or other solid method of screening; and/or other creative means.</p> <p><b>Staff Comments:</b> <i>A Type 1 landscape screen has been provided for the parking area adjacent where the parking lot is adjacent to the street.</i></p>			
<p>4. <b>Public Access – Adjacent to Site:</b> In areas where lakes, parks and scenic or shared use corridors and other recreational areas are adjacent to the project boundaries, public access is encouraged and enhanced in an environmentally sensitive</p>		3-23-15	

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APPENDIX 2, DESIGN STANDARDS	Conditions of Approval	Acceptable	N/A
<p>manner beyond the predevelopment status.</p> <p><b>Staff Comments:</b> <i>Two walkways on site connect to the sidewalk on Issaquah-Fall City Rd. One of those walkways connect to the sidewalk located at the intersection of Issaquah Fall City Rd. and Highlands Dr. providing access to the King County trail along Highlands Dr.</i></p>			
<p>5. <b>Public Access – Within Site:</b> In nonresidential projects, provisions are made for public access to any lakes and to scenic corridor areas within a site. The access is environmentally sensitive in design.</p>			
<p>6. <b>Trail and Nonmotorized Facility Design:</b>                      Pedestrian and bicycle paths are designed to limit conflicts between motorized and nonmotorized modes, by providing a separated walkway system, bicycle facilities, permanent markings, and other methods. Trails or other nonmotorized facilities should use features such as setbacks, landscaping, fencing, grade separation, and sight lines to</p>	<p>1. Non-motorized movement within the parking area shall include the provision of a continuous walkway from on at least one side of parking lot aisles that do not contain angle parking per 18.07.080. Currently there are not enough walkways proposed to meet this requirement.</p> <p>2. Walkways shall be physically separated from</p>		

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APPENDIX 2, DESIGN STANDARDS		Conditions of Approval	Acceptable	N/A
	maximize the privacy provided to any adjacent single family homes.	vehicular areas by grade, landscaping strips, berms, barriers, curbs or similar means providing pedestrian visibility and consistent with ADA access.		
7.	<b>Transition of Design Elements and Amenities:</b> The site plan provides a desirable transition in relation to the streetscape, including adequate planting, safe nonmotorized movement, and parking areas.	See “Trail and Non-motorized Facility Design” above.		
<b>E. Service and Storage Areas</b>				
1.	<b>Screening – Service Yards and Outdoor Storage:</b> Service yards, machinery storage, other storage areas, dumpster/recycling areas and other places which tend to be unsightly are screened through the use of walls and/or fencing of solid material, softened or accented by plantings. The height of the walls/fencing shall be six (6) feet in height, or at least the height of the items to be screened. Screening will be effective in both winter and summer. For example, in the IC zone, although both would be softened by plantings, a	<ol style="list-style-type: none"> <li>1. The trash enclosure is required landscape screening on the north side of the proposed enclosure per the code. The walkway proposed adjacent to the enclosure will need to be shifted three (3) to five (5) feet to allow for a Type 1 screen in between the enclosure and the pedestrian pathway.</li> <li>2. The type of landscape screen required is a Type 1 Dense Year-Round Sight Barrier and the proposed white rock rose will not</li> </ol>		

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APPENDIX 2, DESIGN STANDARDS	Conditions of Approval	Acceptable	N/A
<p>six (6) foot solid fence/wall may be preferable to a twelve (12) foot solid wall/fence which completely screens heavy machinery since the adjacent uses could be “intensive commercial” as well.</p>	<p>sufficiently provide a Type 1 screen. A type 1 screen shall be a combination of approximately 70% evergreen trees backed by a 100% sight-obscuring fence softened or accented with landscaping.</p> <p>3. Chain link is not compatible with the site design or building architecture, so another fence style shall be chosen that complements the character of this development. A new fence material shall be chosen for the trash enclosure fence and fence at the edge of the parking lot.</p> <p>4. Per IMC 18.12.130.D, all mechanical equipment areas, except at the access areas for the utility boxes, shall be screened from view of the public right-of-way through the use of hedges or fencing on all sides. While not shown in the preliminary plans, all mechanical equipment shall be screened as required by the code.</p>		
<p>2. <b>Screening – Mechanical Equipment:</b> Mechanical</p>	<p>Mechanical equipment is required to be</p>		

SDP15-00001 Fieldstone Memory Care

APPENDIX 2, DESIGN STANDARDS	Conditions of Approval	Acceptable	N/A
<p>equipment is completely screened. Screening will be effective in both winter and summer. Examples of mechanical equipment include electrical transformer pads and vaults, communication equipment, and other utility hardware on roofs, grounds or buildings.</p>	<p>screened per IMC 18.12.130 And requires a Type 1 landscaping a minimum of five (5) feet wide).</p>		
<p>3. <b>Screening – Display Areas:</b> Outdoor display areas for vehicles, other equipment for sale or rent, or live plant material are landscaped in a manner that breaks up the mass of pavement or displayed items but need not be landscaped to have the same screening effect required for a service or storage area.</p>			



**I. SEPA**

1. *The applicant shall enhance the wetland buffer at a 1:1 ratio, an area equal to the 3,026 SF buffer reduction area. Final wetland buffer enhancement plans are required for approval by the Issaquah Development Services Department (DSD) prior to issuing construction permits. Final plans shall include a planting plan and a five (5) year monitoring/maintenance plan with performance standards for monitoring success of the enhancement planting. The plans shall meet standards of the King County Critical Areas Mitigation Guidelines for the planting density and monitoring performance standards.*
2. *A permit from the Washington State Department of Archeology and Historic Preservation (DAHP) shall be obtained under RCW 27.53 for archaeological monitoring of construction, prior to issuance of construction permits. A professional archaeologist shall be onsite during ground disturbance. The applicant shall consult with affected Tribes on appropriate content and media for historic public education and/or art element to be developed and installed on the project site.*
3. *The applicant shall mitigate for potential impacts on public services and bicycle and pedestrian facilities. The City may approve a voluntary payment in lieu of other mitigation. The mitigation fees will be assessed with issuance of Building Permits and the actual fee amount will be the adopted fee in effect at the time of permit issuance. The applicant should pay the voluntary contribution prior to issuance of building permits.*

**II. DESIGN STANDARDS (IMC Appendix 2, Design Criteria Checklist)****A. Site Layout and Overall Design Concepts**

1. **Building Location: no conditions**
2. **Energy Efficient Design: no conditions**
3. **Functional Site Design:**
4. *Non-motorized movement within the parking area shall include the provision of a continuous walkway from on at least one side of parking lot aisles that do not contain angle parking per 18.07.080. Currently there are not enough walkways proposed to meet this requirement.*
5. *Walkways shall be physically separated from vehicular areas by grade, landscaping strips, berms, barriers, curbs or similar means providing pedestrian visibility and consistent with ADA access. Where pedestrian paths cross the parking cross aisles, the path should use an alternative material (e.g. concrete), pattern, or be raised (e.g. speed table), and use truncated domes to notify persons with physical limitations that they are crossing a vehicular route. City Street Standards T-37, Typical Crosswalk Strip for Decorative Pavement, is an acceptable crosswalk treatment.*
6. *Additionally, curbs located within the emergency vehicle clear zone shall be rolled, instead of vertical.*
7. *The loading space shall incorporate a maneuvering space of not less than fifty-two (52) feet in length shall be provided adjacent to the loading dock; this maneuvering space shall not include any area designated for off-street parking storage or trash dumpsters.*

8. *Maximum standard parking stall sizes shall be nine (9) feet wide by twenty (20) feet long and compact stalls shall be nine (9) feet wide by fifteen (15) feet long. Wheelstops shall be positioned eighteen (18) inches into the parking stall. As an alternative to the wheelstop, the applicant may extend the landscape eighteen (18) inches into the parking stall, so that cars may overhang the landscaping. Most regular and compact stall sizes shown are too long and will need to be revised with the building permit.*

**4. Lighting:**

9. *The applicant shall submit a lighting plan and site photometric measurements with the Landscape Plan and Site Work Permit. The lighting plan shall comply with the requirements in IMC 18.07.107, Outdoor Lighting. The lighting plan shall include:*
- *identification of specific lighting areas as required by the IMC;*
  - *lighting type, including product specification sheets or representative photos of lighting fixtures proposed;*
  - *lighting photometrics for the entire site;*
  - *location, spacing and height of light fixtures in relation to trees, walkways and parking areas; and*
  - *provisions to minimize glare and light spillover onto nearby properties.*
10. *Light fixtures locations and clearance dimensions from parking spaces, trees, walkways and emergency access shall be shown on the Site Work Permit and the Landscape Permit plans. Light fixture product specifications must be provided with the landscape plan submittal and all light fixtures shall be of a high quality material.*
11. *The lighting levels currently proposed do not fully meet the code requirements. The lighting within the parking area is limited to 5 foot candles, except at the beginning boundary of the critical area buffer is limited to 0.3 footcandles. The minimum lighting levels for common areas, including the courtyard and walkway is 0.3 footcandles. The lighting levels within the parking and common areas shall meet the code requirements.*

**5. Natural Setting – Views: no conditions**

**6. Existing Vegetation/Topography Features: no conditions**

**7. Historical/Cultural Landmarks: no conditions**

**B. Landscape Design and Use of Plant Materials**

**1. Design Elements**

12. *The trash enclosure is required landscape screening on the north side of the proposed enclosure per the code. The walkway proposed adjacent to the enclosure will need to be shifted three (3) to five (5) feet to allow for a Type 1 screen in between the enclosure and the pedestrian pathway. The type of landscape screen required is a Type 1 Dense Year-Round Sight Barrier and the proposed white rock rose will not sufficiently provide a Type 1 screen. A type 1 screen shall be a combination of approximately 70% evergreen trees backed by a 100% sight-obscuring fence softened or accented with landscaping.*

13. *Chain link is not compatible with the site design or building architecture, so another fence style shall be chosen that complements the character of this development. A new fence material shall be chosen for the trash enclosure fence and fence at the edge of the parking lot.*
14. *Courtyard fence shall be compatible with the site design and building architecture as well.*
  2. **Design Unity: no conditions**
  3. **Enhanced Design: no conditions**
  4. **Usable Open Space Design: no conditions**
  5. **Plant Materials – Selection:**
15. *Plant materials and planting locations must be shown in conjunction with locations of utility lines as part of the Landscape Plans to be submitted with the Site Work Permit. Landscape Plans must show how proposed plant materials and planting locations comply with Sec.5b of Appendix 2, Design Criteria Checklist, of the Issaquah Municipal Code (Exhibit 5).*
16. *Trees located near the entrance to the development shall be evaluated based on sight distance and may be required to be removed or relocated from areas impacting vehicle sightlines and emergency vehicle clearance. Additionally, tree locations will be evaluated based lighting levels to ensure the location of the tree does not diminish the required outdoor lighting illumination of the intended pedestrian areas and parking lot.*

**C. Design Harmony and Compatibility**

1. **Accessory Structures: no conditions**
  2. **Building Materials/Components – Refer to Construction Conditions 12 through 14**
  3. **Compatibility: no conditions**
    - **Building Mass and Architectural Design**
    - **Noise**
    - **Lighting**
  4. **Design Components: no conditions**
    - a. **Colors**
    - b. **Modulation**
    - c. **Façade**
    - d. **Ground level (blank walls)**
    - e. **Large Structures (Modulation)**
  5. **Signage**
17. *The proposed colors, material and design of signs shall be complementary or compatible with the design and architecture of the new facility.*
  18. *Signs shall be permitted separately according to IMC 18.11.*

6. **Transition: no conditions**

**D. Nonmotorized and Vehicular Areas**

1. **Barrier-Free – Refer to Construction Condition 5**
  2. **Circulation/Trail Access – Refer to Construction Conditions 4 and 5**
  3. **Design – Parking Areas**
19. *Locate trees in parking lots to ensure their protection at maturity. This may mean repositioning trees, providing tree protection, and/or another solution. Final location of trees in parking lots will be reviewed with the Site Work Permit.*
  20. *The proposed loading spaces for delivery trucks shall be reconfigured to provide maneuvering space of at least fifty-two (52) feet. The revised configuration for the loading area should also take into consideration visual screening of the loading spaces from Evans Street. A 20-foot lane must be kept clear for fire truck access at all times.*
  21. *Loading spaces shall be marked and signed accordingly.*
    4. **Public Access – Adjacent to Site:** no conditions
    5. **Public Access – Within Site:** no conditions
    6. **Trail and Non-motorized Facility Design:** no conditions
    7. **Transition of Design Elements and Amenities:** no conditions
- E. Service and Storage Areas**
1. **Screening – Trash Enclosure: See Conditions 11 and 12**
  2. **Screening – Mechanical Equipment**
22. *The height of rooftop mechanical screening shall be at least equivalent to the height of the tallest mechanical equipment. Section details showing the mechanical equipment and the architectural screen dimensions, materials and colors, must be provided with the Building Permit plans.*
  23. *Ground mounted mechanical and electrical utility boxes shall be screened in accordance with the requirements of IMC 18.12.130.D. While not shown in the preliminary plans, all mechanical equipment shall be screened as required by the code. Mechanical equipment may be required to be and requires a Type 1 landscaping a minimum of five (5) feet wide. Access doors for large utility boxes shall not face high traffic pedestrian areas and public right-of-way. Ground-mounted mechanical and electrical utility boxes should be clustered together to the extent possible and screened with a fence and/or Type 1 landscape screening. Mechanical and electrical utility boxes should be located as far back from pedestrian paths and provided with adequate space for landscape screening. Final locations, sizes and types of mechanical and electrical utility boxes, including those to be installed in the public right-of-way, shall be shown graphically - with adequate notation regarding their heights, sizes and materials - on the Utility Permit and Landscape Permit plans. Approval of Site Work and Landscape permits will be contingent on meeting the requirements of IMC 18.12.130.D.*

**III. UTILITIES AND FIRE**

24. *Fire flow calculations shall be submitted as part of the Site Work Permit. The project shall demonstrate that water supply is adequate to meet fire flow requirements.*
25. *The placement of some lights within the emergency vehicle clear zone may need to be relocated or the height of the pole and fixture shall be 14 feet in height if within the emergency vehicle clear zone to maintain clearance from emergency vehicles.*
26. *The above referenced conditions will be reviewed by the City of Issaquah Development Services Department and approved with the Site Work Permit prior to construction.*

**IV. LANDSCAPING and TREES**

28. *In accordance with IMC 18.12.160(B), in order to insure that all plant materials used in landscapes shall be maintained in a healthy and growing condition, a cash deposit worth fifty (50) percent of the value of the landscaped plant material, cost of labor, irrigation and materials shall be posted with the City prior to receipt of a temporary or final Certificate of Occupancy. The cash deposit will be returned to the applicant in three (3) years if the plants remain in a healthy growing condition and have achieved full coverage. The Planning Director/Manager may accept other suitable security as permitted in Chapter 18.04.*
29. *All of the trees to be preserved that are shown within the "Limits of Construction" will be signed and shall have protective chain link fencing installed around them prior to and throughout construction.*
30. *All compact and accessible parking stalls shall be appropriately labeled in the Landscape Plans.*
31. *All vegetation is required to be planted and maintained so that no plant material or runoff of irrigation water and fertilizers will be diverted into the critical areas or their associated buffers.*
32. *A final tree preservation plan demonstrating how the project complies with the Tree Preservation provisions of the Issaquah Municipal Code sections 18.12.1370-18.12.1390 and 18.12.141 shall be submitted with the Site Work Permit.*

**V. CONSTRUCTION REVIEW**

33. *Landscape Permit is required to be submitted prior to approval and issuance of the Building Permit associated with the project. The Landscape Permit plans shall demonstrate how the proposed landscape design, planting areas, plant types and site improvements meet the minimum standards established in IMC 18.12. The plans shall be drawn to scale and contain the information identified in IMC 18.12.050.B, Content at a minimum and comply with the submittal requirements in IMC 18.12.050.A.*
34. *The NGPE for the wetland and buffer shall be recorded prior to the issuance of a Building Permit.*



